

AGENDA
WYOMING CITY COUNCIL MEETING
CITY COUNCIL CHAMBERS
MONDAY, AUGUST 1, 2011 AT 7:00 P.M.

- 1) Call to Order**
- 2) Invocation**
Pastor Mark Milkamp, Open Hearts Community Church.
- 3) Pledge of Allegiance**
- 4) Roll Call**
- 5) Student Recognition**
- 6) Approval of Minutes**
From the regular meeting of July 18, 2011.
- 7) Approval of Agenda**
- 8) Public Hearings**
- 9) Public Comment on Agenda Items** (3 minute limit per person)
- 10) Presentations and Proclamations**
 - a) Presentations
 1. Wyoming Parks & Recreation Department, 2010 Annual Report.
 - b) Proclamations
 1. National Night Out 2011 - August 2, 2011
- 11) Petitions and Communications**
 - a) Petitions
 - b) Communications
- 12) Reports from City Officers**
 - a) From City Council
 - b) From City Manager
- 13) Budget Amendments**
 - a) Budget Amendments #007 through #014, to Re-appropriate Monies for Open Purchase Orders, Capital Outlay, Uncompleted Projects and Other Encumbrances from Fiscal Year 2010-11 to Fiscal Year 2011-2012
 - b) DDA Budget Amendment #001 to Re-Appropriate \$469,000 from Fiscal Year 2010-11 to Fiscal Year 2011-12 for Open Purchase Orders, Capital Outlay, Uncompleted Projects and Other Encumbrances
- 14) Consent Agenda**
 - a) Of Appreciation to Carrie Knobloch for her Service as a Member of the Greater Wyoming Community Resource Alliance for the City of Wyoming
 - b) To Appoint Margaret Turner as a Member of the Community Development Committee for the City of Wyoming
- 15) Resolutions**
 - c) To Authorize the Distribution of the Draft Turn On 28th Street Corridor Sub Area Plan
 - d) To Authorize the Distribution of the Draft 2035 Thoroughfare Plan
 - e) To Approve Amendments to the Wyoming Community Development 2011/2012 One Year CDBG Action Plan
- 16) Award of Bids, Contracts, Purchases, and Renewal of Bids and Contracts**
 - f) To Award a Contract for the Gezon Parkway Widening
 - g) To Accept a Proposal for Cathodic Protection Systems for Lift Stations

- h) To Authorize the Purchase of Ammunition
- i) For the Purchase of Laboratory Supplies
- j) To Award a Contract for the Lemery Park Parking Lot Ultra Thin Overlay

17) Ordinances

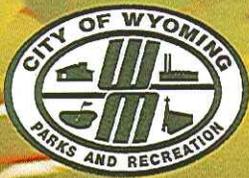
5-11: To Amend Section 90-1 and to Add Sections 90-2 through 90-25 of the Code of the City of Wyoming (Definitions) FINAL READING

18) Informational Material

19) Acknowledgment of Visitors

20) Closed Session (as necessary)

21) Adjournment



WYOMING PARKS AND
RECREATION DEPARTMENT

2010 ANNUAL
REPORT

MISSION

The City of Wyoming Parks and Recreation Department provides programs and services that positively impact the social, economic, health and environmental quality of the community.

VISION

The City of Wyoming Parks and Recreation Department offers programs and services that allow community members to experience physical, mental and social benefits through their leisure time participation, providing opportunities for youth and adults to live, grow and develop into healthy, contributing members of our community.



WYOMING CITY COUNCIL

Jack Poll, Mayor
Sam Bolt, Mayor Pro-Tem
Dan Burrill, At-Large Councilmember
Kent Vanderwood, At-Large Councilmember
William A. VerHulst, 1st Ward Councilmember
Richard K. Pastoor, 2nd Ward Councilmember
Joanne M. Voorhees, 3rd Ward Councilmember

WYOMING PARKS AND RECREATION COMMISSION

Dr. Daniel Lavengood, Chairperson
James Austin
Rhonda Buck
Robert Cook
Megan Harley
Eric Hartfield
Gerald Hopkins
David Skinner
Rod VanOeveren
Doug Wustman
Richard K. Pastoor, Council Liaison



CONNECT



*"The best test of the
quality of a civilization is
the quality of its leisure."*

- Irwin Edman

MAYOR'S MESSAGE



The City of Wyoming has worked very hard to benefit individuals and families, as well as sustain our community through parks and recreation programs and services. Well maintained park facilities improve neighborhood property values and provide safe places for the enjoyment of nature, leisure and play. Our after-school program, T.E.A.M. 21, helps children ground academically, socially and physically. The Wyoming Senior Center broadens opportunities for independent living, civic engagement and cultural exploration. For over 16 years, the citizen supported, dedicated millage for parks and recreation operations has allowed our community to shine throughout the region as a wonderful place to live, work, play and raise a family. Join me in applauding the Parks and Recreation Commission and department staff for their outstanding efforts in 2010.

DIRECTOR'S MESSAGE



Solutions. Collaborations. Innovations. These words reflect the exceptional work by staff and citizens in meeting and expanding community services in parks and recreation this past year. Over \$1.3 million dollars were obtained through grants leveraging the parks and recreation operational millage further than ever before. Over 60 non-profits, businesses and civic organizations joined in supporting over 300 programs and special events resulting in economic and health benefits to tens of thousands of children and adults in our community.

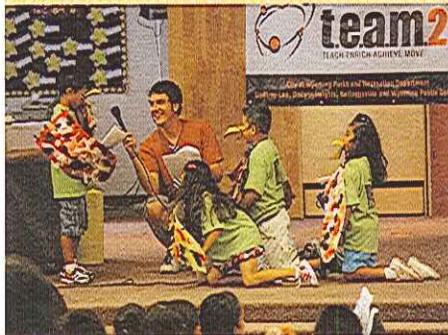
The job of providing high quality, safe after-school, recreation, leisure programs and park facilities is a challenge we work hard to fulfill. This annual report provides you a glimpse of the work we do on your behalf, building and sustaining the community through parks and recreation services in 2010.



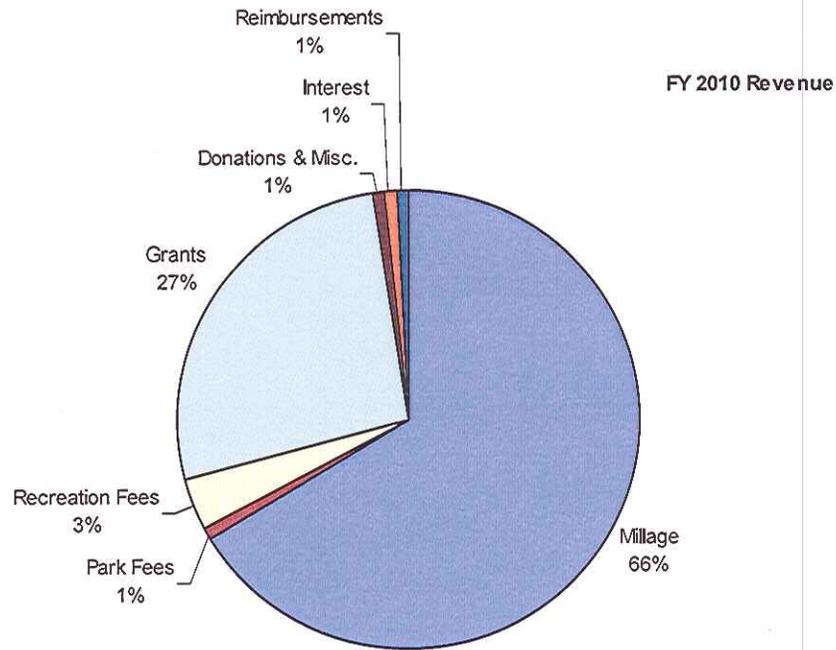
SMILE



"Beth was a super teacher! She involved the kids and parents!...What an asset to Wyoming programs!" - Parent of a Start Smart Soccer participant



FY 2010 FINANCIAL HIGHLIGHTS - REVENUE



Millage	\$3,368,099.59
Grants	\$1,365,256.33
Recreation Fees	\$ 176,223.87
Park Fees	\$ 49,202.95

Reimbursements	\$ 47,610.12
Donations and Misc. Fees	\$ 39,280.17
Interest	\$ 34,502.12

TOTAL OPERATING REVENUE \$5,080,175.15



LEARN

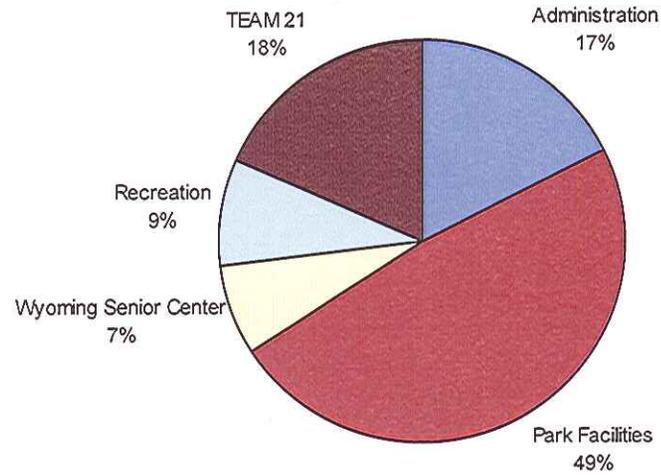
FY 2010 FINANCIAL HIGHLIGHTS - EXPENSES



"What a great time I had! I learned some fantastic new recipes and met some new friends along the way! - Adult Cooking Class participant



FY 2010 Expenses



Park Facilities	\$2,404,066.03
T.E.A.M. 21	\$ 917,789.26
Administration	\$ 866,032.08

Recreation	\$ 423,796.48
Wyoming Senior Center	\$ 361,781.26

TOTAL OPERATING EXPENSE: \$4,973,465.11



CREATE

Parks By The Numbers

Parks:	21
Total Acres:	692
Total Attendance:	57,767
<i>(based on actual reservations)</i>	



"Thank You! We are looking forward to our next event at one of your beautiful parks!" - Pinery Park Reservation Survey Comment

USE AND SURVEYS AFFIRM PARKS QUALITY

During this past year, 5,736 hours were reserved for lodge, shelter and section rentals at Pinery, Lamar and Ideal Parks. As self-reported by user groups, 57,767 people enjoyed the parks system through these reservations. Annual reservation surveys affirm:

- 97% of patrons were satisfied or very satisfied with the conditions of our facilities
- 89% of patrons were satisfied or very satisfied with our restrooms
- 100% of patrons were satisfied or very satisfied with park safety
- 100% of patrons were satisfied or very satisfied with office personnel
- 99% of patrons were satisfied or very satisfied with service by park maintenance personnel

DOG PARK BECOMES A REALITY

Did you know that the City of Wyoming has almost 13,000 dog owning households in our community? That interest was the catalyst for the development of the City of Wyoming's first dog park!

The idea was born three years ago with the Citizen's for Dog Parks Committee. A grassroots, citizen led initiative from site selection, to design, to fundraising, made the park a reality. Constructed at Marquette Park, the public began enjoying the park on a membership basis in November 2010. Donations valued at over \$32,000 funded the construction.

So why a dog park? Community benefits of dog parks are numerous including:

- Promoting responsible pet ownership
- Socializing and exercising of dogs in a safe environment, allowing dogs to legally run off-leash
- Providing tools for realistic enforcement of dog control laws
- Promoting general public health and safety



EXPLORE

ORIOLE PARK UNDERGOES RENOVATION



With grant support from the Michigan Natural Resources Trust Fund, the W.K. Kellogg Foundation Access to Recreation Grant and the dedicated Parks and Recreation Operation Millage, the Oriole Park re-development project (founded on citizen design input) broke ground. In addition to building a splash pad and playground, the re-development includes a shelter, walking trail, tennis courts, basketball court and improved parking. The project represents an investment into our community of \$741,432.

Oriole Park By The Numbers

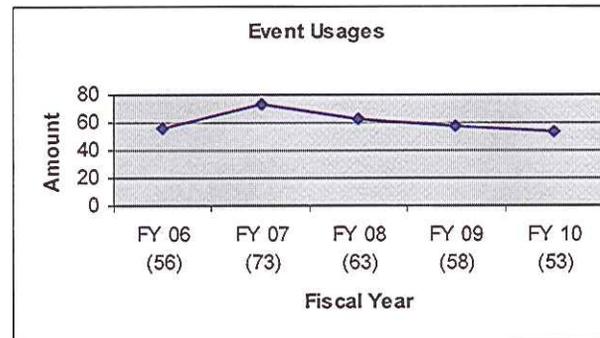
Trees: 215
 Picnic Tables: 5
 Benches: 6
 Walking Path Distance: 2,493 feet

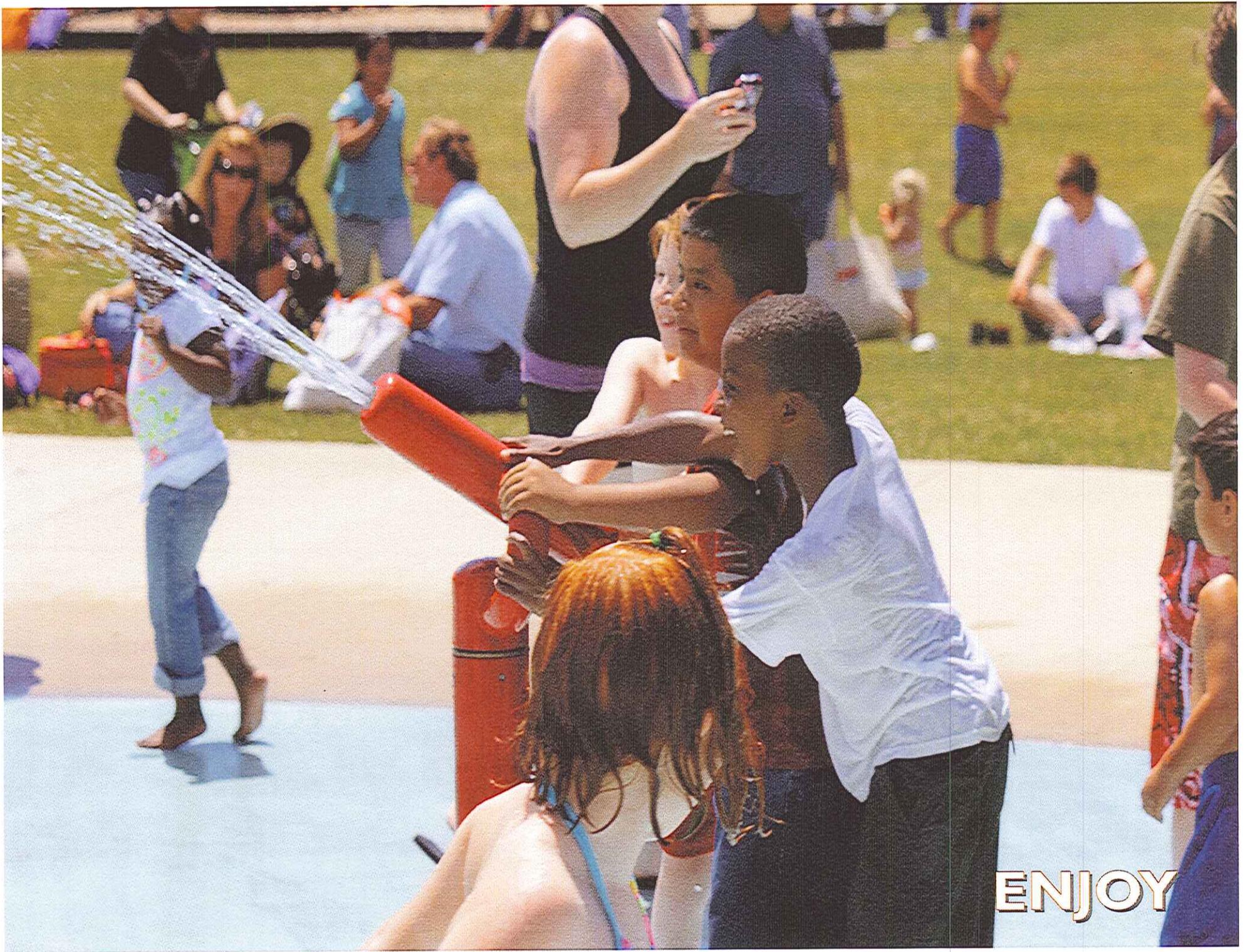


PARKS FACILITIES SUPPORT COMMUNITY EVENTS

Athletic and special event usages of City of Wyoming parks continue to remain consistent over the past five years as schools, businesses, non-profit organizations and churches seek field and gathering space for their events. Some of the entities that utilized facilities in 2010 include:

- | | | |
|--------------------------------------|------------------------------------------|---------------------------------------------|
| <i>American Cancer Society</i> | <i>Kelloggville Public Schools</i> | <i>South Kent Recreation Association</i> |
| <i>Girl Scouts</i> | <i>Messiah Missionary Baptist Church</i> | <i>Special Olympics</i> |
| <i>Grand Rapids First Church</i> | <i>National Heritage Academies</i> | <i>St. Joseph the Worker Church</i> |
| <i>Godfrey-Lee Public Schools</i> | <i>Pinery Park Little League</i> | <i>Wyo. Community Enrichment Commission</i> |
| <i>Godwin Heights Public Schools</i> | <i>Potter's House Christian Schools</i> | <i>Wyoming Jaycees</i> |
| <i>Gordon Food Service</i> | <i>So. Godwin Neighborhood Assoc.</i> | <i>Wyoming Public Schools</i> |





ENJOY



T.E.A.M. 21 by the Numbers

Sites: 9
School Districts: 4

- Wyoming Public Schools
- Godwin Heights Public Schools
- Godfrey-Lee Public Schools
- Kelloggville Public Schools

Total Attendance: 76,975
(based on daily program attendance)

"Without T.E.A.M. 21 our school would not be complete!" – Lee Middle School Teacher

TE.A.M. 21 IMPACTS YOUTH AND COMMUNITY

In its sixth year of existence, the T.E.A.M. 21 After-School Program continues to positively impact the Wyoming community. This year, T.E.A.M. 21 students participated in the Metro Health Farm Market selling homemade items as the only student-operated booth at the market. Students exercised their creativity in designing products including eco-friendly tote bags, jar mixes, sun catchers and matted photographs. On market day, students practiced their math skills and communication skills during interactions with market customers. Proceeds from the market (over \$200) were donated to the construction of the Wyoming Dog Park, where T.E.A.M. 21 was recognized as a "bone" sponsor of the park.

TE.A.M. 21 RECEIVES STATE RECOGNITION

T.E.A.M. 21 received two prestigious recognitions this past year, garnering the Michigan Municipal League's Region 3 Community Excellence Award and the Michigan Association of School Board's Education Excellence Award. MML's Community Excellence Award recognized T.E.A.M. 21 efforts as a part of municipal government to improve the lives of community residents through youth programming, specifically citing the role T.E.A.M. 21 plays in breaking down cultural barriers and promoting positive behavior choices. The MASB's Education Excellence Award specifically honored T.E.A.M. 21's approach to academic improvement through homework assistance, experiential learning and the social-emotional growth of participating students.



michigan municipal league
Better Communities. Better Michigan.





GROW



General Recreation Programs by the Numbers

Programs Offered:	237
Total Attendance:	61,081

"They are both enjoying it!. Abby just wants to learn the bars and Amelia is having a blast doing everything!" - mother of two youth gymnastics participants

PLAY & LEARN PROVIDES FUN FOR YOUTH

2010 was another successful year for the Play & Learn Program. Play & Learn is a summer playground-based program, focused on elementary aged students, which is hosted at various elementary schools in Wyoming. Activities include arts and crafts, board games, sports, field trips and other special events which focus on fun and learning. There were 6,464 registrations across four programming sites (*Oriole Park Elementary, West Elementary, North Godwin Elementary and West Godwin Elementary*).

KID 2 KID SALE GIVES YOUTH BUSINESS LESSONS

In the midst of a tough economy, Wyoming youth had an opportunity this past summer to learn about how to sell items to make money. Based on the garage sale model, youth were encouraged to collect toys, games and other kid-friendly items to price for sale to other youth. The first-time event was held in the parking lot at the Wyoming Senior Center with a total of 44 tables being reserved by area youth. One parent commented, "We participated because we wanted to teach our son the business skills of salesmanship and negotiating. Getting rid of toys he didn't play with anymore was great, too."

WYOMING LAUNCHES TEEN COUNCIL

The first City of Wyoming Parks and Recreation Department Teen Council was selected in 2010 with an original membership of 12 teens between the ages of 12-18. Nine girls and three boys represented seven different schools in the Wyoming area. The Teen Council was created to facilitate teen involvement in program and event planning under the guidance of the Parks and Recreation Department. Their first event was "Rockin @ the Lock-in" which took place at a local elementary school and consisted of a night of sports, video games, board games, movies and snacks. The Teen Council also assisted as volunteers for Parks and Recreation Department events including Pumpkin Path, Candy Cane Hunt and Lunch with Santa, Movie In The Park and the Daddy-Daughter Dance.



BELONG



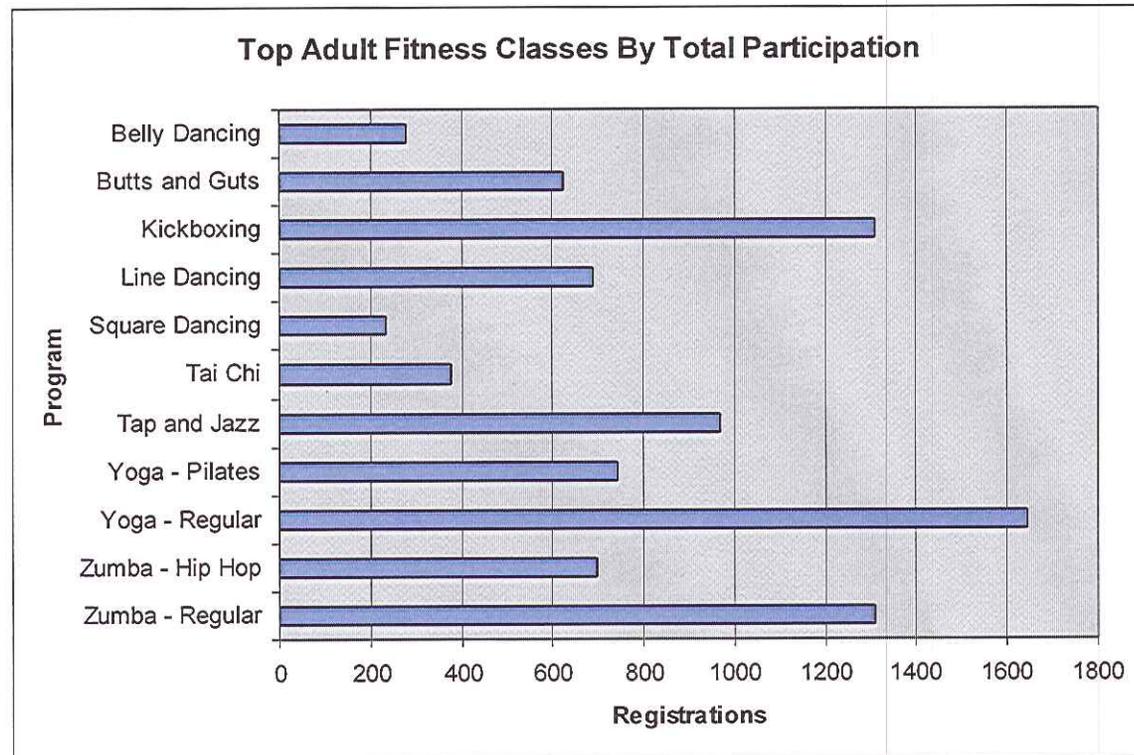
New 2010 Recreation Programs

- Science Exploration—Youth
- Guitar Lessons—Family
- Skateboard Lessons—Youth
- Cooking—Adult and Youth
- Intro to Tennis—Adult
- Kid 2 Kid Sale—Youth

"When you leave (Kickboxing) you know that you had a GOOD workout!" – Kickboxing participant

ADULT FITNESS IMPROVES WELLNESS

The Wyoming Parks and Recreation Department's adult fitness programs continue to provide participants with wellness benefits including improved endurance and fitness, increased muscle tone, decreased risk of illness, reduced stress and weight management control. These individual benefits also have a positive impact on the community as evidence indicates that better individual wellness leads to less sick days off from work or school, improved productivity in the workplace and improved family relationships. 2010 participation in adult fitness programs increased 5% over 2009 with a total of 9,933 registrations in 2010. The most popular 2010 adult fitness programs were Zumba, Kickboxing and Yoga, all of which had over 1,000 registrations for the year.



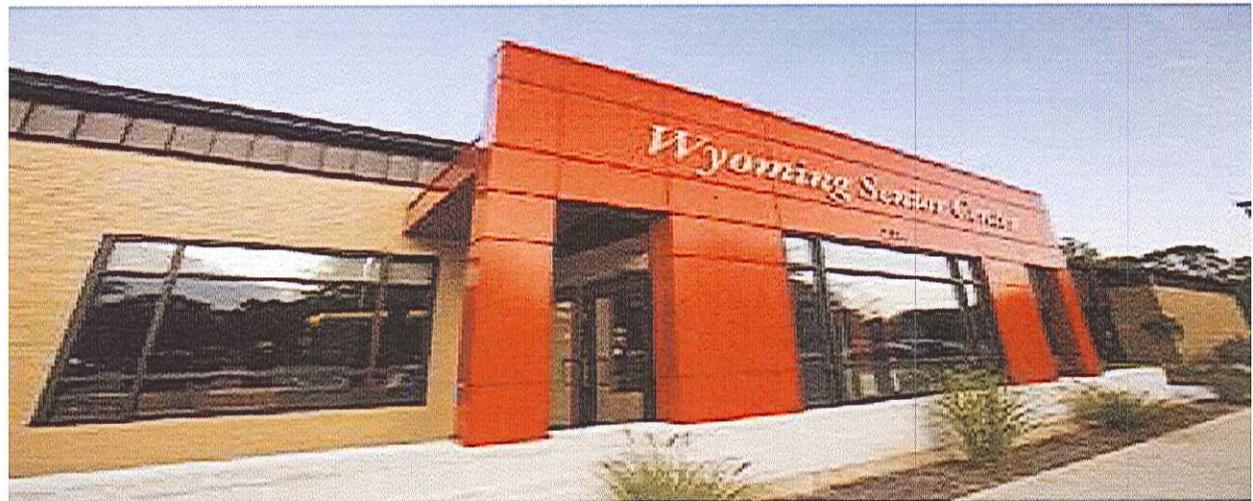
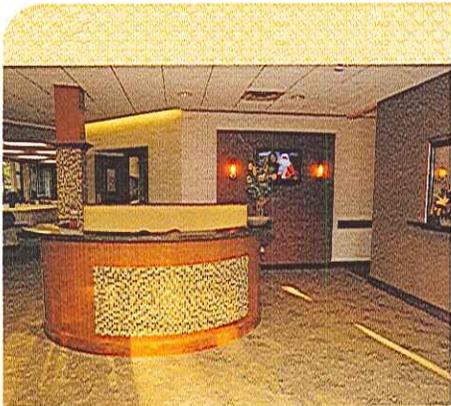


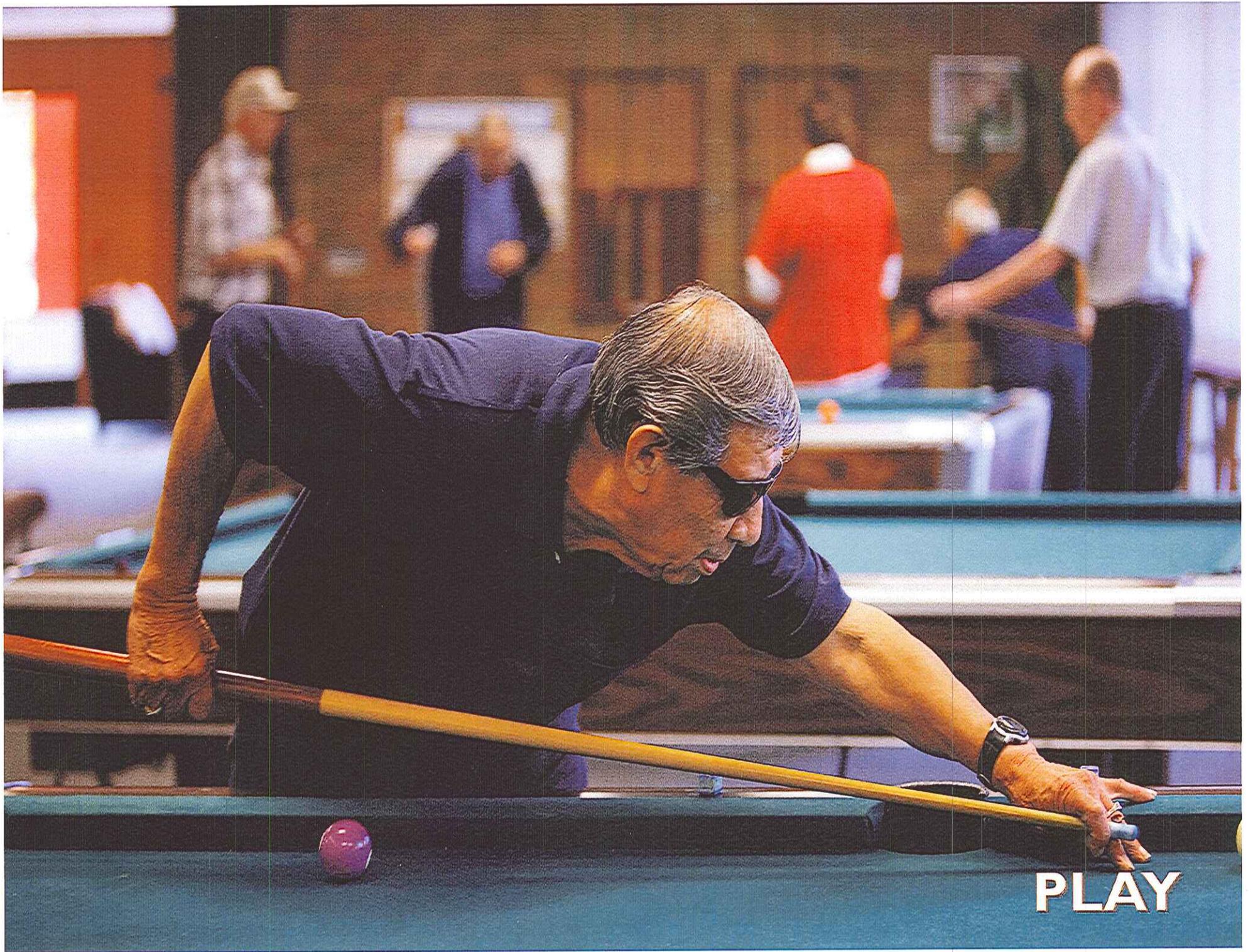
EXERCISE

WSC IMPROVEMENTS AID PARTICIPANT USE

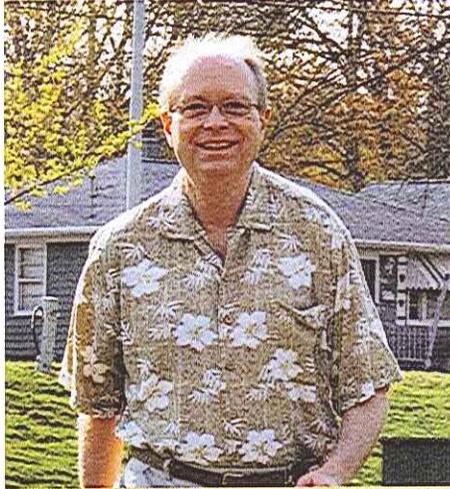
American Recovery and Reinvestment Act funding, in partnership with monies from the dedicated Parks and Recreation Department millage, allowed for a \$140,740 project that renovated the main entrance area and lobby of the Wyoming Senior Center (WSC). The renovations maximized the functionality of the area, providing a more open and inviting design, which eliminated congestions and addressed access concerns for people with disabilities. New furniture provides participants a comfortable place to visit with each other or do some reading. The addition of flat screen television monitors allow for a continuous display of program information and photos.

To complement the main entrance and lobby area remodeling, the WSC invested in an electronic sign to add to the greater curb appeal presence of the facility. The electronic sign provides the opportunity for the WSC to externally display information about programs and services offered, as well as including information about other City of Wyoming events. The modification of the WSC is the latest improvement to City infrastructure on DeHoop and Michael Avenues, joining the Wyoming Branch of the Kent District Library, Wyoming City Hall and the Wyoming Police Department in creating a City service area that community members can easily identify and access.





PLAY



Wyoming Senior Center by the Numbers

Programs Offered:	103
Total Attendance:	61,457

"Who said that getting older is no fun? I can come here and visit with my friends, participate in a class or surf the internet. My life didn't end when I turned 50, it just started down a new path." - WSC participant

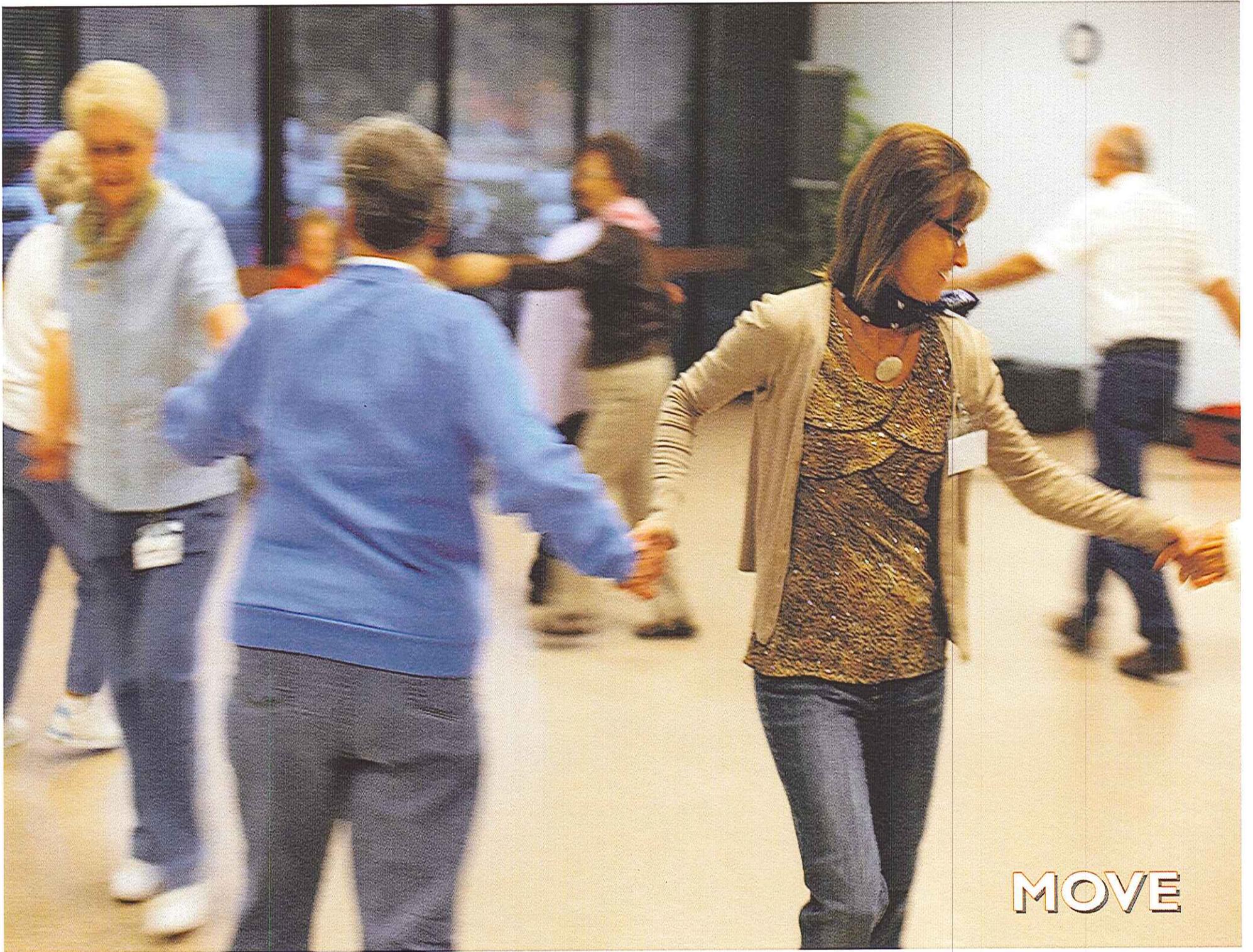
WALK CLUB PUTS IN THE MILES

The Walk Club, a program offered for those community members 50+, grew by 75% in 2010 when compared to 2009. Recognizing that walking is one of the best, easiest and cost-effective methods of exercise, 49 community members took part in 28 walks throughout the year, with participants logging a total of almost 1,600 miles or the equivalent of walking to Salt Lake City, Utah or Key Largo, Florida.

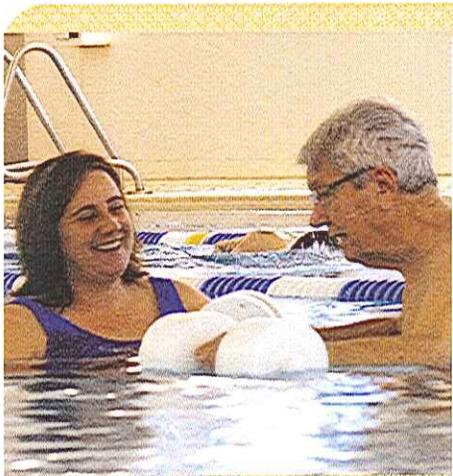
WYOMING ADDS PICKLEBALL COURTS

Some will say it is a relatively new sport, while others have been playing it for years. Whether you have played it or never heard of it, Pickleball is having a coming out party in Wyoming! It has gained quite an interest nationwide with the 50+ crowd as it combines the elements of three sports, badminton, tennis and table tennis. It is played on a tennis court with the same dimensions as a badminton court. The paddle is made of a wood or hard plastic and the ball is made of a hard plastic, similar to a wiffleball in style and shape. The City of Wyoming Parks and Recreation Department created two Pickleball courts in 2010, with one each at Pinery Park and Kelloggsville Park. Programming plans are being made to hold instructional clinics and drop-in play beginning in 2011, target marketing the baby boomer generation. This generation has been identified through the Wyoming Senior Center's Senior Visioning Initiative as an age group that still desires physically active programs like Pickleball.





MOVE



Stepping Stones Partners and Sponsors

Accident Fund Insurance
Area Agency on Aging of Western Michigan
Car City Supercenter
Community Caregivers
Edward Jones
Gerontology Network
Grand Valley State University
Kentwood Public Schools
Mary Free Bed Rehabilitation Hospital
Olmstead Associates
Roger Coles
Salvation Army Kroc Center
SchoolsWork, LLC
Wyoming Senior Fellowship Club

STEPPING STONES CELEBRATES 11TH YEAR

Demand continues to grow for this innovative in-home recreation therapy program which is provided for at-risk older adults aged 60+. Completing its eleventh (11) year of service, Stepping Stones provided 1,556 hours of treatment to 115 individuals throughout Kent County in 2010. The most current data affirms:

- 85% of participants reported maintaining or improving their ability to perform Activities of Daily Living
- 80% of Stepping Stones clients improved their quality of life
- 70% of participants successfully integrated back into the community or met their personal program goals
- 80% of aquatic therapy clients reported improved physical endurance to perform Activities of Daily Living

Funded by the Kent County Senior Millage, with in-kind support from the City of Wyoming Parks and Recreation Department Operational Millage, Stepping Stones is operated from the Wyoming Senior Center.

GO! BUS GETS PEOPLE TO PLACES

Whether it is getting someone to a medical appointment, grocery shopping, worship or the Wyoming Senior Center, the WSC's Transportation Assistance Grant (funded by the Older Americans Act) allows at-risk older adults 60+ to get door-to-door bus service. Interest for Go! Bus tickets continues to be strong as 7,322 tickets were obtained by 80 Kent County residents (who qualified for the service) in 2010. \$12,719 in grant funding was received with participant donations accounting for an additional \$10,301 in monies which were all utilized to purchase Go! Bus tickets. (**NOTE:** Suggested donations are for 1/2 the \$3.00 ticket price which indicates a 94% success rate in obtaining the suggested donations).

GO! BUS
456-6141

E450
SUPER DUTY

THE *PA*

GO





"We participated in the Pumpkin Path because we were a new business and we knew we would get to meet a lot of people. We absolutely loved it. What a great event" - Pumpkin Path Business Host

SPONSORS AND PARTNERS

The following is a partial list of collaborators who assisted the City of Wyoming Parks and Recreation Department in providing quality programs and facilities during 2010:

AARP

Adeline Leigh Catering

Altrusa

American Cancer Society - Relay For Life

American Red Cross

Applebee's

Bam Shirts

City of Wyoming Clean Water Plant

City of Wyoming Fire Department

City of Wyoming Traffic Services

City of Wyoming Police Department

Community Care Givers

Contractors Steel

Edward Jones

Family Outreach Center

Girls Chorale Academy

Godfrey-Lee Public Schools

Godwin Heights Public Schools

GRCC Dental Auxiliary Program

Grand Rapids First - School2Career Progressions

Grand Valley State University

Hearing Association of Grand Rapids

Hearing Consultants, Inc.

Hearthstone Assisted Living

Home Repair Services

Hope Network Foundation

Kelloggsville Public Schools

Kent County Health Department

Kent County Humane Society

KDL - Wyoming Library Branch

Maranda Park Party - WOOD-TV 8

Metro Health

Michigan Amateur Softball Association

My Personal Credit Union

National Heritage Academies - Vanguard

Park Center Lanes

Parkwood Pharmacy

Pines Golf Course

Roman Manufacturing

Senior Meals Program

South Godwin Neighborhood Association

St. Joseph the Worker Church

Tarry Hall Roller Skating Rink

TenderCare Health and Rehabilitation Center

Walgreen's

Wedgwood Christian Services

Wyoming - Kentwood Chamber of Commerce

Wyoming Community Youth Coalition

Wyoming Community Foundation

Wyoming Community Enrichment Commission

Wyoming Senior Fellowship Club

Wyoming Public Schools



MP
My Personal
CREDIT UNION

Maximize your money.



KEY ORF

WILD CATS
17

MP My Personal
CREDIT UNION

Area residents welcome.

COOPERATE

City of Wyoming Parks & Recreation Department

P.O. Box 905
1155 28th St SW
Wyoming, MI 49509



Phone: 616.530.3164
E-mail: parks_info@wyomingmi.gov
www.wyomingmi.gov

Creating community through people, parks, and programs!

PROCLAMATION
NATIONAL NIGHT OUT 2011
August 2, 2011

WHEREAS, the National Association of Town Watch is sponsoring a special, coast-to-coast community crime prevention project on the evening of August 2, 2011 called "National Night Out"; and

WHEREAS, it is essential that all citizens in the City of Wyoming be aware of the importance of crime prevention programs and the positive impact that their participation can have on reducing crime in our neighborhoods; and

WHEREAS, "National Night Out" provides an opportunity for the City of Wyoming to join forces with hundreds of other communities across the country in support of safer neighborhoods and to demonstrate the success of cooperative crime prevention efforts; and

WHEREAS, neighborhood spirit and cooperation is the theme of the "National Night Out" project and is also the key ingredient in helping the Wyoming Police Department to fight crime; and

NOW, THEREFORE, I, JACK A. POLL, Mayor of the City of Wyoming, Michigan do hereby call upon all the citizens of the City of Wyoming to join the National Association of Town Watch in supporting and participating in "National Night Out" on Tuesday, August 2, 2011.

BE IT FURTHER RESOLVED THAT I, JACK A. POLL, Mayor of the City of Wyoming, Michigan do hereby proclaim Tuesday, August 2, 2011 as:

NATIONAL NIGHT OUT

in the City of Wyoming.

JACK A. POLL, MAYOR
City of Wyoming, Michigan

CITY OF WYOMING BUDGET AMENDMENT REQUEST

Date: August 1, 2011

Budget Amendment Number 007

To the Wyoming City Council:

A budget increase of \$ 270,220 For The General Fund is requested for the following reason:
 To reappropriate monies for open purchase orders, capital outlay, uncompleted projects and other encumbrances from the 2010-2011 fiscal year to the 2011-2012 fiscal year per the attached list.

<u>Description/Account Code</u>	<u>Current</u>	<u>Increase</u>	<u>Decrease</u>	<u>Amended</u>
<u>See Attached Sheet</u>				
Fund Balance/Working Capital		_____	_____	<u>270,220</u>

Recommended:  Finance Director _____ City Manager

Motion by Councilmember _____, seconded by Councilmember _____ that the General Appropriations Act for Fiscal Year 2011-2012 be amended by adoption of the foregoing budget amendment.

Motion carried: _____ yeas, _____ nays

I hereby certify that at a _____ meeting of the Wyoming City Council duly held on _____ the foregoing budget amendment was approved.

 Heidi A. Isakson
 City Clerk

August 1, 2011

City of Wyoming
Schedule of Reappropriations
 To the 2011 - 2012 Fiscal Year
For The General Fund
 Budget Amendment Number 007

<u>Account Number</u>	<u>Activity</u>	<u>Purpose</u>	<u>Amount</u>	<u>Total</u>
101-136-13600-984.000	District Court	State Designated Fund		\$ 57,680
101-209-20900-801.000	Assessor	Professional Services		5,500
	Facilities			
101-267-26700-956.014	Maintenance	Veterans Memorial Park		1,800
	Police			
	Administration	Capital Outlay:		
101-305-30500-973.000		Justice Assist Byrne Grant	49,350	
101-305-30500-973.001		Byrne Recover Act	24,840	
101-305-30500-973.056		Computer System	50,990	
101-305-30500-984.017		Computer Equipment	970	126,150
101-305-32100-860.000	Training Act 302	Travel and Training		5,580
101-305-32500-807.000	Communications	Geographic Information Services		42,180
	Fire			
101-337-33800-987.145	Building	Fence		30,000
101-337-33900-740.000	Fire Fighting	Operating Supplies		
		PO #2010-950 Apollo Fire Apparatus	\$ 9,100	
101-337-33900-956.007		Fire Memorial	830	
101-337-33900-956.009		Fire Department Programs	500	1,330
				<u>\$ 270,220</u>

CITY OF WYOMING BUDGET AMENDMENT REQUEST

Date: August 1, 2011

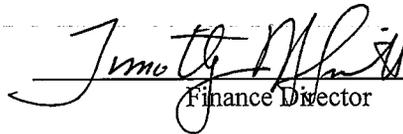
Budget Amendment Number 008

To the Wyoming City Council:

A budget increase of \$ 39,960 For The Major Street Fund is requested for the following reason:
To reappropriate monies for open purchase orders, capital outlay, uncompleted projects and other encumbrances from the 2010-2011 fiscal year to the 2011-2012 fiscal year per the attached list.

<u>Description/Account Code</u>	<u>Current</u>	<u>Increase</u>	<u>Decrease</u>	<u>Amended</u>
See Attached Sheet				
Fund Balance/Working Capital				<u>39,960</u>

Recommended:


Finance Director

City Manager

Motion by Councilmember _____, seconded by Councilmember _____ that the General Appropriations Act for Fiscal Year 2011-2012 be amended by adoption of the foregoing budget amendment.

Motion carried: _____ yeas, _____ nays

I hereby certify that at a _____ meeting of the Wyoming City Council duly held on _____ the foregoing budget amendment was approved.

Heidi A. Isakson
City Clerk

August 1, 2011

City of Wyoming
Schedule of Reappropriations
To the 2011 - 2012 Fiscal Year
For The Major Street Fund
Budget Amendment Number 008

<u>Account Number</u>	<u>Activity</u> Street	<u>Purpose</u>	<u>Amount</u>	<u>Total</u>
	Maintenance			
202-441-46300-801.000		Professional Services PO #2010-1122 URS Corp		\$ 10,390
202-441-46300-807.000		Geographic Information Services		15,080
202-441-46300-930.000		Repairs and Maintenance PO #2011-1090 A-1 Asphalt Sealing & Repair		14,490
	Traffic			
	Services			
202-510.001		Federal Grants LED Street Lighting		(577,500)
202-441-46300-986.960		Capital Outlay Equipment		<u>577,500</u>
				<u>\$ 39,960</u>

August 1, 2011

City of Wyoming
Schedule of Reappropriations
To the 2011 - 2012 Fiscal Year
For The Local Street Fund
Budget Amendment Number 009

<u>Account Number</u>	<u>Activity</u>	<u>Purpose</u>	<u>Amount</u>	<u>Total</u>
	Street Maintenance			
203-441-46300-807000		Geographic Information System		<u>15,860</u>
				<u>\$ 15,860</u>

CITY OF WYOMING BUDGET AMENDMENT REQUEST

Date: August 1, 2011

Budget Amendment Number 010

To the Wyoming City Council:

A budget increase of \$ 48,350 For The Parks and Recreation Fund is requested for the following reason:

To reappropriate monies for open purchase orders, capital outlay, uncompleted projects and other encumbrances from the 2010-2011 fiscal year to the 2011-2012 fiscal year per the attached list.

<u>Description/Account Code</u>	<u>Current</u>	<u>Increase</u>	<u>Decrease</u>	<u>Amended</u>
<u>See Attached Sheet</u>				
Fund Balance/Working Capital		_____	_____	<u>48,350</u>

Recommended:



 Finance Director

_____ City Manager

Motion by Councilmember _____, seconded by Councilmember _____ that the General Appropriations Act for Fiscal Year 2011-2012 be amended by adoption of the foregoing budget amendment.

Motion carried: _____ yeas, _____ nays

I hereby certify that at a _____ meeting of the Wyoming City Council duly held on _____ the foregoing budget amendment was approved.

 Heidi A. Isakson
 City Clerk

August 1, 2011

City of Wyoming
Schedule of Reappropriations
To the 2011 - 2012 Fiscal Year
For The Parks and Recreation Fund
Budget Amendment Number 010

<u>Account Number</u>	<u>Activity</u>	<u>Purpose</u>	<u>Amount</u>	<u>Total</u>
	Administration			
208-752-75200-807.000		Geographic Information Services	\$	1,360
	Facility			
208-752-75600-801.006		Professional Services - Park Design PO # 2011-818 MCSmith Associates		770
208-752-75600-930.000		Repairs and Maintenance PO #2011-890 Procure Tree Services LLC		13,800
208-752-75600-975.183		Capital Outlay - Dog Park		16,920
	Senior Center			
208-752-75800-975225		Capital Outlay - Senior Center		<u>15,500</u>
			\$	<u>48,350</u>

CITY OF WYOMING BUDGET AMENDMENT REQUEST

Date: August 1, 2011

Budget Amendment Number 011

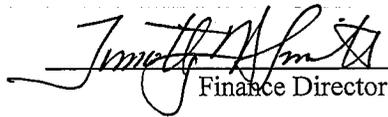
To the Wyoming City Council:

A budget increase of \$ 3,020.00 For The Building Inspections Fund is requested for the following reason:

To reappropriate monies for open purchase orders, capital outlay, uncompleted projects and other encumbrances from the 2010-2011 fiscal year to the 2011-2012 fiscal year per the attached list.

<u>Description/Account Code</u>	<u>Current</u>	<u>Increase</u>	<u>Decrease</u>	<u>Amended</u>
<u>See Attached Sheet</u>				
		_____	_____	
Fund Balance/Working Capital		=====	3,020.00	

Recommended:



 Finance Director

City Manager

Motion by Councilmember _____, seconded by Councilmember _____ that the General Appropriations Act for Fiscal Year 2011-2012 be amended by adoption of the foregoing budget amendment.

Motion carried: _____ yeas, _____ nays

I hereby certify that at a _____ meeting of the Wyoming City Council duly held on _____ the foregoing budget amendment was approved.

 Heidi A. Isakson
 City Clerk

August 1, 2011

City of Wyoming
Schedule of Reappropriations
To the 2011 - 2012 Fiscal Year
For The Building Inspections Fund
Budget Amendment Number 011

<u>Account Number</u>	<u>Activity</u>	<u>Purpose</u>	<u>Amount</u>	<u>Total</u>
	Code			
249-372-37200-807.000	Enforcement	Geographic Information Services		\$ 1,510
	Zoning and Other Programs			
249-722-72200-807.000		Geographic Information Services		<u>1,510</u>
				<u>\$ 3,020</u>

CITY OF WYOMING BUDGET AMENDMENT REQUEST

Date: August 1, 2011

Budget Amendment Number 012

To the Wyoming City Council:

A budget increase of \$ 501,270 For The Sewer Fund is requested for the following reason:
To reappropriate monies for open purchase orders, capital outlay, uncompleted projects and other encumbrances from the 2010-2011 fiscal year to the 2011-2012 fiscal year per the attached list.

<u>Description/Account Code</u>	<u>Current</u>	<u>Increase</u>	<u>Decrease</u>	<u>Amended</u>
<u>See Attached Sheet</u>				
Fund Balance/Working Capital			<u>501,270</u>	

Recommended:


Finance Director

City Manager

Motion by Councilmember _____, seconded by Councilmember _____ that the General Appropriations Act for Fiscal Year 2011-2012 be amended by adoption of the foregoing budget amendment.

Motion carried: _____ yeas, _____ nays

I hereby certify that at a _____ meeting of the Wyoming City Council duly held on _____ the foregoing budget amendment was approved.

Heidi A. Isakson
City Clerk

August 1, 2011

City of Wyoming
Schedule of Reappropriations
 To the 2011 - 2012 Fiscal Year
 For The Sewer Fund
 Budget Amendment Number 012

<u>Account Number</u>	<u>Activity</u>	<u>Purpose</u>	<u>Amount</u>	<u>Total</u>
	Public Works Transmission			
590-441-54200-807000		Geographic Information Services		\$ 13,100
590-441-54200-930.000		Repairs and Maintenance PO #2011-992 B&B Environmental Services LLC		52,910
	Sanitary Sewer Capital Outlay			
590-590-54400-980.600		Operations Equipment #2011-741 Parkway Electric		3,090
590-590-54400-986444		Plant Expansion PO #2008-13 JF New PO # 2008-3 Black and Veatch	\$ 301,450 35,520 <u>15,850</u>	352,820
590-590-54400-98655		Odor Control Project PO #2008-1242 Black and Veatch	\$ 78,910 <u>440</u>	<u>79,350</u>
				<u>\$ 501,270</u>

CITY OF WYOMING BUDGET AMENDMENT REQUEST

Date: August 1, 2011

Budget Amendment Number 013

To the Wyoming City Council:

A budget increase of \$ 5,279,120 For The Water Fund is requested for the following reason:

To reappropriate monies for open purchase orders, capital outlay, uncompleted projects and other encumbrances from the 2010-2011 fiscal year to the 2011-2012 fiscal year per the attached list.

<u>Description/Account Code</u>	<u>Current</u>	<u>Increase</u>	<u>Decrease</u>	<u>Amended</u>
<u>See Attached Sheet</u>				
Fund Balance/Working Capital		_____	_____	<u>5,279,120</u>

Recommended:



 Finance Director

City Manager

Motion by Councilmember _____, seconded by Councilmember _____ that the General Appropriations Act for Fiscal Year 2011-2012 be amended by adoption of the foregoing budget amendment.

Motion carried: _____ years, _____ nays

I hereby certify that at a _____ meeting of the Wyoming City Council duly held on _____ the foregoing budget amendment was approved.

 Heidi A. Isakson
 City Clerk

August 1, 2011

City of Wyoming
Schedule of Reappropriations
 To the 2011 - 2012 Fiscal Year
For The Water Fund
 Budget Amendment Number 013

<u>Account Number</u>	<u>Activity</u>	<u>Purpose</u>	<u>Amount</u>	<u>Total</u>
591-441-56200-807.000	Transportation and Dist Mains	Geographic Information Services		\$ 15,870
591-591-55300-930.000	Pumping and Treatment	Repairs and Maintenance PO #2011-1100 Bry-Air, Inc	\$ 11,180	
		PO #2011-1049 D.L. Contracting LLC	5,990	
		PO #2011-1106 Thyssen Krupp Elevator	3,410	20,580
591-591-55900-930.000	Trans & Dist Gezon Station	Repairs and Maintenance PO #2010-1035 Actuator Specialties	\$ 2,340	
		PO#2011-673 Nelson Tank Engineering	3,140	5,480
591-591-56300-930.000	Trans & Dist Pipeline to Gezon	Repairs and Maintenance PO #2011-889 Actuator Specialties		5,830
591-591-56400-930.000	Trans & Dist Pipeline Meters	Repairs and Maintenance PO #2011-1029 Oudbier Instrument Co.		2,100
591-591-57300-986444	Capital Outlay Water	Plant expansion PO #2011-892 Allied Mechanical Sys.	\$ 2,640,160	
		PO #2008-15 Black and Veatch	7,290	
		PO #2011-893 Black and Veatch	40,870	4,529,910
591-591-57300-987236		Raw Water Line Black and Veatch - PO #2009-138	\$ 673,170	
		Pioneer Construction - PO#2009-1378	21,180	
			5,000	699,350
				<u>\$ 5,279,120</u>

CITY OF WYOMING BUDGET AMENDMENT REQUEST

Date: August 1, 2011

Budget Amendment Number 014

To the Wyoming City Council:

A budget increase of \$ 74,550 For The Motor Pool Fund is requested for the following reason:
 To reappropriate monies for open purchase orders, capital outlay, uncompleted projects and other encumbrances from the 2010-2011 fiscal year to the 2011-2012 fiscal year per the attached list.

<u>Description/Account Code</u>	<u>Current</u>	<u>Increase</u>	<u>Decrease</u>	<u>Amended</u>
<u>See Attached Sheet</u>				
Fund Balance/Working Capital		_____	_____	<u>74,550</u>

Recommended:  Finance Director _____ City Manager

Motion by Councilmember _____, seconded by Councilmember _____ that the General Appropriations Act for Fiscal Year 2011-2012 be amended by adoption of the foregoing budget amendment.

Motion carried: _____ yeas, _____ nays

I hereby certify that at a _____ meeting of the Wyoming City Council duly held on _____ the foregoing budget amendment was approved.

 Heidi A. Isakson
 City Clerk

August 1, 2011

City of Wyoming
Schedule of Reappropriations
To the 2011 - 2012 Fiscal Year
For The Motor Pool Fund
Budget Amendment Number 014

<u>Account Number</u>	<u>Activity</u>	<u>Purpose</u>	<u>Amount</u>	<u>Total</u>
661-441-58300-775.000	Building	Repairs and Maintenance PO #2011-1050 Rohr Gasoline Equipment Inc.		<u>\$ 14,930</u>
662-441-58500-985.000	Capital Outlay Depr Reserve	Vehicles PO #2011-918 Knapheide Truck Equipment		<u>\$ 59,620</u>

CITY OF WYOMING BUDGET AMENDMENT REQUEST

Date: August 1, 2011

DDA Budget Amendment Number 001

To the Wyoming City Council:

A budget increase of \$ 469,000 For The Downtown Development Authority is requested for the following reason:

To reappropriate monies for open purchase orders, capital outlay, uncompleted projects and other encumbrances from the 2010-2011 fiscal year to the 2011-2012 fiscal year per the attached list.

<u>Description/Account Code</u>	<u>Current</u>	<u>Increase</u>	<u>Decrease</u>	<u>Amended</u>
<u>See Attached Sheet</u>				
Fund Balance/Working Capital			<u>469,000</u>	

Recommended:



 Treasurer

Director of Wyoming DDA

Motion by Councilmember _____, seconded by Councilmember _____ that the General Appropriations Act for Fiscal Year 2011-2012 be amended by adoption of the foregoing budget amendment.

Motion carried: _____ yeas, _____ nays

I hereby certify that at a _____ meeting of the Wyoming City Council duly held on _____ the foregoing budget amendment was approved.

Heidi A. Isakson
City Clerk

August 1, 2011

City of Wyoming
Schedule of Reappropriations
To the 2011 - 2012 Fiscal Year
For The Downtown Development Authority
DDA Budget Amendment Number 001

<u>Account Number</u>	<u>Activity</u>	<u>Purpose</u>	<u>Amount</u>	<u>Total</u>
995-000-72800-967.100	Economic Development	Acquisitions/Gap Financing		<u>\$ 469,000</u>

RESOLUTION NO. _____

RESOLUTION OF APPRECIATION TO CARRIE KNOBLOCH FOR HER SERVICE
AS A MEMBER OF THE GREATER WYOMING COMMUNITY RESOURCE ALLIANCE
FOR THE CITY OF WYOMING

WHEREAS, Carrie Knobloch was appointed to the Greater Wyoming Community Resource Alliance on January 5, 2009, and

WHEREAS, Carrie Knobloch, as a member of the Greater Wyoming Community Resource Alliance gave generously of her time and talent to the citizens of Wyoming, now, therefore,

BE IT RESOLVED, the City Council for the City of Wyoming, on behalf of the citizens of the City of Wyoming, does hereby express its sincere appreciation to Carrie Knobloch for her dedicated service as a member of the Greater Wyoming Community Resource Alliance.

Councilmember _____ moved, seconded by Councilmember _____, that the above Resolution be adopted.

Motion carried: _____ Yeas _____ Nays

I hereby certify that the foregoing Resolution was adopted by the City Council for the City of Wyoming, Michigan at a regular session held on the 1st day of August, 2011.

Heidi A. Isakson
Wyoming City Clerk

Resolution No. _____

RESOLUTION NO. _____

RESOLUTION TO APPOINT MARGARET TURNER AS A MEMBER OF THE
COMMUNITY DEVELOPMENT COMMITTEE FOR THE CITY OF WYOMING

WHEREAS, Margaret Turner has submitted an application requesting appointment to the Community Development Committee for the City of Wyoming, and

WHEREAS, a vacancy exists in a regular term ending June 30, 2013 on the Community Development Committee, and

WHEREAS, it is the desire of the City Council that Margaret Turner be appointed to fill the regular term on the Community Development Committee, now therefore

BE IT RESOLVED, the City Council for the City of Wyoming, Michigan, does hereby appoint Margaret Turner as a member of the Community Development Committee for the City of Wyoming for a regular term ending June 30, 2013.

Councilmember _____ moved, seconded by Councilmember _____, that the above Resolution be adopted.

Motion carried: _____ Yeas _____ Nays

I hereby certify that the foregoing Resolution was adopted by the City Council for the City of Wyoming, Michigan at a regular session held on the 1st day of August, 2011.

Heidi A. Isakson
Wyoming City Clerk

Resolution No. _____

RESOLUTION NO. _____

RESOLUTION TO AUTHORIZE THE DISTRIBUTION OF THE DRAFT
TURN ON 28TH STREET CORRIDOR SUB AREA PLAN

WHEREAS, the Wyoming Land Use Plan 2020 adopted in December 2006 pertaining to 28th Street, and the Downtown Development Plan adopted in 2002, are outdated, and

WHEREAS, the Municipal Planning Enabling Act 33 of 2008 requires municipalities to review the Land Use Plan at least every five years, and

WHEREAS, the Turn on 28th Street Committee was appointed to address the declining vitality of 28th Street with the intent of redeveloping the corridor into a sustainable, economically vibrant and desirable place, and

WHEREAS, a consulting team was contracted by the Downtown Development Authority to assist the Wyoming community working with the Turn on 28th Street Committee in developing the corridor sub area plan, and

WHEREAS, over the course of the last year the Turn on 28th Street Committee and consulting team, has been extensively engaged with the Wyoming community in developing the visionary and realistic Draft Turn on 28th Street Corridor Sub Area Plan for potential adoption into the Wyoming Land Use Plan 2020, and

WHEREAS, on July 16, 2011, the Wyoming Planning Commission reviewed the Draft Turn on 28th Street Corridor Sub Area Plan and recommended it to the Wyoming City Council, and

WHEREAS, the Municipal Planning Enabling Act 33 Of 2008 requires, as a part of the Land Use Plan amendment process, the City Council to authorize the Draft Turn on 28th Street Corridor Sub Area Plan for distribution to adjoining communities, and regional planning, transportation and utility agencies, and

BE IT RESOLVED that the Wyoming City Council does hereby authorize the distribution of the Draft Turn on 28th Street Corridor Sub Area Plan.

Councilmember _____ moved, seconded by Councilmember _____, that the above Resolution be adopted.

Motion carried _____ Yeas _____ Nays

I hereby certify that the foregoing Resolution was adopted by the City Council for the City of Wyoming, Michigan at a regular session held on the 1st day of August, 2011.

HEIDI A. ISAKSON
Wyoming City Clerk

RESOLUTION NO. _____



July 25, 2011

MAYOR
Jack A. Poll

AT-LARGE COUNCILMEMBER
Sam Bolt

AT-LARGE COUNCILMEMBER
Dan Burrill

AT-LARGE COUNCILMEMBER
Kent Vanderwood

1ST WARD COUNCILMEMBER
William A. VerHulst

2ND WARD COUNCILMEMBER
Richard K. Pastoor

3RD WARD COUNCILMEMBER
Joanne M. Voorhees

CITY MANAGER
Curtis L. Holt

Ms. Heidi A. Isakson
City Clerk
Wyoming, MI

Subject: Request to authorize the distribution of the Draft Turn on 28th Street Corridor Sub Area Plan

Recommendation: To authorize the distribution of the Draft Turn on 28th Street Corridor Sub Area Plan

Dear Ms. Isakson:

The above referenced request was reviewed by the Wyoming Planning Commission at its regular meeting on July 19, 2011. Staff had the following comments:

The Draft Turn On 28th Street Corridor Sub Area Plan is the culmination of a year long community effort to respond to the extensive range of issues facing the 28th Street business district. To respond to these issues, the Wyoming Downtown Development Authority (DDA) commissioned a consultant team comprised of specialists from multiple firms to work with the community. The paramount objective was to have a transparent planning process to develop a realistic plan for the repurposing of 28th Street into a vibrant and sustainable mixed-use district. The implementation of the plan is envisioned to restore Wyoming's Downtown into a prime retail and office destination. Critical to this plan would be the identification of market niche businesses that would compliment existing businesses and reinvigorate the district.

This planning document should be acknowledged as a true community vision. The planning process began with the formulation of a Steering Committee in October 2009 comprised of officials from the City Council, Planning Commission and DDA, along with several citizens and a development specialist. A mobile tour was held on July 28, 2010 and involved 40 participants including many business stakeholders from the district. The purposes of the tour were to launch the planning process, build momentum, and to identify assets and common concerns. Following that, the consultant team interviewed 18 key stakeholders to gather feedback on the challenges of the district and preferred redevelopment ideas. On October 7, 2010, two workshops were held at the Wyoming Public Library to gather broad community participation and begin formulating ideas for the future of 28th Street. Approximately 110 residents and business people participated. These concepts were then exhibited at a consultant staffed storefront in Rogers Plaza from October

Plaza from October 25 to December 22, 2010. The storefront served to provide individuals opportunities for sharing ideas and opinions. On December 7, 2010 two workshops at the library enabled 60 participants to evaluate and critique three concept designs derived from prior public input. On March 30, 2011 two open house meetings were conducted at the library to present the preliminary final drafts derived from the prior workshops. From the input obtained from the workshops, and after affirmation from the Steering Committee, a preferred concept plan became the favored approach.

After several refinements, the Steering Committee selected the Final Concept Plan which is presented here along with the supporting narrative. The key aspect of this plan is a crescent street extending south of 28th Street through the Rogers Plaza, Wyoming Village Mall and Studio 28 properties. This street will provide the connectivity for the eventual redevelopment and mutual benefit of these, and surrounding, properties. Consideration has been given to locating large retail businesses in the district, but the design is most accommodating to small and midsize businesses that would benefit from the more compact scale. It is anticipated that almost every property within the study area will be redeveloped in the next thirty years. The plan is intended to be purely illustrative of how properties could be redeveloped or retrofitted under the recommendations. The plan does not recommend the removal of any building or business. As the City works with developers to revitalize their properties, it is important to allow market driven redevelopment that will be guided by this plan. The retrofit of this area will be gradual and will occur in phases depending on economics, market demand and access to funding sources for public improvements such as streets, landscaping and other civic amenities.

The Turn On 28th Street Corridor Sub Area Plan is an amendment to the City of Wyoming Land Use Plan 2020. The process for adopting this plan is established by State Law – The Michigan Planning Enabling Act No. 33 of 2008. The Planning Commission is required to submit the plan amendment to the City Council for review and comment. The City Council must then authorize the plan for distribution. The plan will then be distributed to adjoining communities, utilities, and area transportation and planning agencies for review and comment. After 63 days, a public hearing will be scheduled before the Planning Commission for consideration of adopting the plan. The City Council, through resolution, may assert the right to approve, or reject, the plan amendment.

It should be noted that this plan will, at a minimum, incur minor amendments prior to final adoption. The consulting team will address typographical errors and concerns within the text as they arise from public review. If major changes are recommended, the plan will return to the Steering Committee for deliberation.

The adoption of the plan will establish the vision of the City and the greater Wyoming community for the redevelopment of 28th Street. To implement this vision, the City must prepare and adopt new Zoning Codes to guide this redevelopment. Concurrently, City staff will be evaluating potential opportunities for public participation and expenditures in this redevelopment effort.

The Development Review Team suggested the Planning Commission recommend to the City Council the proposed Draft Turn on 28th Street Corridor Sub Area Plan.

A motion was made by Micele, supported by Woodruff, to recommend to City Council the Draft Turn on 28th Street Corridor Sub Area Plan. After discussion, the motion carried unanimously.

It should be noted that prior to the official review of the Plan, and at the beginning of the Planning Commission meeting, several business interests within the study area expressed their concern over maintaining flexibility and supporting existing businesses as the City strives to implement the plan.

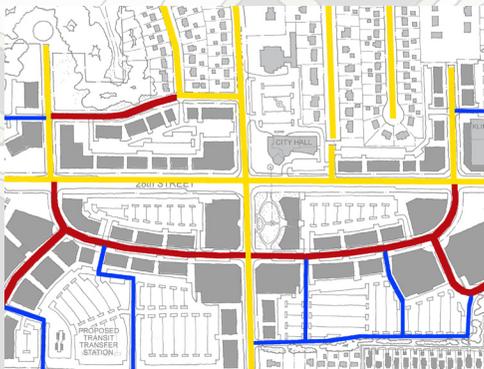
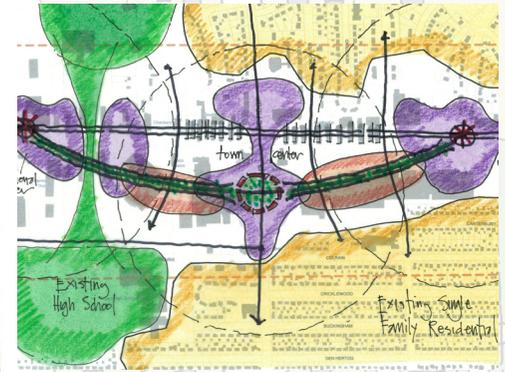
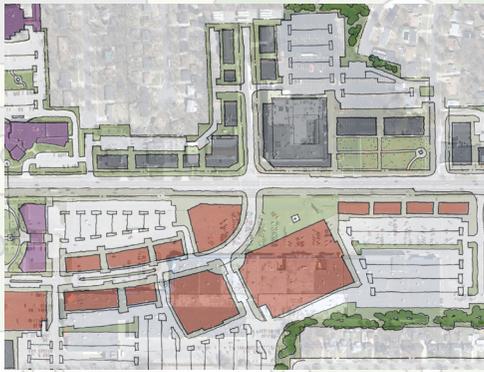
Additional explanation regarding this proposal may be obtained from the Planning Commission minutes of July 19, 2011.

Respectfully submitted,



Timothy Cochran, City Planner
Planning and Development Department

cc: Curtis Holt, City Manager
Rebecca Rynbrandt, Director of Community Services



TURN ON 28TH STREET CORRIDOR SUB AREA PLAN

City of Wyoming, Michigan

MAY 2011

ROGERS HIGH SCHOOL



LandUse|USA
W O N D E R G E M

progressive|ae

Williams & Works
engineers · planners · surveyors
a tradition of service

NEDERVELD





Acknowledgements

Turn on 28th Street Steering Committee

Annette Aranda, Citizen
Sam Bolt, Mayor Pro-Tem
Brian Flanagan, Citizen
Jason Israels, DDA Board
Dennis Kent, MDOT
Doug Kochneff, DDA Chair
James Leach Jr, Citizen
David Micele, Planning Commission
Jim Reminga, Developer
Sherrie Spencer, Planning Commission Chair
Kent Vanderwood, Council Member-At-Large
Tim Cochran, City Planner
Rebecca Rynbrandt, Community Services Director
Barb VanDuren, Deputy City Manager/DDA Director
Bill Dooley, Public Works Director

City of Wyoming City Council

Jack Poll, Mayor
Sam Bolt, Mayor Pro-Tem
William VerHulst, 1st Ward Council Member
Richard Pastoor, 2nd Ward Council Member
Joanne Voorhees, 3rd Ward Council Member
Dan Burrill, Council Member-At-Large
Kent Vanderwood, Council Member-At-Large

City of Wyoming Contributing Staff

Tim Cochran, City Planner
Rebecca Rynbrandt, Community Services Director
Barb VanDuren, Deputy City Manager/DDA Director
Curtis Holt, City Manager
Bill Dooley, Public Works Director
James Carmody, Chief of Police

City of Wyoming Downtown Development Authority

Douglas Kochneff, Chairperson
Jeff Baker, Vice Chairperson
Richard Coombs
Dale Cross
Steven Harkema
Michael Martz
Jack Poll, Mayor
Douglas Vandenberg
Lillian Vander Veen

City of Wyoming Planning Commission

Sherrie Spencer, Chairperson
Rob Postema, Vice Chairperson
Anthony Woodruff
Kirk Bloomquist
John Bueche
Robert Goodheart
William Hegyi
David Micele
Chris Weller

City of Wyoming Zoning Board of Appeals

Dan Burrill, Chairperson
William Vander Sluis, Vice Chairperson
Canda Lomonaco, Secretary
Rodney Beduhn
Blair Dykhouse
Rob Postema
Dave VanHouten
Garry Vandenberg, Alternate
Ronald Palmer, Alternate



Public Meeting Participants

The process to prepare this plan has involved numerous community residents, business owners and interested stakeholders from Wyoming and beyond. The following residents, land owners, and interested persons have made key contributions at the many public input sessions offered during the planning process and their comments, thoughts and ideas are gratefully acknowledged:

Rod Beduhn	Jeff Baker	Lillian VanderVeen	Tessa Westcott	Tom Tilma
Jack Bueche	Lisa Denton	Dave VanHouten	Bob Lomonco	Connie Potterman
Earl Clements	Greg Anderson	Marge Wilson	Dave Dishaw	Eric DeHaan
M. J. Gibrfried	Lupe Plamenden	Dan Burrill	Carol Sheets	Curtis Holt
Matthew Hofstee	Jodi Whew	Jim Carmody	Alan Sheets	Vicki Briggs
Dan Beal	John Russo	Nancy Dermody	William VerHulst	Lorraine Lysoon
Dolores Trimble	Floyd Cage	Robert Israels	Dorothy Semon-Tibbe	Sherry Mix
Harriet Sturim	Dave Bee	Steve Harkema	Ken Horneck	Jan DeGennaro
Ben Shue	Robert Sulaski	J. D. Loeks	Jodi West	Cyndee Cook
Scott Mullarky	Matt Kaluske	Jack Poll	Dani Lane	Steve Loomis
Mary Luchtman	Dennis Kent	Don Stypula	Joy Treece	Bryan D. Belanger
Joanne Voorhees	Doug Kochneff	Tim Timmons	Barb Minier	Bill Hirsch
Amy Payne	Deb Krenz	Eric Hartfield	Uzoma (Zoom) Iwuagwa	Mary Cowles
Dale Cross	Jim Leach	Mark Gorsky	Mark Ruedy	Patrick C. Kelly
Patty Dermody	Kim Lucar	Kathy DenHollander	Leslie Morrissey	Curt Hicky
Todd Duncan	Curt Male	Marsha Harrison	Laurie Kowalczyk	Trudy Kennedy
Matt Howell	Dave Micele	Rachel Connell	Donald VanderVeen	Elizabeth Czabo
Frank Wash	Rich Pastoor	Charlie Steen	Khan Hollis	Bill Velting
Nick Monoyios	Bob Potts	Lee Ann Platschorre	Bob Dolsen	Ken Velting
Rob Arnoys	Sherrie Spencer	Ken Malik	Art Takkinen	Eric Hartfield
Landon Bartley	Tim Timmons	Frank Vitale	Patel Mikl	Ted Westerman
Eric Jorgensen	Dennis Van Dam	Carolyn Brown	Richard Wenger	Wilma Westerman
Russ Lewis	Matt VandeBunte	Mike Rose	Michael Wright	Barb Wingard
Jeanette Vandermyde	May Phillips	Mike Martz	Chris Weller	Julie Buter
Blair Dykhouse	June Keane	Tony Lowe	Megan Sall	Earl Clements
Jon Felske	Barbara Veomans	Karla VerHage	Priscilla English	Joel Anderson
Mike Gallagher	Ginny Jakubowski	Ed Kettle	Jason Wilkins	Melanie Rogers
Brian Gehrcke	Bernadette Burbo	Marilyn Free	Pat Dean	Dennis Cole
Bob Goodheart	Gerri Simons	Sarah Schantz	Pastor JR Pittman	Jerry Fennell
Russ Henkel	Elaine Sulaski	Trisha Baker	Daniel Bouwkamp	



Consultant Team

Mark Miller, AIA - Nederveld

Sharon Woods, MA, MCP - LandUse | USA

Lynée Wells, AICP - Williams & Works

Robert Petko - Progressive AE

Andy Guy - Wondergem

Brian Wegener, AICP, PCP - Williams & Works

Pete LaMourie, PE - Progressive AE

Andy Moore, AICP - Williams & Works

Jay Kilpatrick, AICP, PCP - Williams & Works

Rick Pulaski PE - Nederveld

Terry Sanford - Nederveld

Jay Hoekstra - Grand Valley Metro Council

Certain maps and graphics in this plan were prepared using geographic data provided by the REGIS Agency, and their assistance is gratefully acknowledged.





Executive Summary

Introduction

Background. The 28th Street corridor in Wyoming is one of the original suburban nuclei of retail and entertainment commerce in metropolitan Grand Rapids. The area was home to one of the first enclosed suburban shopping malls in the United States, and at one point, the largest movie theater in the world. More recently, the business district has been confronted with underutilized properties; the loss of several iconic businesses; and other economic and aesthetic challenges common to aging strip commercial areas in first-tier suburbs. As newer centers of commerce have emerged on the suburban periphery elsewhere in the metro area, and as population has shifted, 28th Street has struggled to maintain its identity.

Nevertheless, many viable businesses exist along the centrally-located corridor and healthy, diverse residential neighborhoods flank the district. Area merchants, landowners, community leaders and residents are emboldened with a renewed stake in the rebirth of what some have affectionately called “downtown Wyoming.”

To capitalize on this enthusiasm and respond to current challenges, this Plan was commissioned by the City of Wyoming Downtown Development Authority (DDA) to “Turn On 28th Street.” The Turn On 28th Street process is a comprehensive

effort focused on the preparation of a realistic plan to redevelop and redesign the corridor and to identify new market niches to complement existing viable businesses. The ultimate objective of the planning and design process is to repurpose 28th Street into a vibrant and sustainable mixed-use core to restore its status as a prime retail and entertainment destination.

The Redevelopment Plan recognizes the evolving local marketplace and includes market analyses and design solutions, accompanied by a flexible implementation framework, intended to strengthen the corridor. The recommendations are organized into phased implementation concepts, and are meant to transform the district over time.

Study Area. The extent of the study area is generally limited to the area along 28th Street between Clyde Park Avenue and Burlingame Avenue. These parameters were chosen for several reasons, the most obvious perhaps being that this is the area with the greatest



1959 aerial image looking east down 28th Street. Roger’s Plaza is located in the right hand corner.

concentration of key retail and entertainment sites (i.e., Studio 28, Rogers Plaza, Rogers Department Store/Klingman’s) along 28th Street in the City of Wyoming. In addition, this mile stretch of 28th Street, despite boasting what is likely the highest visibility and greatest mass of important properties, has arguably experienced the most noticeable decline because several of these larger sites sit vacant or are highly underutilized. While this planning and design effort focuses on one key segment of the corridor, it is the intent of the City to generally apply the key design recommendations herein to



the entire 28th Street corridor in Wyoming.

Mission Statement. The planning effort was led by a Steering Committee, comprised of DDA members, City staff, property owners, business owners and other community stakeholders; but included an extensive public input process that generated much interest and participation from area residents. The Redevelopment Plan mission statement, prepared in August of 2010 by the Steering Committee, is as follows:

“The planning process will be transparent and inclusive, resulting in a bold and compelling plan. That plan will incorporate safe, convenient and desirable access and connections for all users. It will present a vision of an inviting and vibrant business community with complementing business types and appealing designs. And it will establish a practical strategy to restore the long-term economic viability of the district.”

- Turn on 28th Street Mission Statement

Structure of Plan. The Plan is organized into the following Chapters:

Chapter 1: Existing Conditions

This chapter includes a map of the region and the study area, and narrative and graphic descriptions of the corridor as it evolved from into an auto-oriented strip development form.

Chapter 2: Market Strategy

This chapter includes a summary of the results of the market analysis containing the retail

gap analysis, import/export analysis, housing assessment, and the office/jobs assessment.

Chapter 3: Public Input

The public input chapter provides an overview of the public involvement activities and the major outcomes.

Chapter 4: The Design Process

The design process chapter includes each concept plan developed by the design team through a process of feedback loops with the steering committee and public.

Chapter 5: Final Plan

Chapter 5 contains the final illustrative plan for the study area in addition to phasing plans for redevelopment.

Chapter 6: Implementation

The implementation chapter presents realistic and strategic steps for the suburban retrofit of the study area. Also included is a matrix outlining each step, timing, responsible party and potential funding sources.

Two Appendices are included by reference in this plan and are on file with the City of Wyoming but not bound into this document. Appendix One is the Market Strategy by LandUse|USA. Appendix Two is the Public Input Report, which contains summaries and outcomes from each public involvement activity.

Recommendations

The following summarizes the key recommendation of this plan:

Design

- The final design combines the properties on the south side of 28th Street between Clyde park and Burlingame into a mixed-use town center with new complete streets and compact blocks. A curving slip street is used as the organizing element of the design and represents the new town center’s “main street”.
- The plan proposes a modest increase in the number of retail uses with a more substantial increase in office (approximately 600,000 square feet) and residential (about 600 dwellings) uses.
- The transformation from the existing development pattern into the pattern proposed in this plan is divided into seven logical steps to be constructed over the next thirty years, or as market conditions warrant.
- Most buildings along the new streets are proposed to be multiple-story mixed use buildings, with retail on



the ground floor and residential or offices on the upper floors.

- Street frontages of these buildings should be transparent and promote activity on the street.

Implementation

- Key first steps to implementation include acquiring easements for the new crescent street and actively targeting new uses as outlined in the Market Strategy.

REGULATORY/PROCEDURAL

- Amend Master Plan and Zoning Ordinance
- Plan for utility extensions, stormwater management

TRANSPORTATION

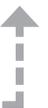
- Explore grant opportunities to aid funding
- Improve sidewalks and non-motorized connections
- Develop a 28th Street Access Management Plan
- Collaborate with The Rapid to plan for future transit service

MARKETING AND PROMOTION

- Public outreach
- Seek out development partnerships
- Market plan and sites to developers

ECONOMIC DEVELOPMENT AND FINANCING ALTERNATIVES

- Focus on recruiting both small and large businesses
- Seek partnering opportunities for economic development
- Secure external funding sources to aid redevelopment process
- Create small business incubator to help startups





Existing Conditions

History

M-11, or 28th Street, is the original “South belt-way” for the Grand Rapids metropolitan area. Following World War II, the suburbs blossomed and this first-tier suburb grew from 20,396 for Wyoming Township in 1940 to 45,829 in 1960 for the newly incorporated city.

28th Street in Wyoming was the place to go for any thing automobile-related. From car dealerships to the drive-in theater, the corridor evolved around the automobile. 28th Street is an iconic corridor in West Michigan with a storied history.



The Loeks Beltline Drive-In movie establishment opened on July 10, 1948, followed by a

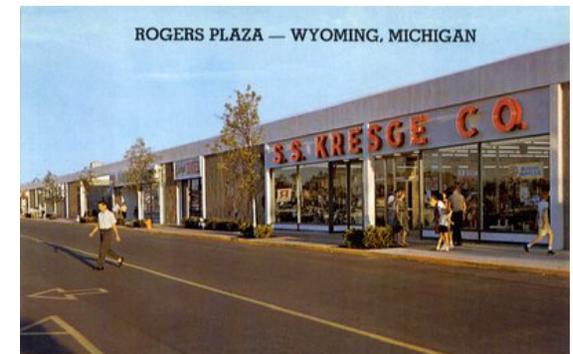


December 25, 1965 opening of neighboring Studio 28. Studio 28 was named to showcase its address at 1350 - 28th Street, and was the first multiplex theater in the United States east of the Mississippi River.

One of the first commercial uses established was a McDonald's restaurant, which located near the northwest corner of 28th Street and Michael Avenue in 1954. It was one of the first of such restaurants to open in the area.

Rogers Plaza, recognized as one of the first enclosed shopping malls in the United States, opened in July of 1960. By May of 1966, 36 different retail establishments had located within the shopping center. Wyoming Village Mall opened across the street from Rogers Plaza in March of 1964, and included Wurzburg, a local department store. In 1968, 28th Street experienced traffic counts of 45,000 vehicles per day - the second busiest thoroughfare in Michigan (only the John Lodge Freeway in Detroit had higher traffic counts).

In 1976, Studio 28 expanded to six screens and in March of 1984, expanded to twelve screens - a move that increased seating capacity to more than 4,000. At this point, Studio 28 became the only 12-screen complex in the country. The Beltline Drive-In closed in September of 1987 to accommodate another addition onto Studio 28. The facility was expanded to twenty screens in November of 1988, increasing seating capacity to more than 6,000 and establishing the world's largest freestanding theater complex, until 1995.



Another notable moment at Studio 28 occurred the day after Thanksgiving in 1990, when a record 16,000 moviegoers visited the theater to watch such films as “Dances with Wolves” and “Home Alone.” This theater attendance record remains unbroken.

Today, the corridor is characterized by significant commercial vacancies, reduced traffic counts and shifting land uses, as described in the following sections.



Study Area

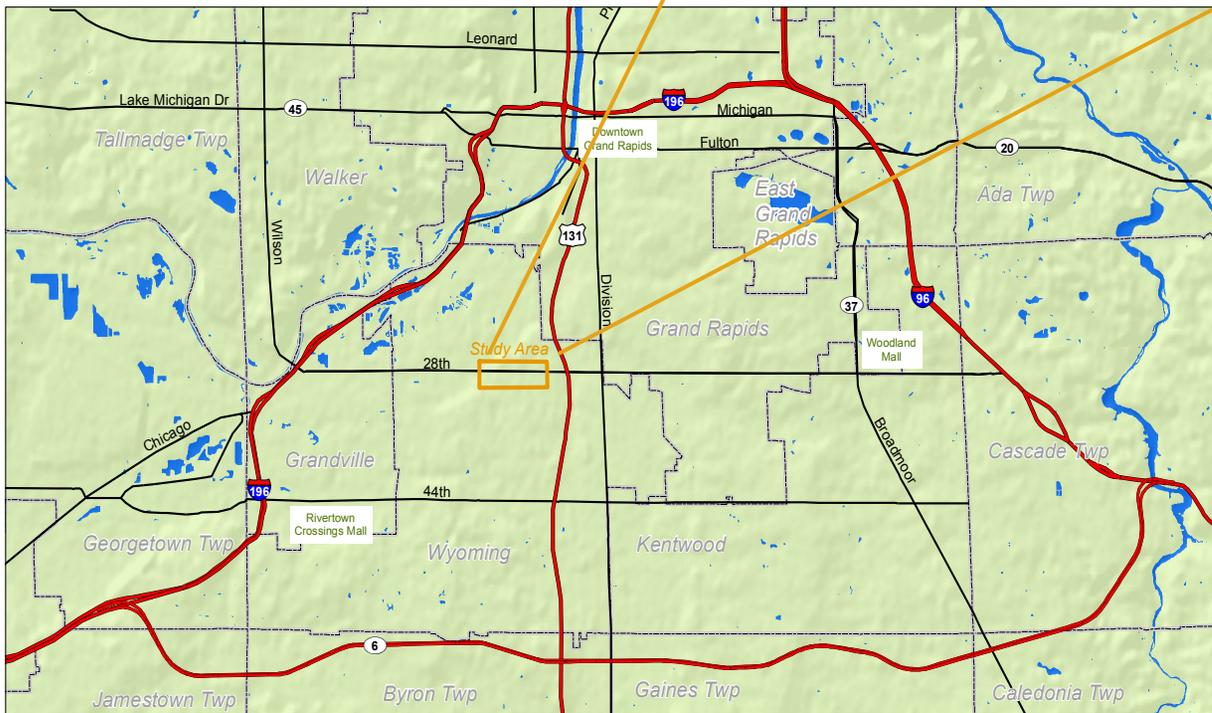
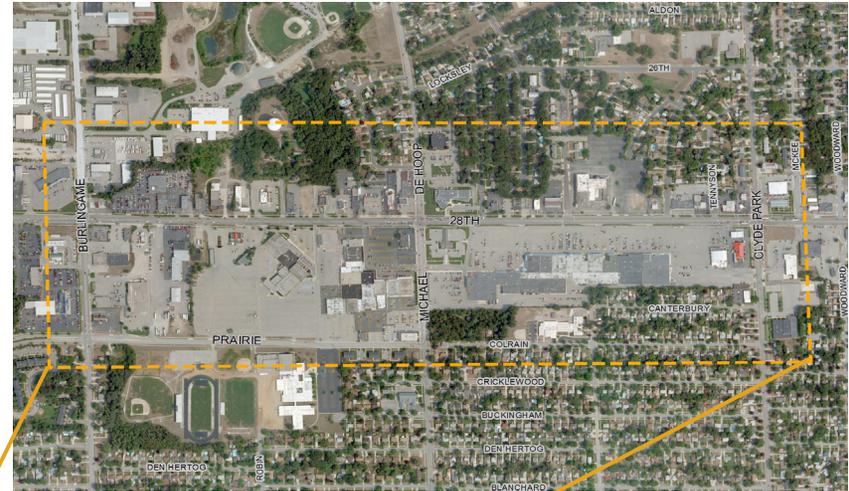
Wyoming's Town Center. The study area includes a portion of 28th Street from Clyde Park Avenue to the east and Burlingame to the west. Selected by the City and the Downtown Development Authority (DDA), this portion of 28th Street is within the City's Downtown Center Zoning District.

From a regional standpoint, the study area is centrally located amongst the commercial focal points in the metro area. The study area is equidistant from downtown Grand Rapids, Rivertown Crossings and Woodland Mall. What makes the area different is the development form of a linear commercial corridor with 45,000 average daily vehicle trips and the mix of large and small lot development.

A half mile to the east, US-131 is the primary north-south corridor traversing the metropolitan area. West of the study area, 28th Street

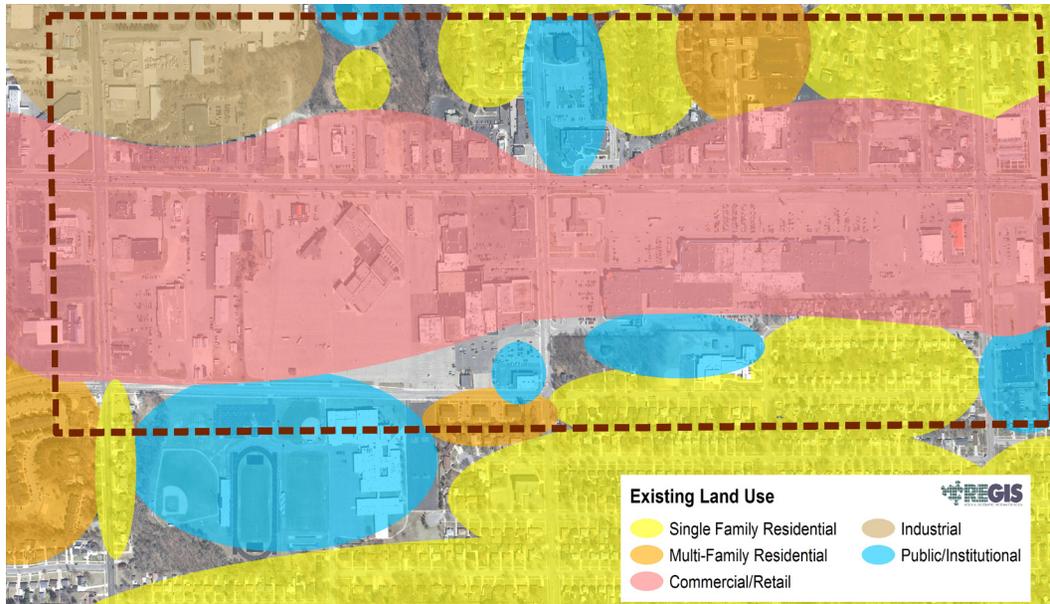
eventually intersects I-196, connecting the lakeshore area with Grand Rapids and eventually merging with I-96 toward Lansing or Muskegon.

This portion of 28th Street, being M-11, is classified as an urban principal arterial and regulated by the Michigan Department of Transportation. It is a five-lane cross section with



a continuous center turn lane. Additional turn lanes are present at the signalized intersections at Clyde Park, Michael/DeHoop and Burlingame.

Accommodations for pedestrians and bicyclists were an after thought in the evolution of the corridor. With intermittent sidewalks, limited bike lanes, driving might be the safest option with the existing framework of blocks, streets, and buildings.



Land Use

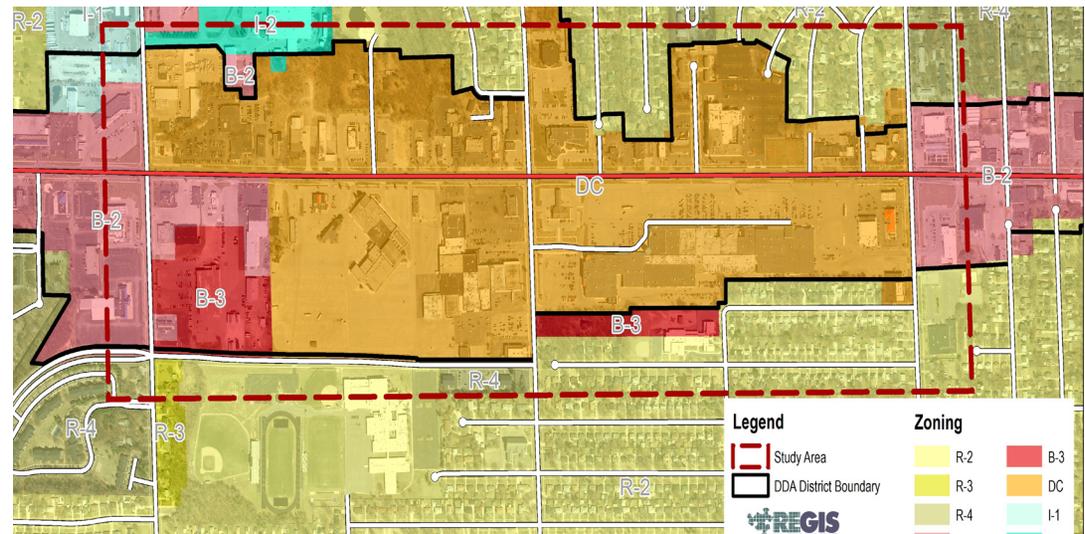
- Commercial land uses are predominant along 28th Street amounting to more than one million square feet. Most commercial buildings are thirty years old or more and many show signs of deferred maintenance and give a dated impression.
- Single family residential neighborhoods are behind the commercial uses. These neighborhoods were once connected via local streets that have since been converted to dead ends or cul-de-sacs.
- Institutional uses, including the City Hall, schools, and post office

are interspersed within the study area.

- A limited amount of multi-family residential housing is present to the south, west and north.

Zoning

- The Downtown Center (DC) district is the predominant zoning district, which allows mixed use commercial and requires building facade treatments such as transparency, and building alignment with roadways.
- The B2 and B3 districts permit general commercial and office uses. Residential uses are not permitted, and design/form is not regulated.





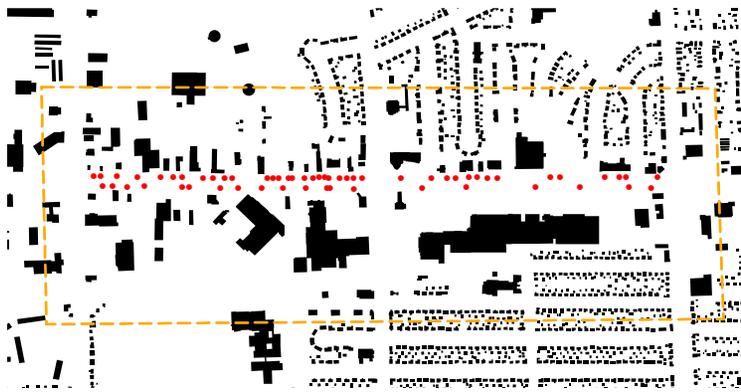
TURN ON 28TH STREET CORRIDOR SUB-AREA PLAN



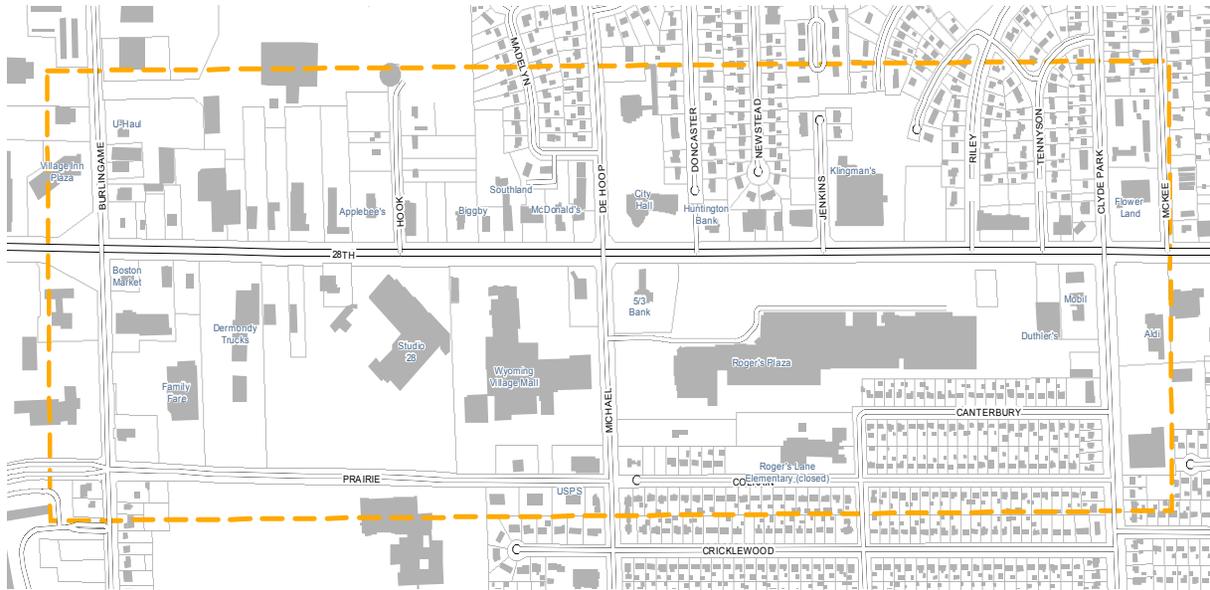
Road Configuration. Few roads penetrate the study area, signifying the “super block” pattern of development, especially south of 28th Street. The large block framework exists to provide access to 28th Street while providing ample parking. The isolation of the residential neighborhoods is evidenced by the intentional dead end street segments and broken connections into the commercial corridor. To prevent “cut-through” traffic from 28th Street invading residential areas, in the 1960s the City intentionally interrupted the former grid pattern.



Impervious Surfaces. Impervious surfaces, including both buildings and parking areas, are shown in grey. Note the almost 100% coverage in the southwest quadrant of the study area. Additionally, the red dots illustrate driveways and curb cuts into the 28th Street right-of-way. There are 59 curb cuts in the study area along 28th Street. Many opportunities exist for driveway consolidation, which could help traffic flow, and provide a safer environment for motorists, pedestrians and bicyclists.

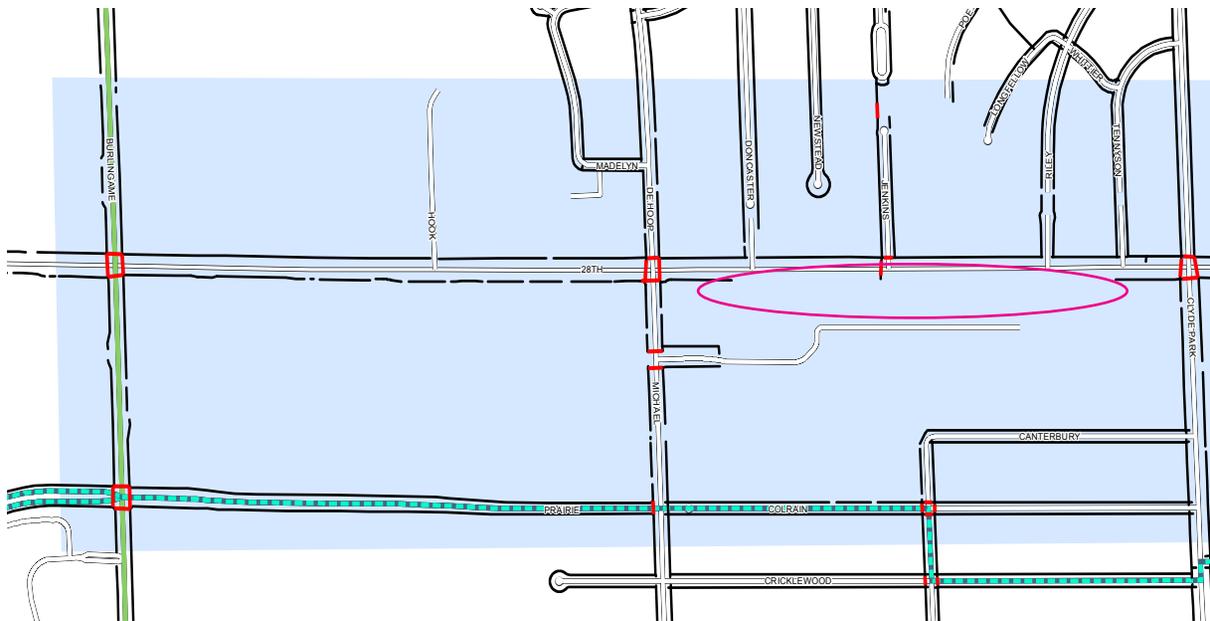


Building Patterns. The “figure ground analysis” examines the patterns of development within the study area. The commercial pattern is linear, especially north of 28th Street where lots are more shallow and buildings generally are built to and fronting the road. To the south, buildings are larger, setback from the road, and sometimes canted to maximize a motorist’s view from the road. The residential neighborhood to the south exhibits a grid pattern, while to the north, the residential pattern is curvilinear with the cul-de-sacs discussed above.



Property lines and key businesses.

Property lines and key businesses illustrate the complexity of the ownership pattern and redevelopment potential. For a suburban retrofit, ownership patterns can make redevelopment difficult. Coordination with landowners, business owners, banks, and other entities is necessary for implementation of any plan. Fortunately, the superblock configuration might facilitate implementation since there are fewer entities to coordinate. The north side of 28th Street, however, might represent more challenges to redevelopment since properties are smaller and more owners have a stake in the process and outcome.



Non-motorized Circulation.

Sidewalks are shown in black, bike routes in teal and trails in green. Sidewalks are intermittent along 28th Street with expanses where dedicated areas for pedestrians are nonexistent. Prairie Street to the south is a designated bike route, although there are no markings or designated lanes for bicyclists on the street. Burlingame Avenue includes a proposed trail with a connection north to Kent Trails and south to the Beltline trail paralleling M-6. Noteworthy is the absence of connected sidewalk along the south side of 28th Street at Roger's Plaza (circled).



Demographics

With the release of the 2010 Census data, it is apparent that the City of Wyoming and the overall West Michigan region have continued to experience growth. However, the table below illustrates that all communities in the immediate region experienced slower growth rates between 2000 and 2010 than in previous decades, and that a couple of communities even lost population. The cities of Grand Rapids and Grandville saw population decreases, as did the State of Michigan, while neighboring Georgetown and Byron Townships saw the largest percentage increases.

Table 1.1 Regional Population Data, US Census Bureau

	1980	1990	2000	2010	% Change 1980-1990	% Change 1990-2000	% Change 2000-2010
Michigan	9,262,078	9,295,297	9,938,444	9,883,640	4.7	6.9	-0.6
Kent County	444,506	500,631	574,335	602,622	12.6	14.7	4.9
Wyoming	59,616	63,891	69,368	72,125	6.8	8.7	4
Grand Rapids	181,843	189,126	197,800	188,040	4	4.6	-4.9
Grandville	12,412	15,624	16,263	15,378	25.9	4.1	-5.4
Kentwood	30,438	37,826	45,255	48,707	24.3	19.6	7.6
Walker	15,088	17,279	21,842	23,537	14.5	26.4	7.8
Byron Township	10,104	13,235	17,553	20,317	31	32.6	15.9
Georgetown Twp	26,104	32,672	41,658	46,985	25.2	27.5	12.8

This trend is generally evident in the West Michigan region, as outlying townships appear to have experienced the bulk of growth, while some cities and villages grew more slowly, if at all. This “shift” in population may have implications for 28th Street and other “older” commercial areas, because as people move further out to newer suburban communities, purchasing power in the immediate areas may decline.

Nevertheless, continued growth in Wyoming speaks to its high quality housing, affordable neighborhoods, and the city’s relatively central and convenient location in the greater Grand Rapids area and ample transportation connections. In addition, it is likely that the bulk of

new growth is occurring in the City’s “panhandle,” that part of Wyoming south of the City of Grandville, and in the area around Metro Health Village at the Byron Center Avenue interchange with M-6. 2010 Census data at the block group level was not yet released at the time this plan was prepared, so it is impossible to analyze growth trends in the study area at this time.

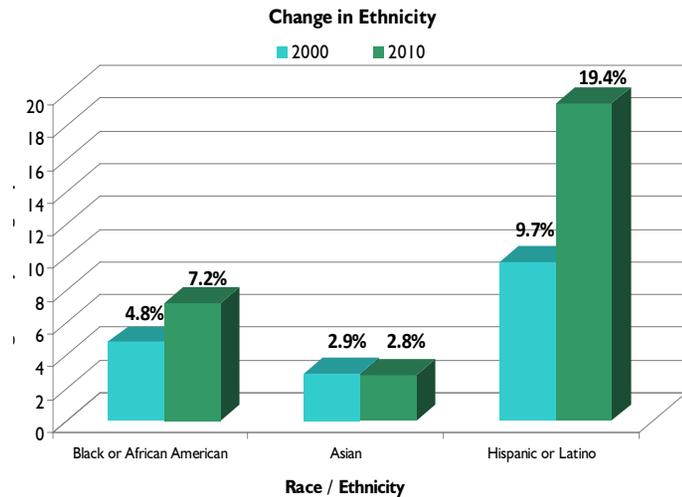
Ethnicity. As the city has gained population over the past few decades, it has also become more diverse, with this change seeming more rapid between 2000 and 2010. Figure 1.1 illustrates that overall ethnic diversity has increased in the City of Wyoming between 2000 and 2010.

Hispanic or Latino residents of Wyoming now make up almost 20% of the City’s population, an increase of 100% from 2000.

An influx of people of a particular ethnic heritage can attract continued waves of growth from families who seek to live near people of their culture, who speak their language and understand their customs. Many indicators in the community suggest that the bulk of Hispanic growth in Wyoming is within a few miles of the study area. This may imply that changes in the type of restaurants, grocery stores, and even the makeup of household units are on the horizon. Additionally, ethnic change may also bring more



Figure I.1



families with children, helping to re-energize and strengthen family neighborhoods and enhance the cultural richness of the community.

Previous Plans/Studies

2002 Downtown Plan. In 2002, the City adopted the Downtown Plan, which envisioned a mid-block road between 28th Street and Prairie. The Plan also hinged on Roger’s Plaza, Rogers Department Store, and Studio 28 as anchor establishments knitting the retail corridor together. Today, those establishments are vacant or facing financial challenges and can no longer be relied upon as destination establishments. The 2002 Downtown Plan included a market study that proposed the following five

major retail additions: discount department store (e.g., Wal-Mart), supermarket (e.g., Family Fare), farmers market, apparel (e.g. A.J. Wright), and home improvement (e.g. Home Depot). In total, the study indicated a market for almost 400,000 square feet of additional retail space.

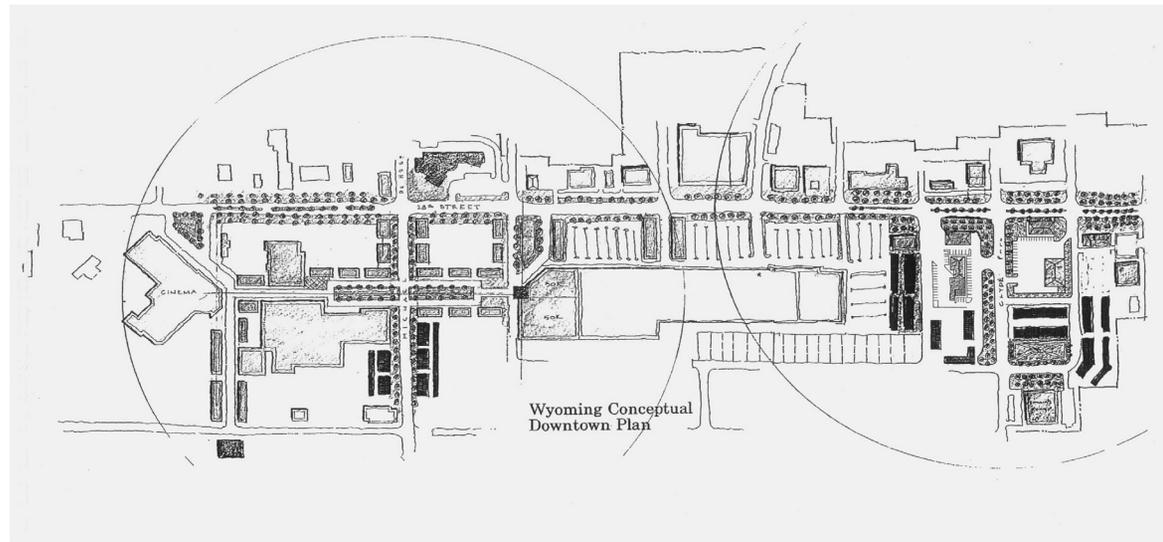
2006 Master Plan/Land Use Plan 2020

In 2006, the City adopted the Land Use Plan 2020. The plan included the following goals for the redevelopment area:

“The 28th Street/Michael/DeHoop

node will become a true downtown center, establishing a distinctive identity and sense of place for Wyoming. Redevelopment will create opportunities for new investment, mixing commercial, residential, office, and entertainment uses in this central core area.

- “Assemble vacant and declining commercial properties for redevelopment as high density residential or mixed use projects.
- “Promote the construction of mid- and highrise buildings within the Downtown Center to form a distinctive urban core, supporting the current businesses in the area and creating the synergy for others to come.”



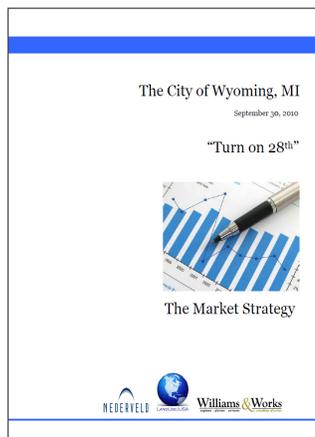


Market Analysis

Summary

In early 2011, LandUse|USA completed an evaluation of market conditions in the corridor area. The complete assessment, included as Appendix 2 and titled “The Market Strategy,” includes analyses of detailed economic data and other information to allow the Steering Committee and the public to understand various local and regional market forces that influence redevelopment of 28th Street.

The Market Strategy concludes that the 28th Street study area can continue to provide essential shopping alternatives within the local market, and that this role is sustainable going forward. The strategy indicates that the corridor should not be viewed as “expired or exhausted” and will continue to serve as a viable shopping district. Nevertheless, the report acknowledges that the corridor is dated and struggling to transition itself in response to changing demographics, and must be redefined and reformatted to include the appropriate mixture of land uses.



The 28th Street Corridor is dated and struggling to transition itself in response to changing demographics

The Market Strategy focuses on the opportunities for retail and office space, and also includes a Housing Assessment. It is concluded that any retail strategy should target sensible *general merchandise and department stores, clothing and accessory stores and speciality grocery stores*. These are the best near-term opportunities for the study area. Additionally, the effort should focus on business retention (see economic gardening sidebar on page 18), fostering small business growth and assisting new ethnic businesses.

The strategy also calls for planning and recruiting important anchors for build-to-suit office buildings. Targeted categories for jobs and office users should include:

- telecommunications
- information, data processing
- book, software publishing

- credit intermediation, financial
- professional technical services
- medical services
- education

Other types of complementary businesses are also supportable for leased office space, but the most efficient approach is to target anchors first. The Market Strategy discourages the development of speculative office space that lacks immediate anchors.

The results of the Housing Assessment indicate that the best opportunities with the lowest risks are in affordable and market-rate products beginning at prices of \$124,900. Ideal formats should include multi-family attached units; a mix of both owner and renter choices; an emphasis on unit formats that would meet the needs of working families; up-and-comers working in downtown Grand Rapids; and seniors or other residents with special needs.

Highlights

Following are some specific highlights taken from the complete market analysis.

- Kent County overall is a strong import market for most top-level retail categories, with the exception of food and beverage stores and motor vehicle parts dealers.



- US-131, located just to the east of the study area, has the region’s highest Average Daily Traffic of 75,000 to 105,000 vehicle trips per day. 28th Street in the Study Area experiences about 28,000 trips per day.
- Today, over 22% of the population within one mile of the study area is Hispanic, compared to just 3.5% in 1990; signaling increases in cultural diversity and growth.
- Grocery stores, limited-service restaurants, clothing and accessories stores, department and discount stores are underrepresented in Kent County.
- Potential retail establishments include business services, Hispanic grocery stores, sporting goods stores, computer supplies, veterinarians,



The Market Study proposes a combined strategy of retention and recruitment

day care centers, furniture repair, music stores, cultural cuisine, apparel stores, etc.

- Retail gap analyses show there is barely enough demand in the local trade area to accommodate a discount department store; however one might still fit with an aggressive market strategy to compete with the market’s local brand, Meijer.

Beyond the task of redeveloping the physical environment within the Study Area, economic gardening activities would benefit existing businesses by nurturing and growing existing businesses. Specific strategies include:

- Assist independent store owners to grow their businesses, trade up into better retail space, improve their marketing, and reach more customers.

- Identify stores that would benefit from more efficient space in a more compact, mixed-use project that includes in-line retail along the streets and sidewalks.

What is Economic Gardening?

Economic gardening builds on community assets and targets existing human capital with a business culture that includes training, mentoring programs, networking opportunities, incubator workspace, media recognition, low-interest loans, and resource-sharing. Based on new leadership in Lansing (including the Michigan Economic Development Corporation), the future of economic gardening must reach beyond the traditional approach of offering incentives to help offset costs for home-grown businesses.

Going forward, true economic gardening must also include an expanded menu of local services and support. Examples include helping relatively small but growing companies map out new strategies; research their competition; explore new markets and niches; and find employees with specialized skills.



The needs of some small and emerging businesses can be met with new incubator buildings, which could anchor business campuses. Ideally, business incubators are physically connected and integrated in training facilities with a campus design that enables innovation and sharing of talent, training facilities and other resources.



Education sessions may provide local business owners with tools necessary to growth their business



Table 2.1 Demographic Summary and Comparison

	Year	Primary Trade Area	Effective Trade Area	Kent County
Population	1990	185,914	466,384	500,631
	2000	205,037	528,539	574,335
	2007	206,200	551,334	602,318
	2009 Est. 2nd Quarter	206,700	561,402	614,724
	2010 Est. 2nd Quarter	205,847	561,083	614,297
Average Age	2010	35.7	36.6	36.7
% with some College Education	2010	22.6%	22.6%	22.9%
2010 Percent Renter Occupied Housing	2010	31.8%	30.8%	29.4%
Median Housing Value	2010	\$102,600	\$120,400	\$119,986

- Offer small business educational sessions, perhaps supported by the DDA and the City, on topics such as business management, book-keeping and taxes, staffing, payroll and benefits, sales, promotions, crossmarketing, customer service, and other best business practices.

Table 2.1 includes the summary of demographic data related to the market strategy. In general, residents within the primary trade area tend to be younger than average for Kent County, are more likely to be renters, and have lower than average home values.

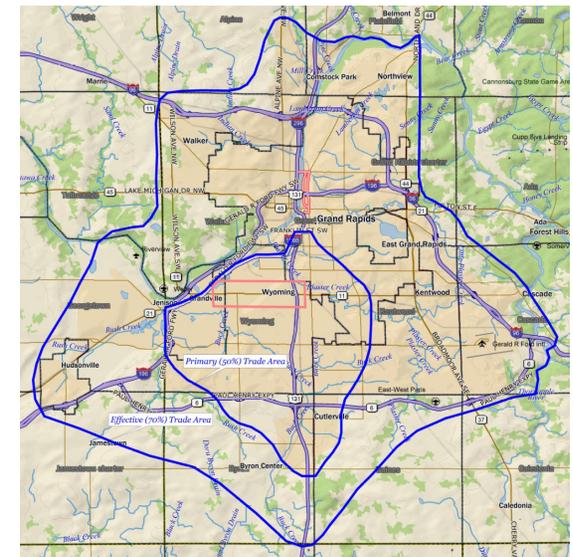
The results of the Market Strategy provide square-footage thresholds for retail and office uses. A combined strategy of retention and recruitment is proposed, with the following parameters:

Retail:

- 350,000 to 660,000 square feet of retail should be retained.
- 310,000 to 475,000 square feet of retail should be recruited.
- So the total retail potential is 660,000 to 1,135,000 square feet in the redevelopment project.

Office:

- 300,000 to 600,000 square feet of office anchors should be added.
- 300,000 to 600,000 square feet of office tenants should be added.
- So the total office potential 600,000 to 1,200,000 square feet in the redevelopment project.



The primary and effective trade areas were based on the distribution of population and income throughout the market; the study area's location relative to highway connectors; its location relative to competing shopping and regional destinations; and results of an import-export analysis.



Total:

- 1,260,000 to 2,355,000 square feet in redevelopment.

For retail, the strategy identified an opportunity for one 120,000 to 140,000 square foot big box general merchandise anchor establishment, and two 60,000 to 90,000 square foot medium box support anchors.

The concept plans and final plan for the corridor incorporate these projected users and the retention of existing retailers.

Other economic catalysts are identified as significant economic drivers, but are also discerning in their location. Advanced education and health/medical services are suggested users, however, attracting them to the City will require proactive recruitment efforts.

For the complete market analysis, see Appendix 1, incorporated here by reference.

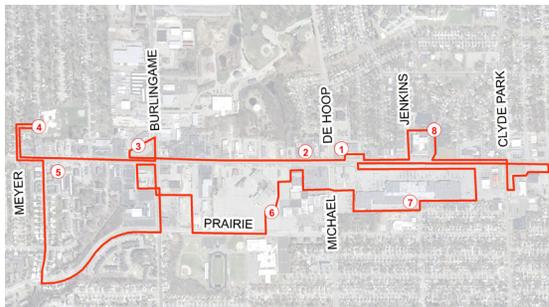


Public Input

The City of Wyoming and the DDA committed to using community input to frame policy development for the future of 28th Street. The planning and design process employed an extensive public input process, providing several opportunities for residents and business owners to participate and influence the outcome of the overall design. This chapter contains a summary of each public input opportunity but a more complete discussion on each can be found in the *Turn on 28th Street Public Input Compilation*, a separate report. (Appendix 2)

Mobile Tour

A mobile tour was held on July 28, 2010 to enable the consultant team to make observations about existing conditions in terms of land use, landscaping, access management, architecture, circulation, signage, lighting, aesthetics and other features. The tour involved forty participants



The Mobile Tour Route gave participants a thorough idea of the challenges facing 28th Street.

on a large bus and included a specific route with eight stops and an informal, yet structured input process.

As the kick-off to the planning and design process, the mobile tour helped to establish a common base from which to move forward by building momentum and excitement about the process and a common understanding of the corridor’s particular physical assets and challenges. Certain features of the corridor were praised while others were identified as areas for improvement. Additionally, the mobile tour served as an opportunity for business and property owners to delve into the background of a particular site or issue and discuss future plans for development of their properties. See the Public Input Compilation (Appendix 2) for the complete report of the mobile tour.

Stakeholder Interviews

As part of the public input process, the consultant team interviewed eighteen individuals representing key stakeholders in July and August of 2010. The interviewees included business owners, landowners, a school district representative, local and regional community leaders, residents, and others. In most cases, the interviews were conducted face-to-face in a conference room at the Wyoming Branch Library and conversations generally flowed freely, although planned questions were asked to ensure a meaningful outcome. The purpose of the stakeholder interviews was to garner feedback on: Any specific

challenges unique to a particular property; information related to site demolition costs or other real estate matters; preferable redevelopment frameworks; and other pertinent issues.

Ideas Workshop

The consultant team facilitated two Ideas Workshop meetings on October 7, 2010; one held in the late morning and the other in the evening to garner the broadest possible participation. The workshop was primarily designed to enable participants to share ideas relative to the



Participants generate concepts at the Ideas Workshop.



Turn on 28th Street Public Input Summary

Event	Number of Participants	Date	Purpose	Outcome or Result
Mobile Tour	40	July 28, 2010	Make observations about existing conditions in 28th Street study area	Generated excitement, project momentum Common understanding of corridor's assets and challenges
Stakeholder Interviews	18	July and August 2010	Specific challenges unique to a particular property Information related to site demolition costs or other real estate matters Preferable redevelopment frameworks	A number of common opinions about 28th Street that likely reflect widely-held views about the corridor and the challenges it faces
Ideas Workshops	About 110	October 18, 2010	Garner broad public input and ideas regarding the future of 28th Street	The development of themes and priorities regarding 28th Street redevelopment Assisted in creating the foundation for the redevelopment strategy
Rogers Plaza Storefront	At least 50 - 60	October - December 2010	Provide an informal venue for mall patrons to stop and share some thoughts about the future of 28th Street.	Suggestions for a number of land uses and actions to aid in the redevelopment of 28th Street
Design Workshops	About 60	December 7, 2010	Enable participants to review and critique three concept designs	Refinements to the layout Selection of most important design features Continued discussion regarding most desirable land uses
Informal Customer Survey	68	October - December 2010	Provide insight into a lack (or perceived lack) of services, businesses and entertainment opportunities along the corridor	Wide range of responses to the question: "What type of business would you like to see on 28th Street in the future?"
Open House	About 50 - 60	March 30, 2010	Present preliminary final drafts of overall design framework	Further refinements to the preliminary designs
Facebook Page	Several hundred	August 2010 - May 2011	Provide project updates Gather additional public input	Increased project interest and awareness



The process offered many outlets to express ideas.

future of 28th Street. The City engaged in an extensive effort to assure that many people participated before the workshop, and those efforts didn't disappoint: Approximately 70 individuals attended the morning session and about 40 people attended the late session. The meetings included an overview presentation, precedent examples of other successful suburban retrofits, and a summary of the market assessment.

Rogers Plaza Storefront Office

To test concepts generated at the two Ideas Workshop sessions and to garner additional input, the consultant team established a "storefront office" in a highly accessible donated space near the center of the Rogers Plaza shopping center. The storefront office was open consistent with a posted schedule, which included most Mondays and Wednesdays for two hour sessions from October 25, 2010 to December 22, 2010. Concepts generated at the Ideas Workshop were exhibited in the storefront office, along with several

informative maps and a summary of the market analysis. The primary intent of the storefront office was to provide an informal venue for mall patrons to stop and share some thoughts about the future of 28th Street. It was believed that such an informal setting brought to the people would increase candor and the range of opinions recorded. It is difficult to evaluate participation in the storefront office, because of its informality; but it has been estimated that at least fifty to sixty people offered input.

Design Workshop

On December 7, 2010 the consultant team facilitated two Design Workshops, which enabled about sixty participants to review and critique three concept designs. One workshop was conducted in the late morning and the other was conducted in the evening. Participants were encouraged to propose modifications and refine the general layout models presented in the concepts. In addition, participants were asked to



Alternatives being discussed at the Design Workshop.

select the most important design features from each proposed configuration and to continue the discussion on desirable land uses.

Informal Customer Survey

Marge Wilson, owner of Marge's Donut Den on 28th Street, developed and implemented an opinion survey instrument and asked her patrons, "What would you like to see on 28th Street?" While the survey was not conducted by the consultant team, the results of the survey are beneficial and add to the collection of perspectives gained throughout the public input exercises. The results of the survey are presented in the Public Input Compilation, under separate cover.



A consultant presents the evolution of design development at the Open House.

Open House

Two Open House meetings were held on March 30, 2011, one in the late morning and the other



TURN ON 28TH STREET CORRIDOR SUB-AREA PLAN

in the evening to foster broad participation. The purpose of the Open House was to recapitulate the planning and design process and to present the preliminary final drafts of the overall design framework. A combined fifty to sixty people attended both the morning and evening session.

Facebook / Web Interaction

A Facebook page was established to connect with Wyoming residents and others interested in the Turn On 28th Street effort. Updates were posted periodically to inform individuals of upcoming meetings, input opportunities, and results of various phases of the project.



MDOT Coordination Meetings

Two meetings were conducted with MDOT during the Turn On 28th Street project. In addition to these specific meetings, Dennis Kent was MDOT’s representative on the steering

committee during the process and provided input regarding the concept iterations during the design phase.

Selected Comments from the Turn on 28th Street Facebook page

I don’t know much about revitalizing towns but this seems like a great start. Good for you Wyoming, involving the citizens in the planning. Wyoming could once again be a great town and a great place to live.

How about getting an IKEA where Studio 28 is? This would put a store smack dab between Ann Arbor and Chicago! Let’s make Wyoming a destination city for Michigan!! Think of the jobs and revenue!!! Just a thought. It makes me sad to see all of the businesses closing up shop on what was once a major street for commerce.

Wyoming used to be a classy area. Now, I would not even use the Wyoming public schools. Growth is good but at what cost to the residents?

Many of the ideas that I have heard so far are really great., but one thing people have to keep in mind is that we must insure that established businesses are incorporated into any idea, and any businesses that are incorporated into the plan should get face lifts and or moved to another part of the downtown area.

The number one thing I would say is Keep the Flea Market!!!! Flea Markets are a dying breed, and a lot of people depend financially on the Studio 28 Flea Market. I usually go down there to walk through and look/buy every week. It’s a good way for me to get some exercise in walking around the place. Sometimes I go there to sell stuff...

I have a suggestion about the Studio 28 building. Maybe it could become a second-run “Celebration Cinema”, similar to the one at Woodland Mall. I think a lot of people would go to it because, for one thing, people don’t always want to pay high ticket prices, and plus there are so many people who loved that theater, such as myself!





The Design Process

Essential to any design and planning activity is a process for carefully reviewing and refining the proposed concepts. A process called feedback loops enabled the design team to continuously refine the concepts and ultimately, advance a final plan that was thoroughly vetted and accepted by the stakeholders and steering committee. Each concept incorporates the

market strategy as well as input from all public involvement activities. There were five feedback loops/design iterations.

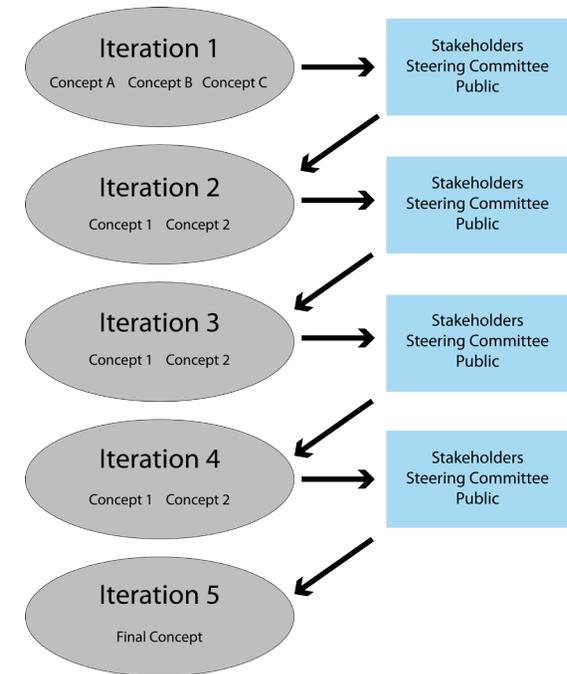
Iteration One: Three Concepts

11. Concept A. This design combines the properties on the south side of 28th Street into a mixed-use town center with new streets and

Key Input Incorporated Into Design Concepts

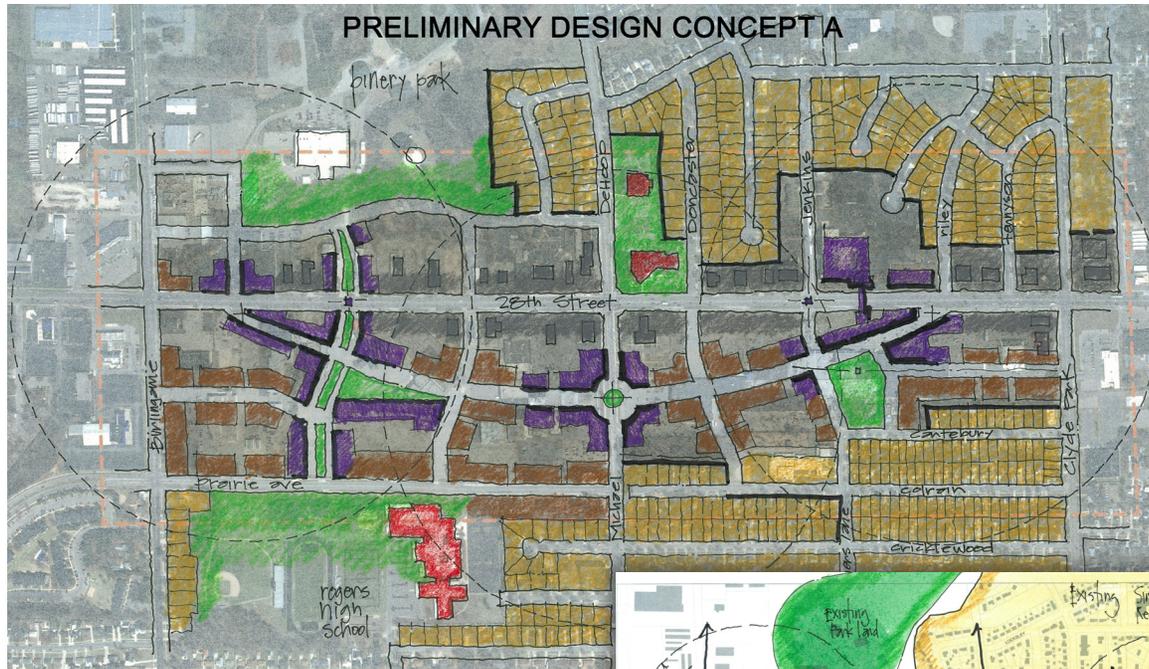
Event	Key Input
Mobile Tour	condense Roger’s Plaza and Wyoming Village Mall, limit commercial access into neighborhoods, maintain sidewalks, design should allow for some parking in front of retail establishments, align driveways for access management purposes, “de-mall the mall”
Stakeholder Interviews	create a flex spaces for outdoor markets, design needs to accommodate traffic, area needs a destination
Ideas Workshops	small blocks promote walkability, mixed use development, more pedestrian amenities, add green space, connect to Pinery Park, increase housing opportunities/housing types
Rogers Plaza Storefront	do not connect commercial to residential area, add more landscaping, wider sidewalks, keep auto-oriented uses north of 28th street, increase height to 2-3 stores for mixed use
Design Workshops	keep park concept in final plan, the crescent street is favored, incorporate round-a-bouts, provide multipurpose entertainment facility, include Hispanic and Asian community
Informal Customer Survey	business suggestions included a big box (Wal-Mart or other), theater, clothing stores, boutique shops, restaurants, kid-friendly businesses, IKEA, hobby shops, etc.
Open House	save the historic Roger’s homestead, maintain dead-end at Colrain and Michael, ensure new streets are bike-friendly
Facebook Page	keep the flea market or at least have area for outdoor market, make sure existing businesses are incorporated, phasing and implementation is key
MDOT Meetings	Decrease the quantity of non-signalized driveways on both sides of 28th Street, channelize any new non-signalized drives at 28th Street, do not align new non-signalized drives with existing streets

The Design Process



compact blocks. A curving slip street is used as the organizing element of the design and represents the new town center’s “main street”. This slip street promotes the movement of slower-paced, localized traffic into the redevelopment area while also maintaining 28th Street as the highway commercial corridor.

The new main street is envisioned as a complete street that promotes transit, bike, pedestrian and automobile use while also providing the framework for a vibrant public realm. The street’s gentle curve allows opportunities for deflected vistas that frame more quaint public



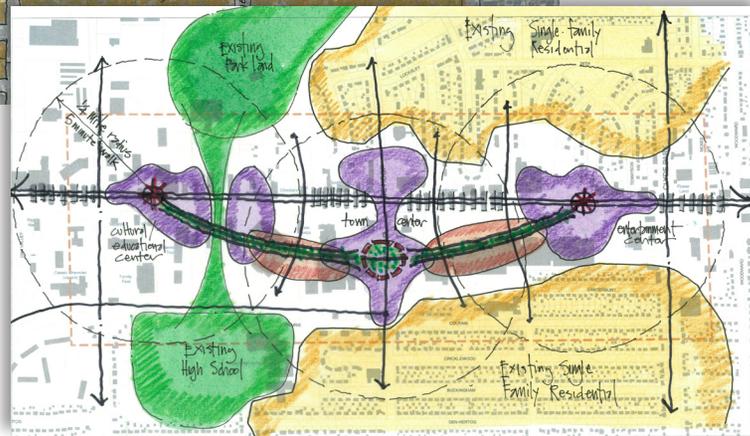
potential customers are coming from the east. The western gateway is anchored by cultural and educational uses because of its proximity to Rogers High School and Pinery Park. Both of these neighborhoods may also incorporate a small amount of ground floor retail as well as office and residential uses.

The urban center of this proposed concept is located just south of city hall at the intersection of the new main street and Michael Avenue. This commercial core is envisioned to have a focal center (indicated as a traffic circle in this design) and the highest and most intense concentration of retail within the town center. This neighborhood may also provide residential and office opportunities.

spaces while also creating interesting and dynamic views of buildings and storefronts.

New north-south streets are proposed to intersect with the main street and to provide connections with the existing nearby neighborhoods. This new street grid also provides the network for a compact and walkable block structure.

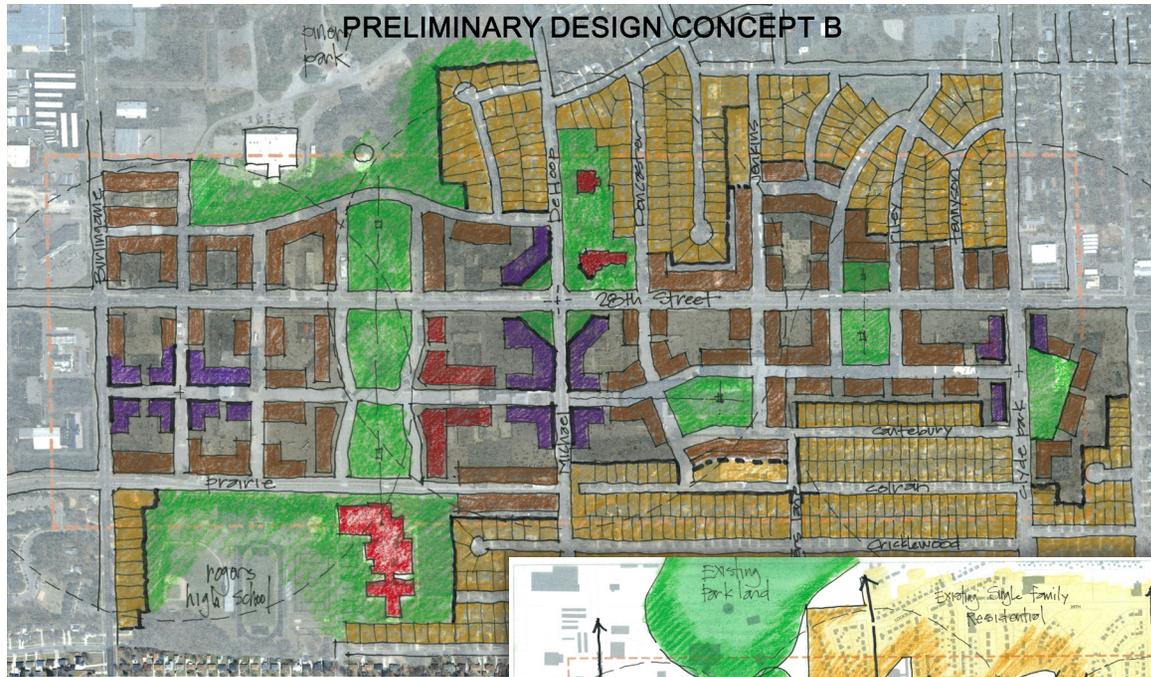
Concept A is divided into three distinct “neighborhoods” that are based on the ¼-mile radius from center to edge. Representing the distance that an average person can walk in 5-minutes, they are indicated as dashed line circles on the preliminary design.



The connective fabric between these three neighborhoods, along the new curving main street, is envisioned to be multi-story buildings that contain primarily residential and office uses on the upper floors and retail opportunities at street level. These buildings are located close to the street edges in order to help to define the public realm.

Two of the proposed “neighborhoods” act as gateways that can provide potential opportunities for an entrance or threshold into the new town center. The eastern gateway is anchored by entertainment uses due of its proximity to the US-131 corridor and the likelihood that more

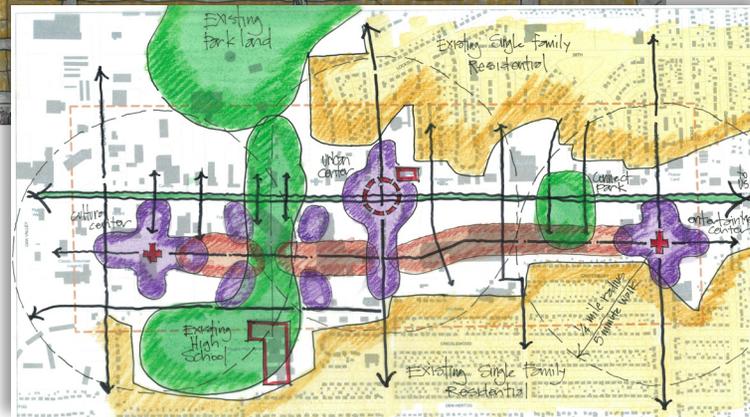
Parking lots are envisioned to be located in the center of the block structure and obscured from the streets by buildings, especially within the town center. Many parking lots will be visible from 28th Street because of the



open design of the blocks that flank this corridor. This openness will provide easy access to parking and visibility of the town center from the 28th Street corridor.

11. Concept B. Concept B also incorporates a new town center just south of 28th Street by assembling the large parcels into a compact block structure interconnected with a new street grid.

Concept B uses a new main street that extends linearly from Burlingame to Clyde Park with a small deflection on the eastern side. This deflection is the result of the narrower depth

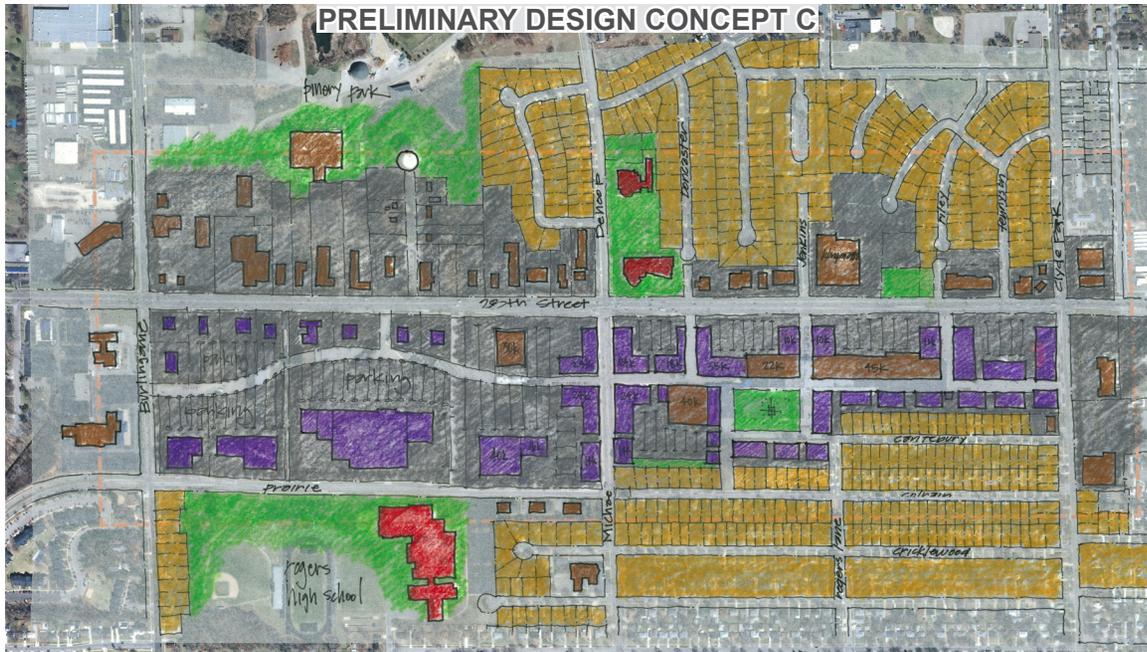


of the eastern property (Rogers Plaza) and the adjacent residential neighborhood to the south (Rogers Lane Neighborhood). Proposed new north-south streets cross this main street, providing direct connections into the new town center.

A large linear park is envisioned on the west side, linking Rogers High School to Pinery Park while also providing potential frontages for a cultural center that could incorporate an educational component to complement the existing high school. The concept of this educational component is the result of both the market analysis and discussions that occurred during the Ideas Workshops.

A smaller park connection is proposed on the east side of the new town center to help to incorporate the existing Klingman's park into the redevelopment area. Additional town greens are scattered throughout the design in order to provide public spaces while also accentuating the city's existing park system. These parks and open spaces provide for a hierarchy of public spaces and are defined by new buildings on all of their edges.

Similar to the ideas of the Concept A design, this design uses three neighborhoods to form the nodes on the new main street. These are outlined with dashed circles and represent the 1/4 mile radius (5-minute walk) that helps to define walkability. The urban core of this concept is at the Michael and 28th Street intersection, extending south to the intersection with the new main street. This urban core acts as the commercial hub for the design and is an attempt to incorporate city hall more directly into the new town center.



After meeting with the Steering Committee and gathering input at the storefront office, Concept A became the favorite approach. The new “crescent” street was uniformly liked. The design offered the benefit of creating smaller blocks while offering creative solutions to access management by reducing curb cuts, creating focal points, and providing strong connections to 28th Street to facilitate commerce and provide greater visibility for end users. The crescent street solution is included in all subsequent design concepts.

The eastern neighborhood node is primarily office use and may include entertainment and some retail functions at the street level. The western neighborhood node is primarily focused on the educational component with some retail, again at the street level. These nodes are connected with residential and office uses along the new main street.

Parking lots are internal to the blocks, with many visible from 28th Street for both access and visual connection to the new main street area. This plan envisions more redevelopment directly along the 28th Street corridor, potentially as higher density residential. The new block structure of the northern blocks along 28th

Street has the end grain of the blocks facing 28th Street in order to limit direct frontages. In this concept, the redevelopment along 28th Street would most likely happen after, and as a result of, redevelopment along the new main street.

11. Concept C. Concept C is a hybrid plan with conventional development continuing on the north side of 28th Street and a mixed-use town center developing on the south. A similar slip street is proposed to shorten the existing super blocks and facilitate walkability. A grid system is proposed, and new retail and mixed use development is centered around a courtyard. The street grid aligns with the existing grid to the north, facilitating access and turning movements along 28th Street.



Iteration 2 - Concept 1

Iteration Two: Two Concepts with Crescent Street

12. Concept 1. Concept one includes the following:

Red buildings are mixed-use with retail at the ground floor. About 660,000 square feet of street level retail is available. This amount is consistent with the market strategy in that half would be new businesses recruited to the area and half would be retained.

Yellow buildings (west side) represent community college, vocational school or office uses. These buildings may be multi-story mixed use with office and/or residential above. There is approximately 180,000 square feet of school/office on the street level.

Iteration 2 - Concept 1 Statistics

Retail	660,000 square feet (total)		
	180,000 sq ft at street level		
Office	420,000 sq ft on second floor		
	600,000 sq ft total		
Residential	400 units on 2nd or 3rd floor		
	200 units for sale (blue buildings on the map)		
	600 units total		
Provided parking	3,800 surface spaces		
	600 on-street spaces		
	4,400 total spaces provided		
Parking demand	Retail	3 per 1,000 sq ft	1,980 spaces required
	Office	3 per 1,000 sq ft	1,800 spaces required
	Residential	1.5 per unit	600 spaces required
Total parking demand			4,380 spaces required



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Blue buildings represent residential uses and include rowhouses, live-work buildings and apartments. There are approximately 200 dwelling units.

KEY FEATURES:

- Crescent Street acts as main street with two lanes of traffic and a center turn lane with parking on both sides. The street also includes 12' to 14' wide sidewalks.
- Redevelopment at edge of Rogers Lane neighborhood includes extension of two streets into the neighborhood, a large park that connects the redevelopment with the neighborhood and residential infill around the park (which requires the loss of some existing homes).

- The concept includes three circular nodes that accentuate key intersections and thresholds of the crescent street.
- The crescent street is extended visually to the west and intersects with 28th Street. This helps to maintain the concept of the crescent visually.
- Archways, integrated into the architecture, will frame the viewshed at both ends of the crescent allow for visual and pedestrian connections.
- Buildings at the Michael-28th Street intersection are oriented to face 28th Street and help to connect city hall to the redevelopment. This pattern could be replicated at the northwest corner of the intersection to complete another activity node at city hall.

- The primary big box site is along Michael, just south of the crescent intersection.

12. Concept 2.

Concept two includes the following:

Red buildings represent retail on the street level (first floor). These buildings are multi-story mixed-use with office and/or residential above. It represents approximately 620,000 square feet of street level retail.

Blue buildings represent office or entertainment uses on the street level. These buildings are multi-story mixed use with office and/or residential above. They represent approximately 175,000 square feet on the street level (including the former Klingmans site).

Purple buildings represent civic uses on the street level. These buildings may be multi-story with offices above the street level. They

Iteration 2 - Concept 2





Iteration 2 - Concept 2 Statistics

Retail	620,000 square feet (total)		
	175,000 sq ft at street level		
Office	400,000 sq ft on second floor		
	575,000 sq ft total		
Residential	400 apartment units on 2nd or 3rd floor		
	200 units for sale (“brown buildings”)		
	600 units total		
Provided parking	2,500 surface spaces		
	600 on-street spaces (primarily on Crescent Street)		
	3,100 total spaces provided		
Parking demand	Retail	3 per 1,000 sq ft	1,860 spaces required
	Office	3 per 1,000 sq ft	1,725 spaces required
	Residential	1.5 per unit	600 spaces required
Total parking demand			4,185 spaces required*

* With mixed-use coefficient of 1.2, which assumes a certain amount of shared parking, parking requirements could be reduced to approximately 3,400 required spaces, which is closer to the number provided.

represent approximately 25,000 square feet on the street level. The buildings across from city hall potentially provide new locations for the post office and Secretary of State office that are presently located in Rogers Plaza.

Brown buildings represent residential uses and include rowhouses, live-work buildings, apartments and single-family homes. They represent approximately 200 dwelling units on the street level.

KEY FEATURES:

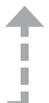
- The crescent street acts as main street with two lanes of traffic, an

alternating landscaped median or turn lane at the center and parking on both sides. The street also includes 12’ sidewalks.

- Redevelopment at the edge of Rogers Lane neighborhood includes an additional of single-family homes, a lack of through streets, park connections and there is no loss of existing homes.
- The concept includes strategically-placed north-south connections.
- A plaza opposite 28th Street from

the former Klingmans store visually links that property to the activity of the redevelopment.

- The large park across from City Hall attempts to visually link City Hall to the activity of the redevelopment, while also providing a physical greenway connection.
- A north-south boulevard on the west side connects Rogers High School to the retail core and small central park.
- The southern edge of the redevelopment area, along Prairie, is a naturalized edge with informal landscaping.
- The northern edge, along 28th Street, is a more formalized edge with regularly-spaced deciduous street trees, a sidewalk and wide parkway buffer between redevelopment and 28th Street.
- Parking lots allow for visibility of businesses from 28th Street and the big box site is conventionally located relative to 28th Street with a large parking lot in between the store and the corridor. The big box has a pedestrian scaled corner that interfaces with the small elliptical park.





Iteration Three: Refinement of Crescent Street Concepts/ Market Strategy

13. Concept 1: Concept One, below, is a further refinement of the crescent street concept with 660,000 square feet of office uses above retail uses located along the east and west flanks of the study area. Park land and green spaces are prevalent along corridors with a larger park area across from Klingmans to help bolster the existing Klingmans park on the north side of 28th Street. Additionally, a predominant viewshed is located west of Michael and creates an entry to an educational campus. A sports complex (pink building) south of the educational campus with parking and residential uses fronts Burlingame and Prairie. Concept One includes big box retailers within the confines of the Loeks' property as well as Wyoming Village Mall. Liner

13. Concept 1



13. Concept 2

buildings complete the street edge and hide the parking lots along the crescent street.

Additionally, a transit exchange station is incorporated into this iteration based on recommendations from the planners at The Rapid.

13. Concept 2: Concept Two, above, shows the big box retailer located at the intersection

of 28th Street and Michael with a medium-box retailer south of the crescent street along Michael. Mixed-use retail buildings line the crescent street, providing retail at the street level with parking located behind buildings. Office uses are located along Clyde Park (east side of study area). Along Burlingame (west side of study area) are attached residential uses. A sports complex (green building) anchors the western end of the redevelopment.



Concept Two also includes two roundabouts providing focal points at the terminus of the crescent street and helping to align the crescent street with 28th Street.

Iteration Five: Final Concept Incorporating Additional Retail

15. Concept 1: After reviewing the two concepts in Iteration Four, the Steering Committee, together with the design and economic development team, selected 14: Concept One. The fifth iteration includes a slight modification to the office and retail mix at Clyde Park and 28th Street. In this concept, retail uses are proposed, creating a formal retail corridor consistent with the existing retail along the north side of 28th Street. There is 560,000 square feet of ground floor retail proposed.

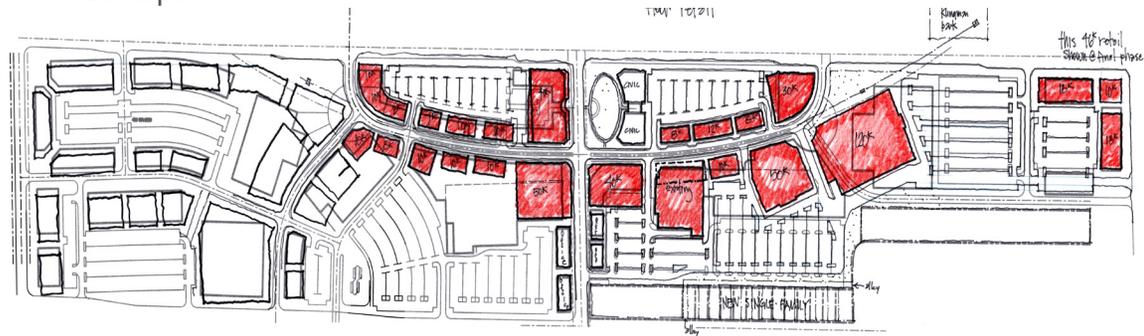
This concept also begins to refine the odd-shaped large-format retailers from the previous concepts into more realistic rectangular shapes.

Access to 28th Street is limited to the crescent street and two driveways (one of the west side, one on the east side). Secondary access from the crescent street includes streets that connect Burlingame to Clyde Park.

The eastern street, to Clyde Park, connects to the crescent street at the large format retailer and provides better traffic flow patterns for the redevelopment of the study area.

The eastern connection to 28th Street is at the existing Jenkins traffic signal. The western connection of the crescent to 28th Street, while

15. Concept 1



currently not signalized, may warrant a future signal.

15: Concept One becomes the final plan proposed for the suburban retrofit of the Study Area. Chapter 5, Recommendations, includes the final design plan, a full description of the plan and suggested phasing for redevelopment.



Introduction

The illustrative master plan described on the following pages reflects the final plan for the suburban retrofit of a portion of the 28th Street corridor. It is based upon extensive input from the public throughout the planning process, as well as input from the steering committee and MDOT.

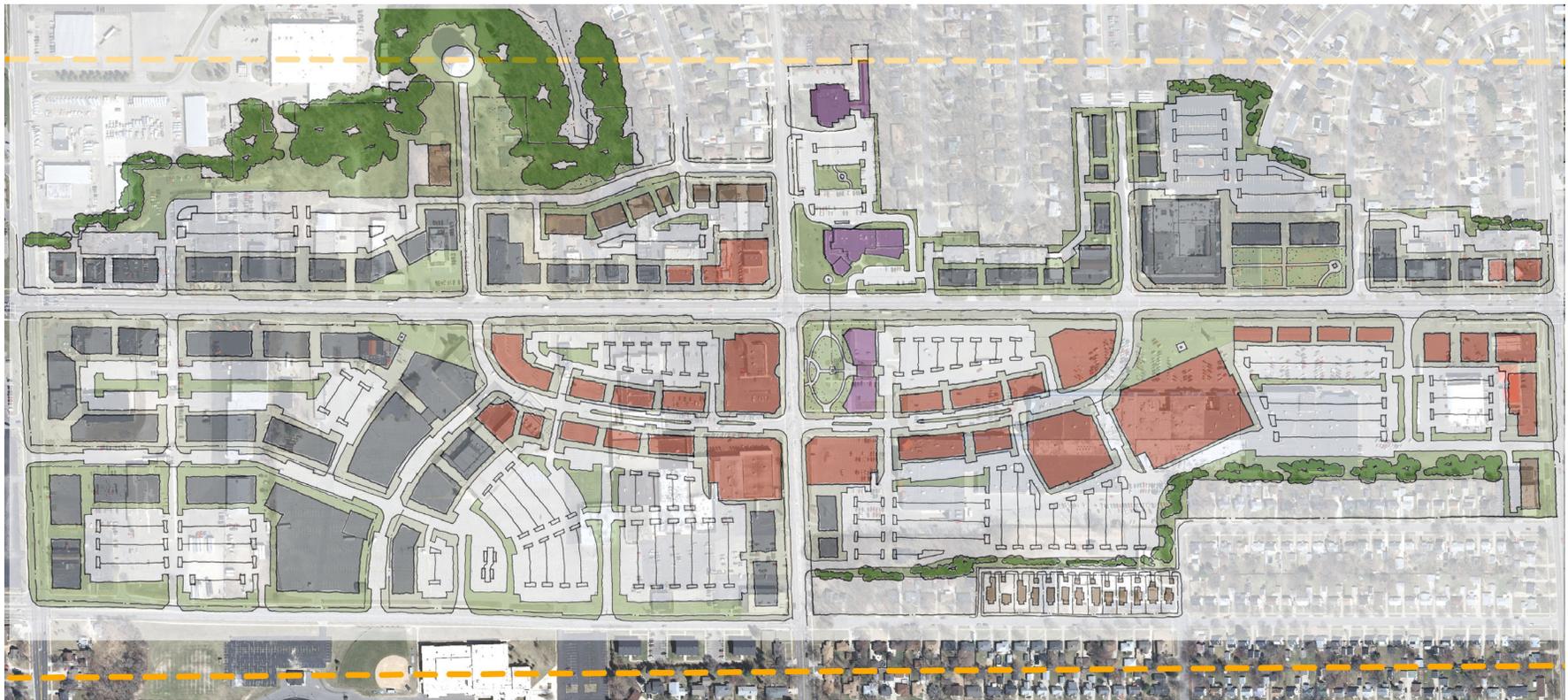
The Final Plan

The final plan depicts the vision for the eventual redevelopment and transformation of the 28th Street corridor over the next 30 years through a gradual retrofit that occurs in phases over time, depending upon economic factors, market demand and access to available funding sources for public improvements such as streets, landscaping, and other civic amenities.

The layout and orientation of buildings, streets, open space, landscaping and parking is intended

to be purely illustrative of how various parcels could be redeveloped, revitalized or retrofitted under the recommendations of this plan. Deviations from the illustrative plan are anticipated and expected during the course of market-driven redevelopment.

An example of one significant, privately influenced deviation is in regard to the illustrated location of the large format retailer on the east end of the crescent street. This location could “flip” to the west end of the crescent street without





substantially compromising the overall vision of the final plan, provided that the elements of the crescent street remain intact. This deviation will most likely depend on the sequence of development that occurs on the existing parcels and the real estate market.

The final plan does not recommend the immediate removal or renovation of any existing land use, building, or business within the study area. The phasing illustrations that are associated with the final plan intend to demonstrate that gradual redevelopment can occur while a majority of the existing buildings and businesses remain in place. The incorporation of existing uses and businesses into redevelopment projects and revitalization efforts may be encouraged in an effort to retain existing commercial enterprises.

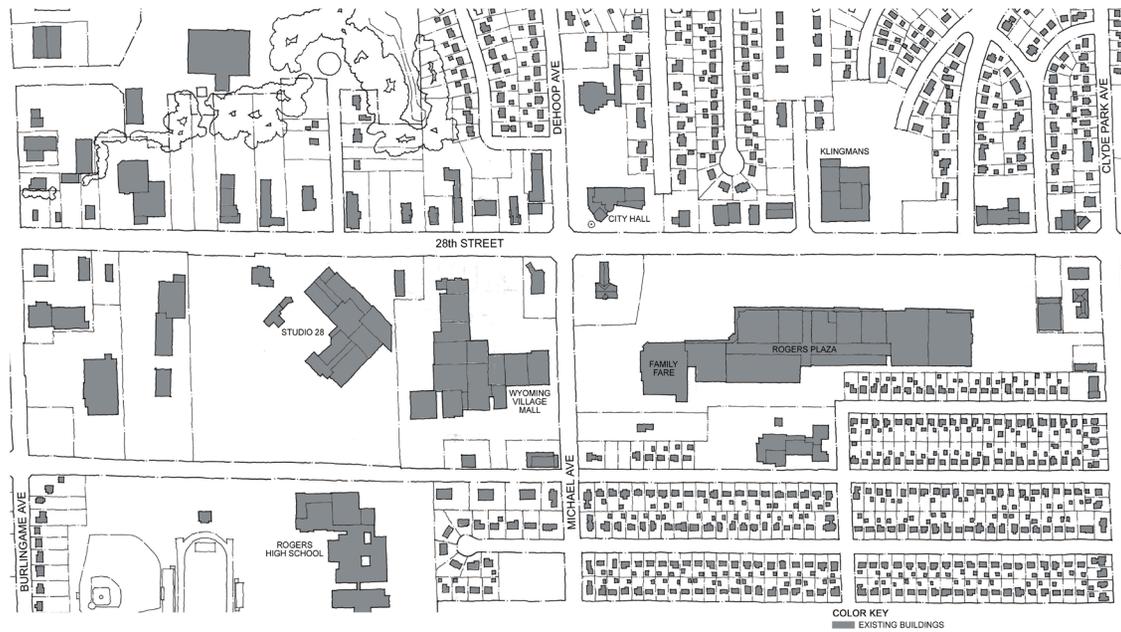
The Turn On 28th Street planning effort assumed that almost every parcel and land use within the study area (with the exception of existing civic destinations) would redevelop in some way over the next 30 years. The majority of existing buildings along the corridor are over 20 years old and largely nearing the end of their serviceable life. Therefore, even without intervention through this sub-area plan, the normal actions of the market may result in the reconstruction of a majority of the study area over time. As this process naturally occurs, the sub-area plan provides the opportunity to define the form of the future community.

Illustrative plans portray retrofitting and redevelopment in a series of colors, so that progress can be diagrammatically demonstrated. The light grey color represents existing buildings. Yellow represents buildings that are removed in the specific phase indicated and orange represents new buildings that are added as part of the specific phase. Dark grey represents new buildings that were added in a preceding phase - these buildings will show the cumulative effect of the possible redevelopment sequences. Finally, the grey hatch pattern illustrates new streets that are added as part of the specific phase.

Existing Conditions Figure Ground Illustration

Graphic 5.1 depicts the layout and orientation of existing buildings and streets and provides the base reference point for the transformation studies that follow. It also indicates approximate location of existing property lines. The development pattern is informed by these property lines and they help to provide a framework for the illustrative phasing.

Figure 5.1 Existing Figure Ground Analysis





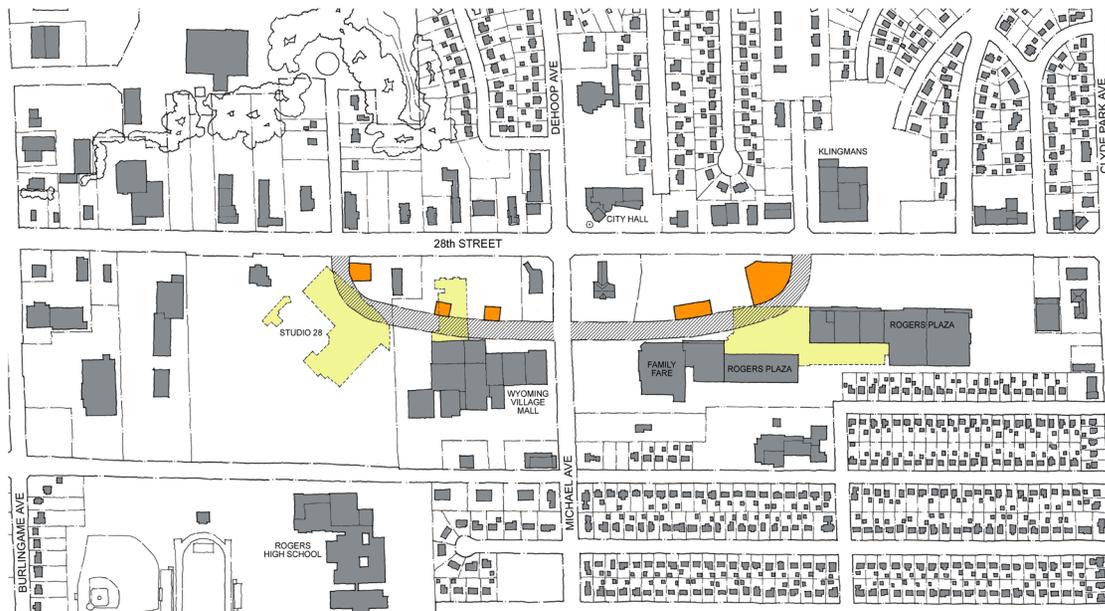
Transformation Study 1 (Years 1-4) The initial illustrative phase depicts a possible first step in the retrofit process and includes the following key efforts:

- The crescent street is added, extended from the east and west of Michael Avenue to 28th Street. This new street is envisioned as a tree-lined boulevard with an intermittent center turn lane, wide sidewalks, bike lanes and on-street parking. Refer to the street section diagram that illustrates the crescent street.

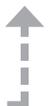
- The location of the crescent street allows a majority of the existing buildings to remain in place, although the Studio 28 building (currently vacant), a minor northern extension of the Wyoming Village Mall and a portion of the Rogers Plaza Mall, including the concourse, are removed to accommodate the new street pattern. These removals may provide the existing malls with short-term opportunities to adjust and refine their retail model to meet current demands through selective remodeling.

- New buildings (in orange) may start to infill along the new crescent street. To proactively urbanize the new street, these buildings should orient their frontages along the crescent street. Frontages should include retail storefronts that provide transparency while promoting activity along the street. The rear of the buildings may provide a secondary entrance associated with the parking lots and also signage that is scaled to the auto-oriented arterial. These buildings should be multiple-story mixed-use buildings. Although in the early years of the redevelopment, upper floors may remain unfinished until the market materializes.

Figure 5.2 Transformation Study I



- This phase may also be executed in two steps, one with the crescent street implemented to the west of Michael Avenue and one with it implemented to the east. The phased crescent street could function as either an east or west singular element until the second phase is constructed.





Transformation Study 2 (Years 5-7)

The second illustrative phase depicts the following key efforts:

- A western connector street is added between the crescent street and Prairie Parkway with a future connection to Burlingame Avenue started and temporarily terminated at the Studio 28 property line.
- An eastern connector street is added between the crescent and Clyde Park Avenue.
- The western connector provides an opportunity for a larger format building footprint (indicated at approximately 90,000 square feet) that is envisioned to be a sports complex, academic institution or office building in this scenario. This building should be a multiple-story mixed-use building with street frontages that provide transparency while promoting activity along the street.
- The eastern connector provides an opportunity for a large format retailer (indicated at approximately 120,000 square feet) along with its associated parking field. The big box is oriented to provide a pedestrian-scaled entry at the crescent street (curved corner) and an

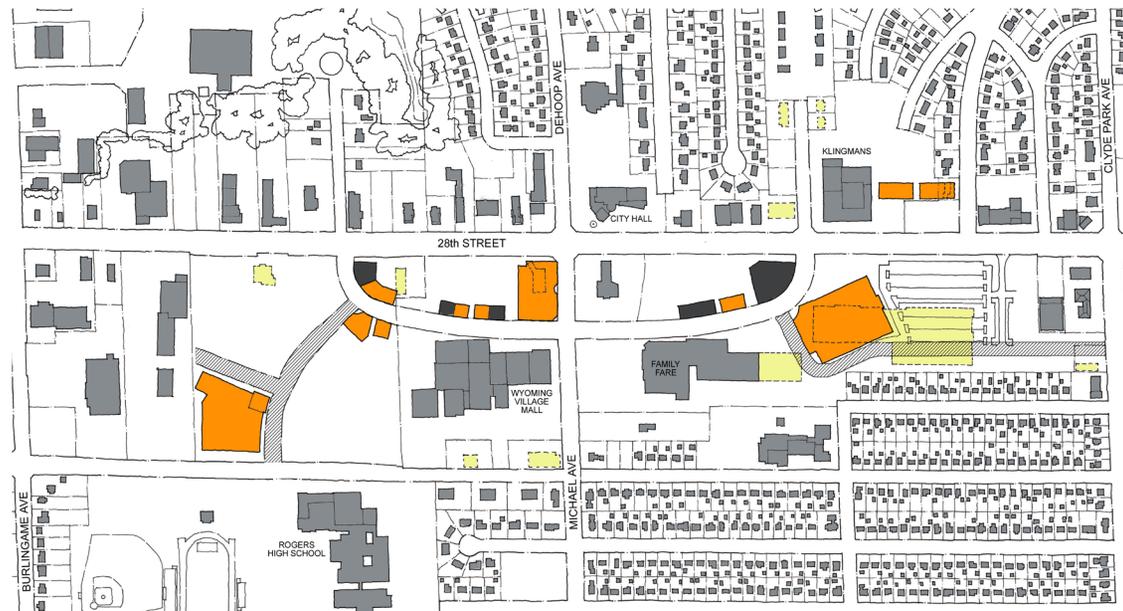
auto-scaled entry at the parking lot (along its east facing wall).

- The proposed big box retail building is also angled relative to 28th Street to provide a defined public space at the end of the crescent. This public space is intended to provide a visual and practical link to the existing Klingman's Park on the north side of 28th Street. Additionally, the big box building should have fully articulated and pedestrian scaled architectural details on the two sides that are visible from the

crescent street. This articulation should include a large amount of transparency and storefronts as well as vertical elements that divide the long horizontal façades into pedestrian-scaled sections.

- The placement of the big box allows for preferred visibility from the 28th Street corridor and a closer proximity to traffic coming from US-131 than other parcels within the sub-area. The adjacent parking lot has a direct connection from 28th Street, the crescent street and Clyde Park Avenue and provides conventional suburban access to the building.

Figure 5.3 Transformation Study 2





- The existing bank building on the southwest corner of Michael and 28th Street is replaced by a mid-sized regional or national retailer (indicated at approximately 50,000 square feet). This building should be multiple stories (two to three floors) to accentuate the intersection and this future node. Upper floors may incorporate residential or office uses and may be unfinished initially. Frontages should include retail storefronts that provide transparency while promoting activity along the street. The rear of the buildings may provide a secondary entrance associated with the parking lots and also signage that is scaled to the auto-oriented arterial.
- New buildings continue to infill along the crescent street in a form that is consistent with the descriptions indicated in Transformation Study 1.
- Two new office buildings are depicted adjacent to the Klingman's building and begin to form a defined edge for the existing Klingmans Park. These buildings should be multiple-story mixed-use buildings with street frontages that are transparent and promote activity along the street.

- The existing Klingman's building may also begin to be retrofitted as part of this phase. It is envisioned to be an office facility as part of this retrofit scenario.
- Existing buildings at key locations are removed (as indicated in yellow) to prepare for the next sequence of redevelopment. A significant portion of Rogers Plaza is removed to accommodate the large-format retailer.

Transformation Study 3 (Years 8-12)

The third illustrative phase depicts the following key efforts:

- Western redevelopment continues at the crescent / 28th Street / connector intersections with a series of larger footprint buildings that are envisioned to be academic institutions or offices. These buildings should be multiple-story mixed-use buildings with street frontages that are transparent and promote activity along the street.

Figure 5.4 Transformation Study 3





TURN ON 28TH STREET CORRIDOR SUB-AREA PLAN

- A portion of the Wyoming Village Mall is removed to accommodate a new mid-sized regional or national retailer (indicated at approximately 40,000 square feet). This building should be a multiple-story mixed-use building with street frontages that are transparent and promote activity along the street. Upper floors may incorporate residential or office uses.
- The existing bank building on the south-east corner of Michael and 28th Street is replaced by a focal point building that incorporates a public open space at the intersection. This building is envisioned to be either a civic building for the relocated Secretary of State's office and Post office or a retail building that may have a restaurant use. This building should be a multiple-story mixed-use building with street frontages that are transparent and promote activity along the street. Upper floors may incorporate residential or office uses and by this phase, the market for upper floor uses should be more apparent.
- The open space at this intersection is envisioned to be a hard-scaped plaza or a landscaped park and to have active spaces that could

include a seasonal ice-skating rink. Additionally the space is envisioned to connect the existing city hall to the crescent street, while also extending the existing city hall open space across 28th Street.



The existing bank building at Michael and 28th Street

- The final part of Rogers Plaza is removed to accommodate an additional mid-sized regional or national retailer (indicated at approximately 40,000 square feet). This building should be encouraged to be multiple stories with the upper floors incorporating residential or office uses. Additionally, this building should have fully articulated and pedestrian scaled architectural details on the side facing the crescent street. This articulation should include a large amount of transparency and storefronts as well as vertical elements that divide the long horizontal façades into pedestrian-scaled sections.
- The eastern Family Fare grocery that was connected to Rogers Plaza remains.
- The western Family Fare is depicted as removed, along with the Dermody Truck Sales facility. This will accommodate the final extension of the west connector to Burlingame while also promoting the future redevelopment of this portion of the study area.
- The buildings at the southwest corner of Clyde Park and 28th Street are removed and new retail buildings are constructed. These buildings should be multiple-story mixed-use buildings with street frontages that are transparent and promote activity along the street.
- Infill buildings are now indicated adjacent to Klingman's. These are envisioned to be primarily offices and should be multiple-story mixed-use buildings with street frontages that are transparent and promote activity along the street.
- Existing buildings continue to be removed (as indicated in yellow) to prepare for the next sequence of redevelopment.



- The Rogers homestead (large parcel on east side of Michael Avenue, just behind Family Fare) is indicated as removed in this phase. Some public input during the design portion of the project indicated a desire to preserve this building in either its current location or at a new location. The City of Wyoming Historical Commission has not done a study on the historical need to retain this property, nor whether or not the home is in a condition which would allow it to be relocated. It is the recommendation of this plan that the homestead be relocated, if feasible, in order to not impede the retrofit and to preserve any historic resource that may result from further studies.
- New buildings continue to infill along the crescent street in a form that is consistent with the descriptions indicated in Transformation Study 1.

Transformation Study 4 (Year 13-18)

The fourth illustrative phase depicts the following key efforts:

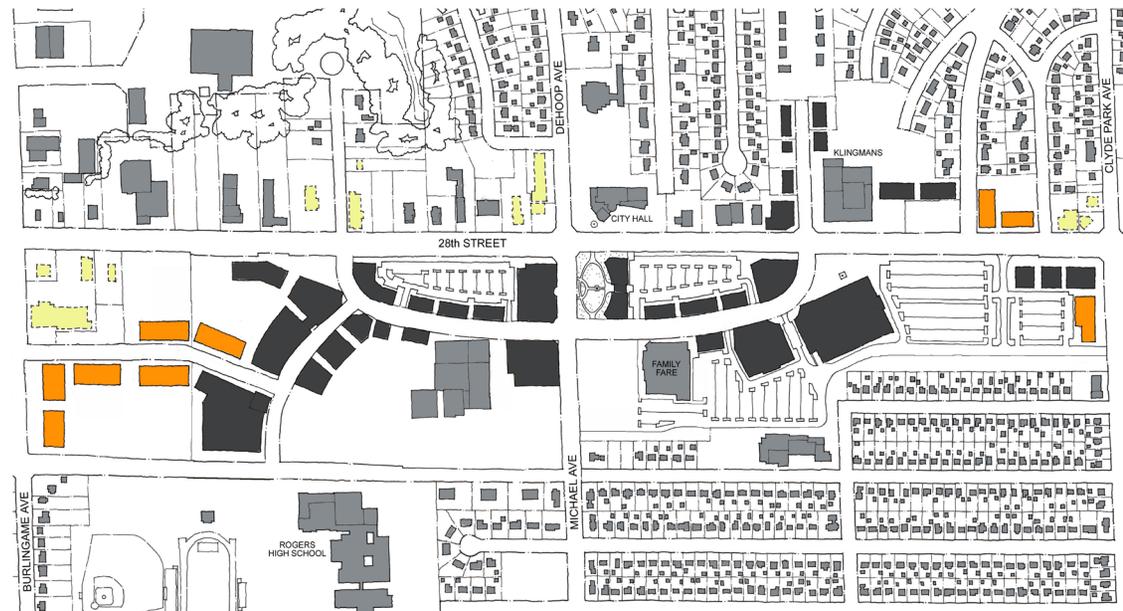
- A majority of the retrofit is now complete along the crescent street, with the existing Family Fare and part of the Wyoming Village Mall still present.

- The existing Rogers Lane School (not currently used as an elementary school) is still present at this stage, although its outbuildings have been depicted as removed in order to facilitate the infrastructure for the redevelopment along the crescent street.
- New buildings are infilled along the western connector. These buildings are envisioned as offices and/or support buildings for academic institutions. These buildings should

be multiple-story mixed-use buildings with street frontages that are transparent and promote activity along the street.

- The retail at the corner of 28th Street and Clyde Park Avenue is added to complement the previously installed retail at this intersection. These buildings should be multiple-story mixed-use buildings with street frontages that are transparent and promote activity along the street.

Figure 5.5 Transformation Study 4





- Two new office buildings are depicted at the Action Tire Center site east of Klingman’s Park. These buildings complete the urban framework around the existing park, providing it with a defined edge. They also complement the previously installed buildings in this area of the retrofit. These buildings should be multiple-story mixed-use buildings with street frontages that are transparent and promote activity along the street.
- Existing buildings continue to be removed (as indicated in yellow) to prepare for the next sequence of redevelopment.

- The last remaining existing building, the Family Fare, is still in place in this illustrative phase.
- Three new office buildings are depicted at the northwest corner of Clyde Park Avenue and 28th Street. These buildings complement the previously installed buildings in this area of the retrofit. These buildings should be multiple-story mixed-use buildings with street frontages that are transparent and promote activity along the street. As part of this retrofit, the partial Tennyson

Street connection at 28th Street is removed, although it potentially could be utilized as a drive for the rear parking lots. This southern portion of Tennyson Street does not connect to the northern portion of Tennyson at this location and is not proposed to do so as part of this sub-area plan.

- A new building is depicted at the northwest corner of DeHoop Avenue and 28th Street. This building (indicated as approximately 20,000 square feet) should be a multiple-story mixed-use building with street

Figure 5.6 Transformation Study 5

Transformation Study 5 (Years 19-21).

The fifth illustrative phase depicts the following key efforts:

- The remaining portion of Wyoming Village Mall is indicated as removed to facilitate redevelopment of the site in subsequent phases.
- Rogers Lane Elementary School (not currently used as a school) is also indicated as removed to facilitate redevelopment. While the illustration depicts this building as removed, the vision is not detrimentally impacted if the building were to remain in place and be adaptively re-used.





frontages that are transparent and promote activity along the street.

- The new building to the north of the above mentioned mixed-use building is envisioned to be a row-house building that introduces a distinct and non-represented residential type to the study area, and to the city in general. The building front should face the existing street and the garage access should be from the rear either via an alley or through a shared parking lot. The rowhouse building allows for a transition from the mixed-use elements of the retrofit to the single-family elements of the existing adjacent neighborhoods.
- New buildings are depicted at the Hook and 28th Street intersection, to begin to make the transition from the south to the north side of the arterial. These buildings are envisioned primarily as office uses and should be multiple-story mixed-use buildings with street frontages that are transparent and promote activity along the street. The building to the west of the intersection is set back from the street in order to provide a complementary civic space to the civic space previously installed on the south side of 28th

Street. This civic space provides the western node of the crescent street while promoting a connection to Pinery Park (to the north of this intersection).



Mixed use buildings in Mashpee Commons, MA, one of the precedent studies explored during the design phase.

- New buildings are shown at the south east corner of Burlingame Avenue and 28th Street. These buildings are envisioned as offices and/or academic institutions. These buildings should be multiple-story mixed-use buildings with street frontages that are transparent and promote activity along the street.
- The new buildings along Burlingame are depicted as set back (not built to the right-of-way line). This is to enhance the existing street

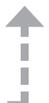
with landscaping opportunities and is consistent with the envisioned treatment of 28th Street as a parkway, with green buffers, street trees and a more naturalistic edge.

- The last of the proposed new streets is added connecting DeHoop Avenue with Hook Avenue. This will allow for alternative circulation and provide opportunities to promote the removal of driveways along 28th Street. This street will provide the southern edge to the Pinery Park access.
- Existing buildings continue to be removed (as indicated in yellow) to prepare for the next sequence of redevelopment. This removal also includes the homes located along Hook Avenue in order to extend the access to Pinery Park.

Transformation Study 6 (Years 22-25)

The sixth illustrative phase depicts the following key efforts:

- The existing Klingman’s building is lined with new infill that increases its street presence and accentuates the enclosure of Klingman’s Park. To proactively urbanize the existing intersection, these liner building should orient their frontages toward the intersection. The frontages



should include transparency and encourage activity along the street.

- Buildings are depicted to continue the western expansion of the retrofit from the DeHoop Avenue and 28th Street intersection. The buildings facing 28th Street are envisioned to gradually transition to office uses and it is likely that the only retail frontage at this location is the previously installed corner building. These buildings should be multiple-story mixed-use buildings with street frontages that are transparent and promote activity along the street.
- The buildings that front on the newly constructed connector between DeHoop and Hook Avenues are envisioned to be rowhouses, consistent with the description indicated in Transformation Study 5.
- A proposed transit transfer station is depicted near Prairie Parkway as part of an integration of this planning process and the ongoing planning process of The Rapid. This transit station may also provide student access to an academic institution that is envisioned on the west side of the study area.

Figure 5.7 Transformation Study 6



- The final building along the southern connector is depicted adjacent to the transit station. This building, in following the programming of previously installed adjacent buildings is envisioned as office and/or an academic institution support building. This building should be a multiple-story mixed-use building with street frontages that are transparent and promote activity along the street.
- The final two buildings are installed along the western leg of the crescent street in a form that is consistent with the descriptions indicated in Transformation Study 1.
- New single-family residential is indicated along Colrain Street in the previous site of the school. This is intended to continue the residential fabric of the Rogers Lane Neighborhood and may also be in the form of rowhouses. This depiction also indicates an alley way for



automobile access to the rear of these residences, with the intent that no other connections occur from the existing neighborhood to the redevelopment. This is an effort to avoid traffic in the neighborhood, which was a high priority of many of the stakeholders who participated in the public input sessions.

- A new building along Clyde Park Avenue is depicted to complete the redevelopment of the eastern edge of the study area in the form of a rowhouse. The building front should face the existing street and the garage access should be encouraged to be from the rear either via an alley or through a shared parking lot. The rowhouse building allows for a transition from the mixed-use elements of the retrofit to the single-family elements of the existing adjacent neighborhoods.
- Existing buildings continue to be removed (as indicated in yellow) to prepare for the next sequence of redevelopment.

Transformation Study 7 (Years 26-28)

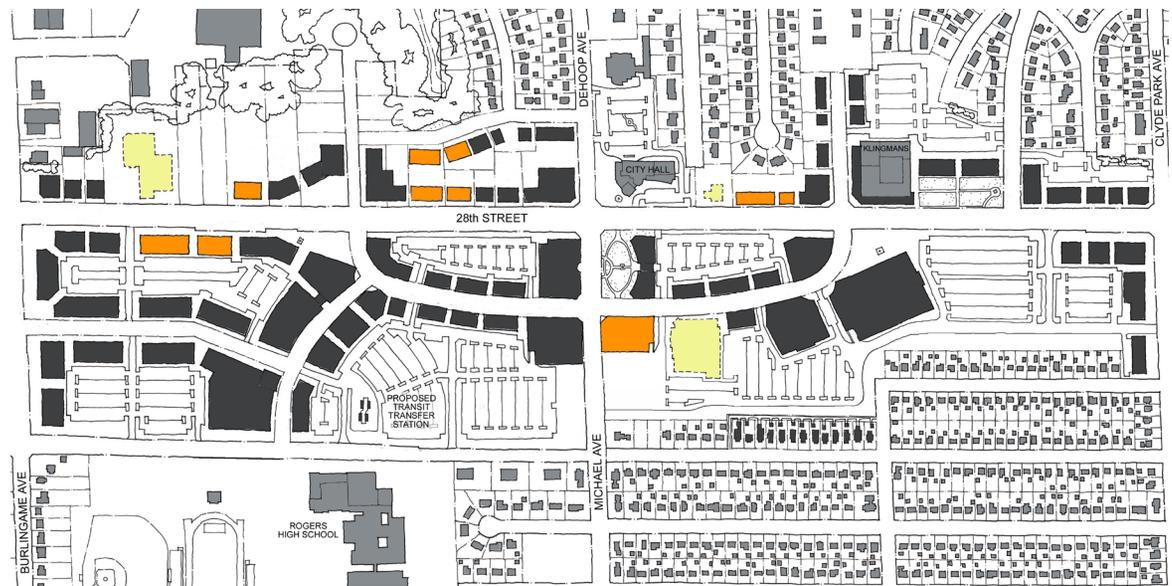
The seventh illustrative phase depicts the following key efforts:

- A new (approximately 30,000 square foot) building is depicted at the corner of the crescent street and Michael Avenue. This building is envisioned to provide a new retail space for the Family Fare, which is indicated as removed in this illustration. The new location will provide the grocery store with visibility and place it as an anchor at the intersection and to the public plaza to the north. This should be a multiple-story mixed-use building with street

frontages that are transparent and promote activity along the street.

- New buildings continue to fill out the block structure. These buildings are envisioned to be consistent with the previously described adjacent buildings. These buildings should be multiple-story mixed-use buildings with street frontages that are transparent and promote activity along the street.
- Existing buildings at remaining locations continue to be removed (as indicated in yellow) to prepare for the next sequence of redevelopment.

Figure 5.8 Transformation Study 7





Transformation Study 8 9Years 29-30)

The eighth illustrative phase depicts the following key efforts:

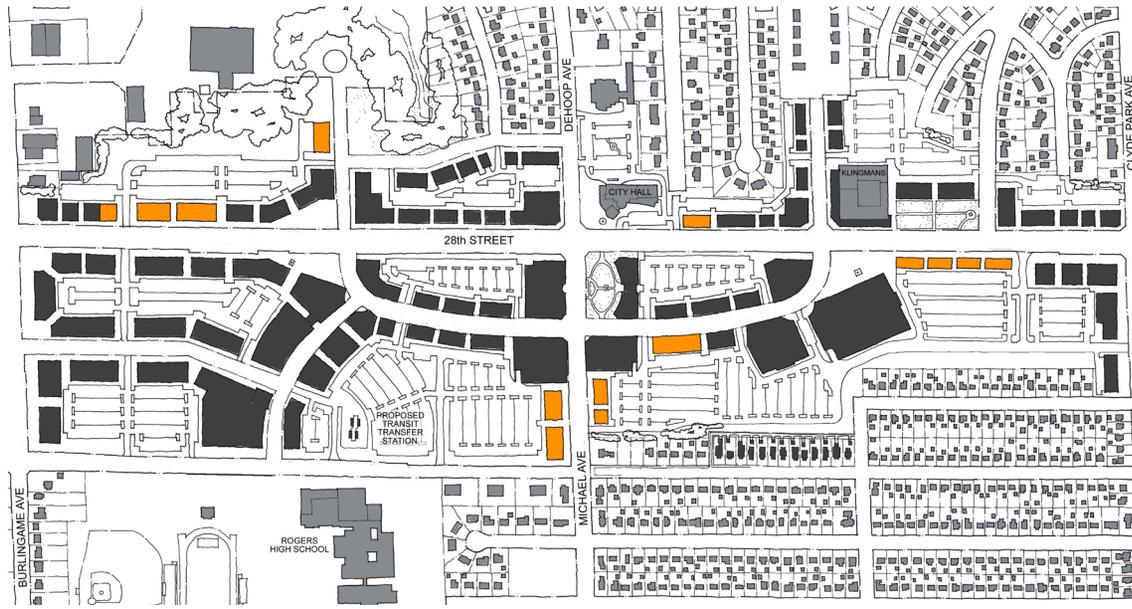
- The final building is installed along the eastern leg of the crescent street in a form that is consistent with the descriptions indicated in Transformation Study 1.
- Office buildings are depicted along Michael Avenue to obscure the parking lots. These buildings should be multiple-story mixed-use

buildings with street frontages that are transparent and promote activity along the street.

- New buildings continue to fill out the block structure. These buildings are envisioned to be consistent with the previously described adjacent buildings. These buildings should be multiple-story mixed-use buildings with street frontages that are transparent and promote activity along the street.

- New liner retail buildings are depicted along 28th Street near the large format retailer. These are envisioned as one of the last phases of the retrofit in an effort to provide a formalized southern edge for Klingman's Park and to complete the block structure of the eastern side of the sub-area.

Figure 5.9 Transformation Study 8





Final Plan: Figure Ground Illustration

Figure 5.10 depicts the final plan layout and completion of the envisioned suburban retrofit. All buildings indicated in dark grey are the result of the retrofit and redevelopment that is anticipated to take place over a 30-year period. Light grey buildings are existing buildings that remain in place.

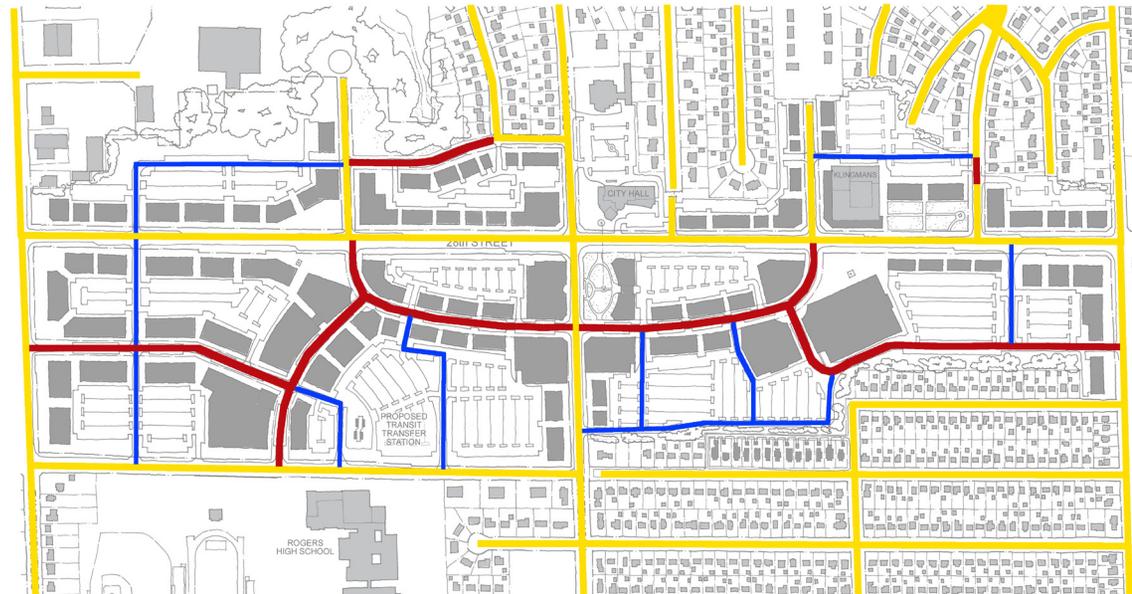


Figure 5.10 Final Figure Ground Illustration

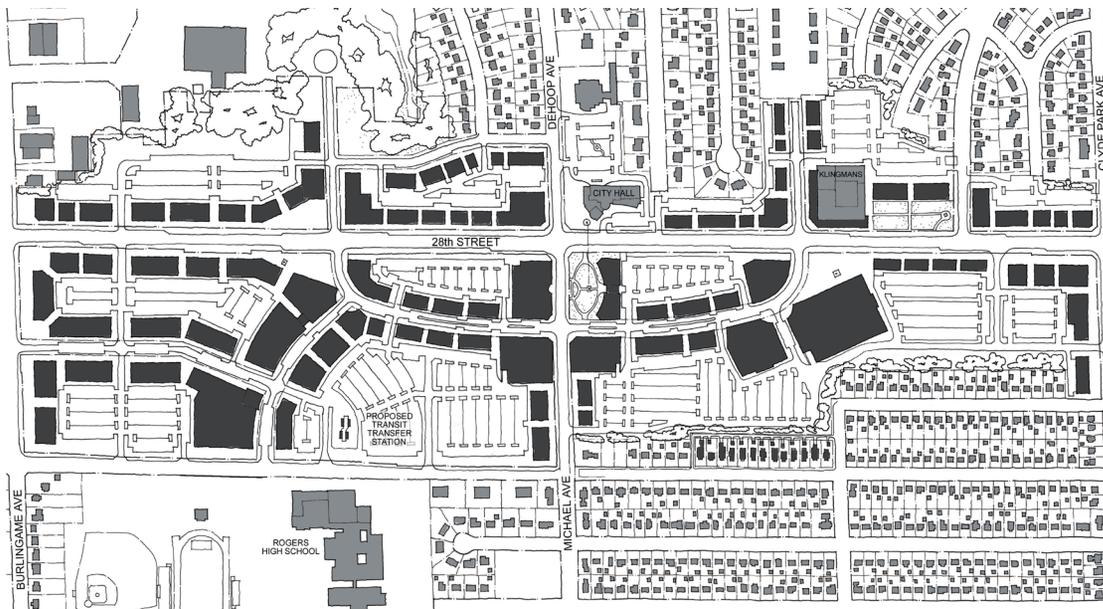


Figure 5.11 Connection Plan

Final Plan: Final Connection Plan

The connection plan shows existing streets in yellow, proposed new streets in red and driveways/secondary connections in blue.

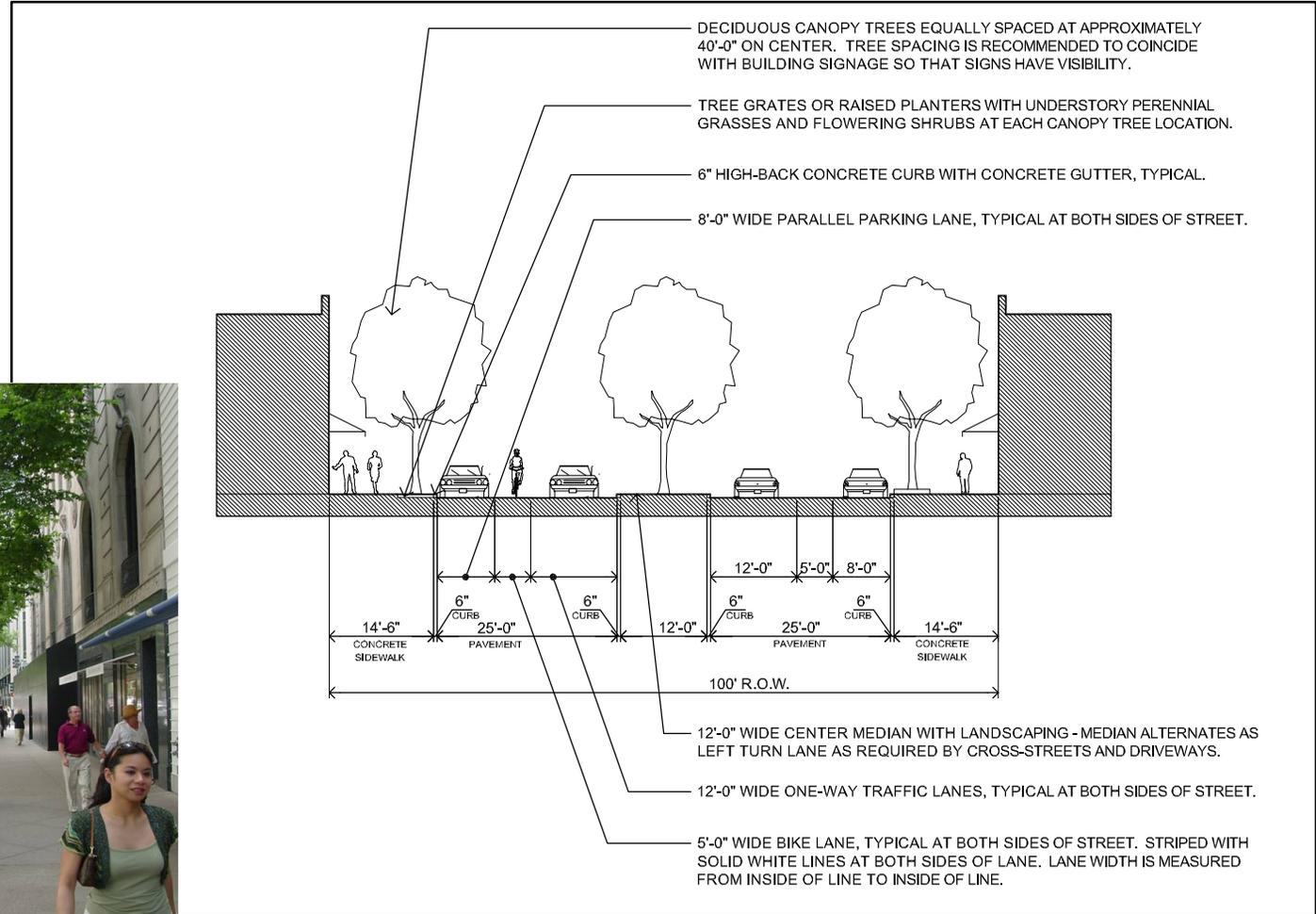
The following page includes a proposed cross section for the crescent street. It is a two lane cross section with on-street parking. Five foot wide bike lanes are proposed for each direction of travel. A center median provides a planting area. The fourteen foot sidewalk accommodates pedestrians, street furniture, and outdoor cafe seating.





TURN ON 28TH STREET CORRIDOR SUB-AREA PLAN

DIAGRAM CROSS SECTION OF CRESCENT STREET



An example of a sidewalk system with space allocated for street furnishings and plantings with buildings built to the edge of the pavement.



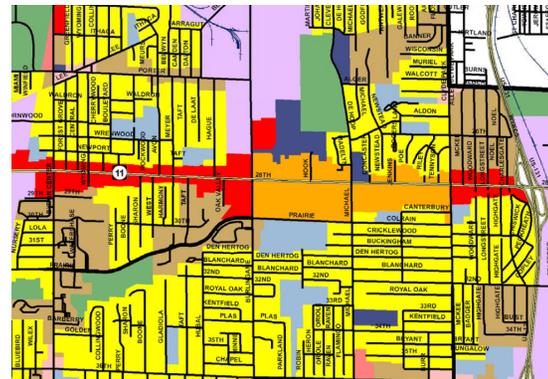
Plan Implementation

Empty properties along 28th Street pose a major challenge to attracting new investment. It can be cost-prohibitive to demolish obsolete structures and prepare a site for new development, especially compared to greenfield sites in newer suburban areas. Nevertheless, these sites along 28th Street must be viewed as opportunities. 28th Street is a popular, heavily traveled roadway and the study area is within a core part of the Grand Rapids metropolitan region, surrounded by established, well-populated neighborhoods and existing infrastructure.

One key to a successful rebirth of 28th Street will be the availability of funding. Successful brownfield projects often result from a union of federal, state, and local financing mechanisms. One important part of the puzzle to securing funding is the combination of willing private partners and a viable plan for reuse. While redevelopment of the 28th Street study area may take time and occur on a site-by-site basis, as private sector partners emerge and as funding becomes available, it is essential that redevelopment generally follows the overall vision presented in this Plan.



Vacant buildings pose a challenge to attracting new investment



Much of the redevelopment area is designated as Downtown Center in the 2006 Master Plan

It is anticipated and preferred that redevelopment of the study area be driven by private-sector development and investment, but public-private partnerships are also anticipated. The City of Wyoming and/or the City's DDA may need to take the first steps in implementing the first phase of redevelopment, as outlined in Chapter 5. This could take the form of providing financial incentives, purchasing property for redevelopment, working to attract new development or other actions. Regardless, the City and the DDA will need to play a proactive role in realizing the ambitious vision presented in this Plan.

The following tasks are intended to be undertaken by the City of Wyoming and DDA and should be seen as steps necessary to encourage and facilitate new investment that follows the vision presented in this plan.

Key First Steps to Implementation

While much of the elements proposed by this plan will develop as economic conditions warrant, there remain steps that can be taken in the interim to get redevelopment "off the ground." For example:

- Acquire easements for new roads in redevelopment area. The alignment of future roads will define the entire effort. Easements will confirm the desired pattern and, conversely, prevent the emergence of a development that could thwart this plan's vision. The process of acquiring easements for the placement of new roadways can be difficult and time consuming. Following the Phasing Plan outlined in Chapter 5, the city should begin discussions with property owners to acquire easements necessary for the construction of the road network proposed by this Plan even as existing buildings remain for the interim.



TURN ON 28TH STREET CORRIDOR SUB-AREA PLAN

- Actively target new uses as per Market Strategy. Using the Market Strategy as a guide, the City should target new types of land uses contemplated by this plan and seek to establish these land uses as redevelopment occurs. These targeted land uses can also be used to drive discussions with developers and guide redevelopment opportunities along the corridor.
- Expand and maintain discussions with the decision-makers for keystone properties, such as Studio 28, Wyoming Village Mall and Roger’s Plaza to assure clear and complete communication of public and private sector objectives.

Regulatory and Procedural Strategies

Short-term priorities

Amend Master Plan to Include Turn on 28th Street

A Master Plan establishes a direction for the physical development of a community. It is Wyoming’s blueprint for growth and development, and is used by the City to make zoning and land use decisions that affect the future

of the City’s natural and built environments. Therefore, it is imperative that the City’s Master Plan, adopted in 2006, embrace this Sub Area Plan and incorporate it into a cohesive, forward-looking land use policy that will guide development in the City for the next thirty years.

The process to amend the Master will involve the Planning Commission, City Council and the City’s planning staff. It may include revising certain components of the plan document to reflect the vision proposed in this plan, changes to the Future Land Use map, and other amendments to goals and objectives.

Amend zoning ordinance

The Zoning Ordinance is the primary mechanism for plan implementation. However, some of the development forms and uses envisioned in this plan may not fully conform to the City’s current zoning ordinance. As



Access management is a significant issue for some properties on 28th Street



Incorporating mass transit opportunities is an important component of this plan

such, the Zoning Ordinance should be amended to promote and encourage the development proposed in this plan.

Much of the Turn on 28th Street study area is located in the “Downtown Center” zoning district, which allows for mixed uses and contains general standards pertaining to street layout, streetscaping, and architecture. However, this section should be revised and enhanced to incorporate the concepts from this plan, and graphics should illustrate the preferred land uses and building form. A form-based code format would be a favorable approach for regulating development in the study area.

Concepts proposed by this document should be incorporated and clearly addressed in the City’s Zoning Ordinance, including:

- Building types/use



- Frontage types
- Building form/stories
- Multi-story requirements

The amended zoning ordinance should provide for some limited flexibility to meet market demands and needs of developers. However, conformance with the basic concepts of this plan – mixed primary uses, walkability, density, buildings built to the front lot line, etc – must be mandatory, regardless of the development proposed. Otherwise, the City risks undermining this plan’s vision, as well as the public support and interest that has been generated.

Expedite project review process

For redevelopment projects that fulfill the goals of the master plan, the City should consider an expedited review process. This process could include pre-approving several building designs for eventual application on multiple sites and staff approval of elements that meet the intent of the master plan and/or comply with the amended zoning ordinance. Ongoing efforts to streamline development review should be continually evaluated for improvement of the municipal review process.

Mid-term priorities

Develop a plan for utility extensions.

While the study area is currently well-served with

public utilities, a logical plan to extend and/or relocate public utilities should be undertaken in conjunction with this plan’s recommendations for phasing, making certain portions of the site as “shovel-ready” as possible. Communications and electric infrastructure should be addressed as well. When other improvements are undertaken, wires should be placed underground and sites should have telephone, internet and similar services available.

Develop stormwater management plan.

Stormwater management should be provided in a holistic manner as redevelopment occurs, being sensitive to conservation design principles and consistent with the urban design envisioned in the final plan whenever possible. Stormwater management systems should be treated as amenities and integrated into the overall site as visual features.

Parcel acquisition

In instances where willing sellers come forward or as foreclosed or tax delinquent properties become available, the City and DDA should consider parcel acquisition. This will allow the City to exert more control of the redevelopment process over time and utilize the acquired properties as part of an incentive mechanism to promote redevelopment and investment. Generally, it is expected that land assembly will occur through private negotiations between willing property

owners and developers and that much of the redevelopment process will be implemented from the private sector rather than through public sector property acquisition and condemnation for economic development is not anticipated.

The City should prepare acquired sites for redevelopment by proactively addressing potential environmental concerns.

Public open space dedication

In order to incorporate the public open space, particularly at the connection nodes along 28th Street, as envisioned by the final plan, the City should consider dedicating or assessing fees in lieu of dedication as part of any redevelopment agreements. This would apply to any redevelopment within the study area and would require that developers either dedicate land for parks and open space or pay a cash-in-lieu fee. The expectation is that the public land dedication



Carefully planned open space is an important component in the final design



requirement is sufficient to provide open space that is consistent with that depicted on the final plan.

Ongoing assessment of existing business needs

An ongoing assessment of the current and future needs of existing businesses and users within the study area should be periodically revisited during the redevelopment and retrofit cycle, to better understand the potential for business relocation, retention or expansion within the sub area.

Encourage sustainable development practices.

Environmentally sustainable development practices should be required for all redevelopment projects in the sub area, in particular if financial incentives are being requested. Developers of new commercial, mixed use and residential buildings should be encouraged to seek LEED or LEED-ND certification, utilizing "green" construction techniques and energy conservation measures in building design. LEED



certification, or certification utilizing similar standards should be a requirement for projects seeking financial assistance from the City.

Transportation

Short-term priorities

Explore grant opportunities to aid transportation funding.

With a growing movement to reverse a trend of disinvestment in inner-ring suburbs, facilitate multi-modal transportation and promote healthy lifestyle choices such as walking and bicycling, funding opportunities may be available for some of the improvements contemplated by this plan. The City should explore opportunities for grants to help offset the cost of implementing this plan.

Safe Routes to School

Safe Routes to School (SR2S) is an international movement—and now a federal program—to make it safe, convenient and fun for children to bicycle and walk to school. One of the primary purposes of the SR2S program is to develop projects and encourage activities that will improve student health and safety while reducing traffic, fuel consumption, and air pollution in the vicinity of schools.



Sidewalks, traffic calming measures, pedestrian and bicycle crossing improvements, bicycle facilities and traffic diversion improvements are examples of infrastructure that is eligible for SR2S funding.

Transportation Economic Development Fund

The Transportation Economic Development Fund (TEDF) was created to assist in the funding of highway, road, and street projects necessary to support economic growth. As part of the TEDF, category A: Economic Development Road Projects promotes projects that increase economic potential and improve the quality of life through support of job creation and retention in Michigan.

The fund will provide matching grants (with a minimum 20% local match) for projects that address a transportation need that is critical to

an economic development project that will create or retain jobs.

Eligible projects are those that address a transportation

need (condition, safety or accessibility) that is critical to an economic development project that relates to food processing, high technology



research, manufacturing or office centers that are 50,000 square feet or more.

Transportation Enhancement Program

The Transportation Enhancement (TE) program is a competitive grant program that funds projects such as nonmotorized paths and streetscapes that enhance Michigan’s intermodal transportation system and improve the quality of life for Michigan citizens.



Some portions of the 28th Street corridor are lacking sidewalks.

TE funding requires matching funds of at least 20% of project cost and proposed TE projects must have a relationship to surface transportation.

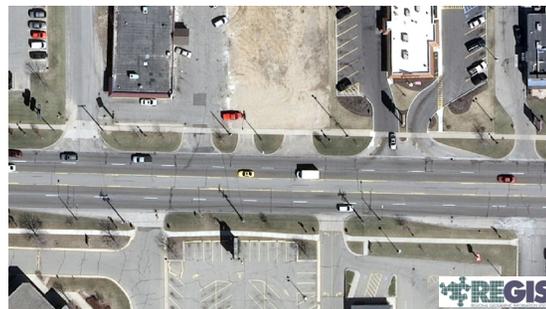
Eligible projects may include provisions for facilities for pedestrians and bicycles, transportation aesthetics including landscaping and control and removal of outdoor advertising.

Complete and improve sidewalks

Sidewalks currently exist along both sides of 28th Street, but they are disjointed and frequently interrupted with curb cuts, driveways and parking lots. As driveways close and curb cuts are removed, the sidewalks should be connected with the eventual goal of completely connected sidewalks along the north and south sides of the 28th Street corridor. Such improvements to 28th Street will also need to be coordinated with MDOT.

Continue implementing non-motorized connections

The city should continue to enhance the existing non-motorized network, specifically in regards to connections that are adjacent to the study area and in support of the Turn on 28th Street Final Plan. In many cases, short-term improvements may include affordable strategies such as restriping existing streets. Such improvements to 28th Street will also need to be coordinated with MDOT.



The proliferation of driveways and curb cuts along 28th Street can impede traffic flow.

Mid-term priorities

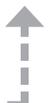
Develop a 28th Street Access Management Plan

Currently, 59 driveways exist along 28th Street between Burlingame and Clyde Park Avenues leading to individual businesses and/or properties. This proliferation of driveways and curb cuts make pedestrian and bicycle travel difficult and dangerous and create potential vehicular conflicts at each curb cut reducing the roadway efficiency. To address this issue, the City should work with property owners and MDOT to develop an Access Management Plan for the 28th Street corridor. The plan should look to close unused or under-used driveways, particularly on the north side of 28th Street, and to develop access roads either in front of or behind existing properties to improve traffic flow and safety for all users of 28th Street.

Long-term priorities

Collaborate with The Rapid to plan future transit services and amenities

Working with The Rapid, plan for one or more accessible and logical locations for a transit stop, both along 28th Street and along the proposed crescent street, depending on feasibility.





Marketing and Promotion

Short-and mid-term priorities

Public outreach

A plan that does not have the support of residents is often ignored and therefore doomed to failure. Given the ambitious redevelopment design proposed by this document, the City must actively seek the ongoing support of residents and business owners to carry the plan through implementation.

Public outreach may take the form of updates in the City’s newsletter, social media (facebook, twitter, etc.) or periodic public meetings. It will also be important to consult frequently and cultivate a positive and fruitful relationship with local business owners, as their support is also crucial to the implementation of this plan. This group should be reminded of the plan and notified when key benchmarks are readied.

Seek out development partnerships.

As redevelopment begins to take shape, the City will need to be actively involved in seeking out partnerships with developers to ensure an orderly and successful transfer of property, if necessary, and to ensure the properties are developed in accordance with this plan and the City’s Master Plan.

As this process moves forward, the City should work to cultivate collaborative partnerships

with developers to facilitate the redevelopment process.

Market sites and the plan to developers.

If this plan is to be realized, developers must redevelop sites in a way that follows the blueprint provided by this plan. However, it cannot be assumed that development will simply arrive without being actively and aggressively recruited by the City.

Using information from this plan and the Market Study, the City should identify a group of developers and land owners with the capability to redevelop the site in a way that generates the desired land uses and building form outlined in this plan and the Market Study.

Economic Development and Financing Alternatives

Short-term priorities

Focus business recruiting on both large retailers and smaller local businesses.

For the vision outlined in this plan to be fully realized, the redeveloped 28th Street corridor will need to contain a healthy mix of land uses owned by a mix of both small and large businesses. As such, business recruitment and retention efforts should include economic gardening (see

page 16) and the recruitment of large national retailers.

Seek partnering opportunities for economic development.

Partnering with a non-profit development entity who can assemble and hold land in cooperation with the city. This partnership could assist with land swap transactions and also undertake infill and redevelopment projects within the sub area. Potential local non-profit development partners could include LINC, ICCF and Dwelling Place. The Right Place is another excellent resource.

Mid- and Long-term priorities

Explore and secure external funding sources to aid the redevelopment process.

Redevelopment and retrofit of the sub area may present financial gaps that prohibit successful reconstruction and infill. The magnitude of this gap will vary based on the infrastructure upgrades and other public improvements necessitated by the project, market value of properties to be acquired and any increase in the allowable density being considered.

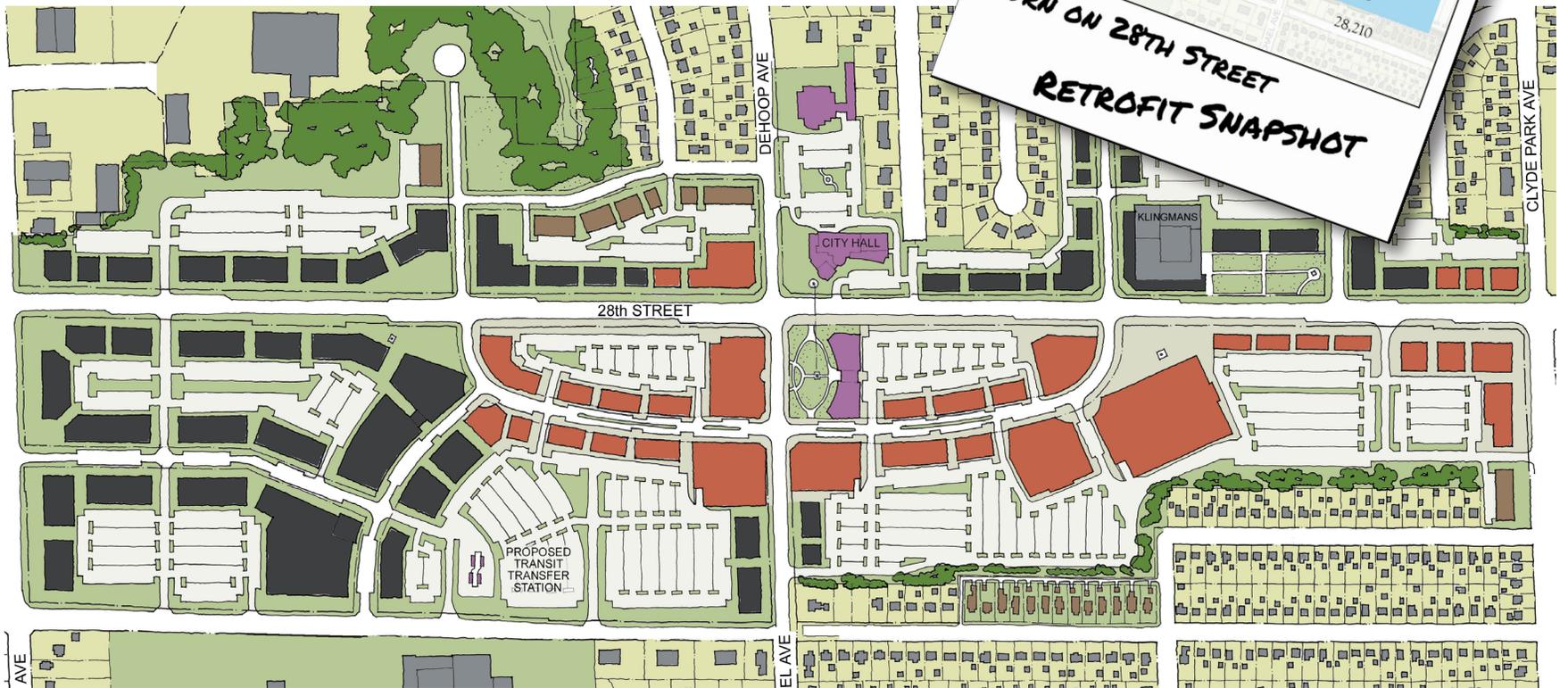
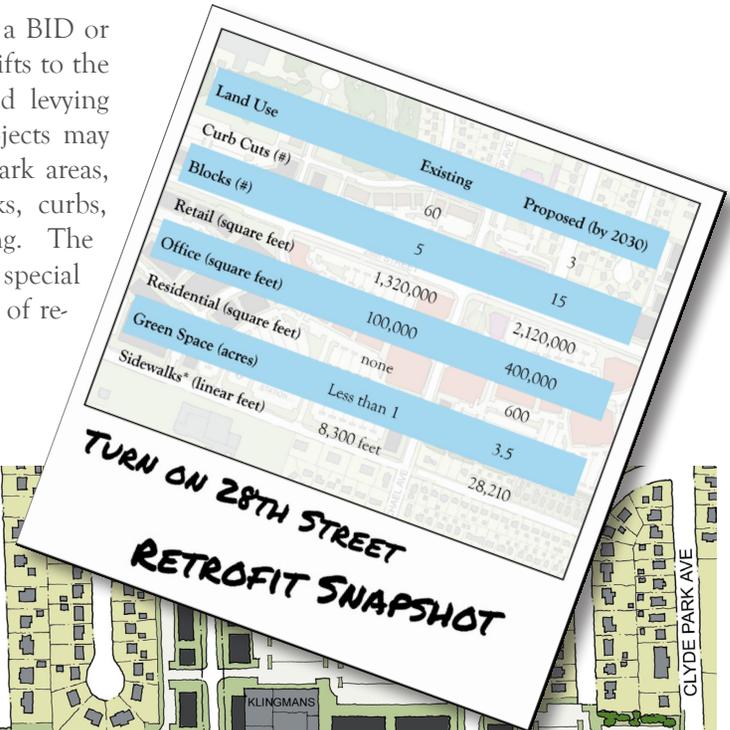
The City and DDA should consider assisting in the redevelopment process to reduce the financial gaps through various means, potentially including the following State of Michigan programs:



- Business Improvement District (BID)
- Principal Shopping District (PSD)
- Business Improvement Zone (BIZ)

A BID/PSD allows a municipality to collect revenues, levy special assessments and issue bonds in order to address the maintenance, security and operation of that district. A BIZ may be created by private property owners and may level special assessments to finance activities and

projects. The cost of projects within a BID or PSD may be financed by grants and gifts to the City or district, municipal funds and levying of special assessments. Example projects may include maintaining and operating park areas, relocating or reconstructing sidewalks, curbs, street medians, fountains and lighting. The BIZ may be financed by grants, gifts, special assessments and loans in anticipation of receipt of assessments.





Brownfield tax increment financing (TIF)

Through the Wyoming Brownfield Redevelopment Authority, contaminated, blighted or functionally obsolete properties may be allowed to capture state and local property taxed to pay the costs associated with the brownfield site.

Community development block grant (CDBG)

The Michigan CDBG program consists of grants for local units of government for activities such as economic development, downtown development and planning. Specifically, monies may be available to aid in funding infrastructure improvements, securing development, and funding planning activities that lead to other eligible development projects.

Create small business incubator to help startups.

Because of the high capital costs of new construction, many newer buildings are often occupied initially by franchise or “chain” commercial establishments. While a certain number of these uses is anticipated and desired, this plan also seeks to encourage local entrepreneurial businesses in the study area. To aid local entrepreneurs, the City should create a small business incubator program that would provide assistance to businesses looking to establish themselves or grow their existing business in Wyoming.

Consider rent subsidies or other financial assistance for new small businesses in redevelopment area.

Because many new construction projects are simply too expensive for local merchants, and as a result, local businesses are forced to find other accommodations in the City. To attract and retain local businesses in the redevelopment area, the City and/or DDA should consider funding a program to subsidize rent payments or offer other types of financial assistance. Such a program may attract a healthy and desirable mix of larger “destination” businesses as well as local establishments.

Create a Community Development Corporation

Community development corporations are non-profit, community-based organizations that anchor capital locally through the development of both residential and commercial property, ranging from affordable housing to developing shopping centers and even owning businesses.

Conclusion

The suburban retrofit of 28th Street is ambitious yet feasible. While the predominant suburban development pattern is well established, its current form provides opportunities for redevelopment from the large expanses of parking lots to the “super block” parcel configurations. The DDA and the City must be the stewards of this plan and ensure that all land use decisions consider the vision and the implementation of the retrofit. Decisions must advance the design concept and move it forward.

Each incremental change and step towards redevelopment should be acknowledged and celebrated as it will provide reminders to those who participated that the elected and appointed officials are moving in a common direction. Daniel Burnham said “Make no small plans, they have no power to stir men’s blood.” Indeed, this is not a small plan and it will take time to accomplish, but momentum will build with each small step achieved and with each step achieved it will be easier and easier to keep the vision in mind. It took 40 years for 28th Street to reach its zenith, and it will likely take decades for it to revitalize and redevelop.

RESOLUTION NO. _____

RESOLUTION TO AUTHORIZE THE DISTRIBUTION OF THE DRAFT
2035 THOROUGHFARE PLAN

WHEREAS, the City of Wyoming Thoroughfare Plan adopted in 1998 and incorporated into the Wyoming Land Use Plan 2020 adopted in December 2006, is outdated, and

WHEREAS, the Municipal Planning Enabling Act 33 of 2008 requires municipalities to review the Land Use Plan at least every five years, and

WHEREAS, the City of Wyoming contracted with URS to develop the Draft 2035 Thoroughfare Plan as a comprehensive analysis of existing and future thoroughfare conditions based on extensive traffic modeling, and recommend any necessary future changes and enhancements to improve the thoroughfare system, and

WHEREAS, the Development Review Team of the City of Wyoming concurs with the analysis and recommendations contained in the Draft 2035 Thoroughfare Plan, and

WHEREAS, on July 16, 2011, the Wyoming Planning Commission did review the Draft 2035 Thoroughfare Plan and recommended it to the Wyoming City Council, and

WHEREAS, the Municipal Planning Enabling Act 33 of 2008 requires, as a part of the Land Use Plan amendment process, the City Council to authorize the Draft 2035 Thoroughfare Plan for distribution to adjoining communities, and regional planning, transportation and utility agencies, and

BE IT RESOLVED that the Wyoming City Council does hereby authorize the distribution of the Draft 2035 Thoroughfare Plan.

Councilmember _____ moved, seconded by Councilmember _____, that the above Resolution be adopted.

Motion carried _____ Yeas _____ Nays

I hereby certify that the foregoing Resolution was adopted by the City Council for the City of Wyoming, Michigan at a _____ session held on the _____ day of _____, 2011.

HEIDI A. ISAKSON
Wyoming City Clerk

RESOLUTION NO. _____



July 25, 2011

MAYOR
Jack A. Poll

AT-LARGE COUNCILMEMBER
Sam Bolt

AT-LARGE COUNCILMEMBER
Dan Burrill

AT-LARGE COUNCILMEMBER
Kent Vanderwood

1ST WARD COUNCILMEMBER
William A. VerHulst

2ND WARD COUNCILMEMBER
Richard K. Pastoor

3RD WARD COUNCILMEMBER
Joanne M. Voorhees

CITY MANAGER
Curtis L. Holt

Ms. Heidi A. Isakson
City Clerk
Wyoming, MI

Subject: Request to authorize the distribution of the Draft 2035
Thoroughfare Plan

Recommendation: To authorize the distribution of the Draft 2035 Thoroughfare
Plan

Dear Ms. Isakson:

The above referenced request was reviewed by the Wyoming Planning Commission at its regular meeting on July 19, 2011. Staff had the following comments:

The current City of Wyoming Thoroughfare Plan was adopted by the Planning Commission in February 1998. That Plan proposed numerous street enhancement projects in response to the pending Southbelt Freeway (M-6) and the rapid development occurring in South Wyoming. These street improvement projects included Wilson Avenue, Byron Center Avenue, 44th Street and Gezon Parkway that were subsequently undertaken.

With the City of Wyoming nearly built out, and with the existing significant street infrastructure in place, the focus of the new 2035 Thoroughfare Plan shifts to primarily endorsing less significant road widenings and other system enhancements. Ivanrest Avenue north of 52nd Street, and 56th Street between Ivanrest and Byron Center Avenues, are recommended to be improved to four-lane undivided streets. Other considerations include widening Ivanrest Avenue south of 52nd Street to either 3 or 4 lanes undivided and the widening of Division Avenue south of 54th Street. Intersection enhancements at Clay Avenue and 54th Street and Buchanan and 28th Street are also recommended to improve traffic flow.

The document includes additional commentary acknowledging the benefits of developing an expanded non-motorized system within Wyoming. This system expansion was not a part of the scope of this Thoroughfare Plan. However, a future planning process is recommended to identify the corridors that would best serve the non-motorized needs of the community.

The Draft 2035 Thoroughfare Plan was developed by URS under contract with the City. Traffic information and modeling was provided by the Michigan Department of Transportation and the Grand Valley Metropolitan Council.

The Draft 2035 Thoroughfare Plan is an amendment to the City of Wyoming Land Use Plan 2020. The process for adopting this plan is established by State Law – The Michigan Planning Enabling Act No. 33 of 2008. The Planning Commission is required to submit the plan amendment to the City Council for review and comment. The City Council must then approve the plan for distribution. The plan will then be distributed to adjoining communities, utilities, and area transportation and planning agencies for review and comment. After 63 days, a public hearing will be scheduled before the Planning Commission for consideration of adopting the plan. The City Council, through resolution, may assert the right to approve, or reject, the plan amendment. It is the desire of the DRT to track the approval of the Draft 2035 Thoroughfare Plan with the Draft Turn On 28th Street Corridor Sub Area Plan.

The Development Review Team suggested the Planning Commission recommend to the City Council the proposed Draft 2035 Thoroughfare Plan.

A motion was made by Heygi, supported by Arnoys, to recommend to City Council the Draft 2035 Thoroughfare Plan. After discussion, the motion carried unanimously. Additional explanation regarding this proposal may be obtained from the Planning Commission minutes of July 19, 2011.

Respectfully submitted,



Timothy Cochran, City Planner
Planning and Development Department

cc: Curtis Holt, City Manager
Rebecca Rynbrandt, Director of Community Services

FINAL REPORT

CITY OF WYOMING 2035 THOROUGHFARE PLAN

Prepared For:



CITY OF WYOMING
2660 Burlingame Avenue
Wyoming, MI 49509

Prepared By:

URS

GRAND RAPIDS – SOUTHFIELD – TRAVERSE CITY

FEBRUARY, 28 2011

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1.0 INTRODUCTION

1.1 Background

The City of Wyoming, Michigan, which adjoins the City of Grand Rapids, created its first Thoroughfare Plan in 1970. The Plan was updated and amended in 1976, 1987 and 1998. Each of these plans have been important community planning tools which have assisted City officials with decisions related to traffic demands placed upon the City's thoroughfare system.

The preparation of this 2035 Thoroughfare Plan incorporates traffic data collected by the City of Wyoming and the Michigan Department of Transportation (MDOT) in 2008 and 2009. The Plan utilized the latest version of the transportation planning model developed by the Grand Valley Metropolitan Council (GVMC)—the local Metropolitan Planning Organization (MPO) for the Grand Rapids metropolitan area. The GVMC is responsible for organizing transportation improvement projects and allocating funds to complete such projects. The GVMC planning model guided the development of Average Daily Traffic projections for city thoroughfares. The traffic projections in turn were used to determine areas of need.

1.2 Report Organization

This report is organized into the following sections:

- 2.0 *Existing Conditions* – This section provides an inventory of current physical conditions and an analysis of the existing operational quality of the City's thoroughfares.
- 3.0 *Future Conditions* – This section contains an operational analysis of the City's thoroughfare system under traffic conditions projected to the year 2035. The need for future changes and enhancements to the current thoroughfare system was studied and analyzed utilizing the GVMC transportation planning model.
- 4.0 *2035 Thoroughfare Plan* – The results of the future conditions analysis were used to develop the updated Thoroughfare Plan. The Plan contains recommendations regarding the expansion of existing thoroughfares to provide the needed capacity for future years.

All analyses documented in this report were performed in accordance with MDOT, FHWA, and AASHTO practices, guidelines, policies, and standards, including the 2000 Highway Capacity Manual (HCM), A Policy on Geometric Design of Highways and Streets (AASHTO, 2004) and the Michigan Manual of Uniform Traffic Control Devices (MMUTCD, 2005).

2.0 EXISTING (2009) CONDITIONS

This section contains an inventory of current physical conditions and an analysis of the existing operational quality of the City's thoroughfare system. The inventory includes presentations of functional classification, existing traffic volumes, crash analyses, on-street parking, existing number of lanes and pavement widths, and existing right-of-way widths. These data are used to determine the operational characteristics and existing Level-of-Service of each of the City's thoroughfares.

2.1 Functional Classification

Any street in a roadway network can be classified as to the character of service it is intended to provide. Its functional classification is necessary for communication purposes. Each thoroughfare in the City of Wyoming has been assigned to the following hierarchical system of functional classes by the Grand Valley Metro Council (GVMC):

- ***Principal Arterials***
The function of principal arterials such as M-11 (28th Street), 44th Street, Wilson Avenue, and Byron Center Avenue are to provide regional travel capabilities as well as to serve the City's major activity centers. Principal arterial streets typically carry large volumes of traffic over long distances.
- ***Minor Arterials***
Minor arterial streets such as Burlingame Avenue and 52nd Street augment the principal arterial system by distributing traffic to smaller geographical areas within the City. Mobility is emphasized less on minor arterial streets than on principal arterials, while access to abutting land is emphasized more. Minor arterial streets can provide access between communities, but they do not typically enter specific neighborhoods.
- ***Collectors***
The function of collector streets such as 40th Street and De Hoop Avenue is to funnel traffic from the arterial system to local streets and other destinations. Collector streets typically provide access to neighborhoods as well as commercial and industrial areas within the City.
- ***Local Streets***
The function of local streets is to provide access to abutting land; mobility is minor as local streets carry minimal traffic at low speeds over short distances.

Figure 2-1 depicts the functional class of each thoroughfare in the City of Wyoming as defined by the Grand Valley Metro Council.

2.2 Existing Traffic Volumes

The City of Wyoming and MDOT maintain annual traffic count programs. This data was used to determine the existing traffic volumes on each of the City's thoroughfares, including those streets owned and maintained by MDOT (M-11 and I-196BS). The I-196 and US-131 freeways are not included as they are not deemed as city thoroughfares for the purposes of this Plan. A bandwidth plot of the existing Average Daily Traffic (ADT) on each of the City's thoroughfares is presented in Figure 2-2. The 2-way, 24-hour traffic counts shown in Figure 2-2 were collected in 2008 and 2009. The counts were collected at intersection approaches during various calendar months. Seasonal adjustments to the counts were not made.

As shown in Figure 2-2, the highest traffic volumes occur on 54th Street, 44th Street, 36th Street, and M-11 which are the principal east-west arteries. 54th Street carries the highest volume of traffic in the city, with more than 40,000

ADT west of Division Avenue. In general, the north-south streets do not carry as much traffic as the east-west corridors.

Existing (2009) ADT values were compared to the 1996 ADT values as presented in the previous Thoroughfare Plan. The comparisons are shown in **Table 2-1** for selected approaches along selected corridors.

**TABLE 2-1
TRAFFIC VOLUME COMPARISON ON VARIOUS THOROUGHFARES (1996 TO 2009)**

Thoroughfare	Location	ADT (1996)*	ADT (2009)	% Change
56th Street	Just west of Byron Center Avenue	1,500	12,100	+707 %
Byron Center Avenue	Just south of 52 nd Street	6,500	19,700	+203 %
Wilson Avenue	Just south of 52 nd Street	6,000	16,600	+177 %
Ivanrest Avenue	Just north of 52 nd Street	6,000	11,500	+92 %
54 th Street	Just east of US-131	35,000	43,400	+24 %
52 nd Street	Just west of Clyde Park Avenue	10,500	10,000	-5 %
36 th Street	Just east of US-131	32,000	29,800	-7 %
Division Avenue	Just south of 36 th Street	27,000	22,900	-15 %
Clyde Park Avenue	Just south of 44 th Street	19,000	14,500	-24 %
Byron Center Avenue	Just north of 36 th Street	21,000	14,300	-32 %
M-11	Just west of Byron Center Avenue	37,000	24,000	-35 %
44 th Street	Just east of US-131	53,000	33,000	-38 %
Eastern Avenue	Just south of 36 th Street	23,000	13,500	-41 %
Burlingame Avenue	Just north of M-11	22,000	12,800	-42 %

* - as presented in the previously-adopted 2020 Thoroughfare Plan.

As shown in Table 2-1, the traffic on some of the City's thoroughfares has decreased in the last ten to fifteen years. The economic slowdown beginning in 2008 has played a part in reducing vehicular travel (e.g. closure of the Wyoming Stamping Plant on 36th Street). Road construction may also have impacted traffic volumes along some of the corridors depicted in Table 2-1 and in Figure 2-1. For instance, traffic volumes along 44th Street may be lower than normal due to the construction at the US-131 interchange in 2009, while traffic volumes along 54th Street may be higher than normal as a result of the 44th Street construction.

The opening of the M-6 freeway has resulted in increased travel in the southwest corner of the city, particularly along the Byron Center Avenue and Wilson Avenue corridors which have interchanges along M-6. The completion of Gezon Parkway several years ago has reduced travel on 52nd Street and increased traffic on 56th Street. The Metropolitan Hospital complex along Byron Center Avenue between M-6 and Gezon Parkway has likewise increased travel in the southern part of the city.

2.3 Number of Lanes

In addition to traffic volume, the number of travel lanes is an important factor in determining thoroughfare Level-of-Service because it greatly impacts the capacity of a street. The number of travel lanes for each thoroughfare in the City of Wyoming is presented in **Figure 2-3**.

As shown in Figure 2-3, some streets are 4-lane undivided facilities (Gezon Parkway, Burton Street, Burlingame Avenue) while other 4-lane streets are divided (boulevard) facilities (44th Street, Clyde Park Avenue, Wilson Avenue, Byron Center Avenue). Boulevard thoroughfares generally have raised center medians which separate opposing traffic flows. Whereas 4-lane undivided streets are free-access facilities, boulevard facilities have more access control by allowing access to side streets at selected locations only. The presence of a center median provides a pro-

tected queuing area for left-turn movements and increased access control. As a result, boulevard facilities are typically able to operate more efficiently.

Some streets in the City of Wyoming are 5-lane undivided facilities (M-11, Division Avenue, Eastern Avenue). Instead of having a center median like a boulevard facility, a continuous two-way center left-turn lane separates opposing traffic flows. Streets with 5-lane cross-sections are typically found on heavy-volume roads with numerous drive-ways, or where right-of-way may be limited preventing the construction of a boulevard facility.

2.4 Level-of-Service

Level-of-Service is a qualitative measure of how well (or poorly) a street operates. The quality of service is dependent on many factors including peak-hour traffic volumes, traffic composition (percent heavy-vehicles), vehicle speeds, the number of travel lanes, traffic control (signs and signals), and on-street parking. Chapter 21 (*Multilane Highways*) of the 2000 Highway Capacity Manual (HCM) defines each of the levels as shown in **Table 2-2**.

**TABLE 2-2
PEAK-HOUR LEVEL-OF-SERVICE RANGES**

Level-of-Service	Definition
A	Free-flow conditions. Drivers travel at speeds at which they feel comfortable.
B	Slightly reduced maneuverability due to presence of other vehicles. Delays at intersections are not bothersome.
C	Stable operation. Drivers feel appreciable tension as maneuverability becomes more restricted, and vehicular queues form behind any traffic disruption.
D	Maneuverability severely restricted as small increases in traffic flow may cause significant increases in delay.
E	Unstable operations which are at or near the capacity of the roadway; significant delays occur at intersections.
F	Forced Flow. Vehicles arrive at a faster rate than is serviceable creating stop-and-go traffic conditions with extensive queuing and high delays.

Source: 2000 Highway Capacity Manual

Level-of-Service "C" is considered desirable for urban and suburban arterial streets during peak traffic hours, while Level-of-Service "D" is typically deemed acceptable. The methods of Chapter 21 of the 2000 HCM were used to determine the peak-hour Level-of-Service for each thoroughfare in the City of Wyoming. Chapter 21 of the HCM utilizes "maximum service flow" to quantify the boundaries of each Level-of-Service for peak-hour conditions. These maximum peak-hour service flows and the approximate ADT values are shown in **Tables 2-3a** thru **Table 2-3d** for various types of thoroughfare facilities present within the city.

Caution is advised when using the information displayed in **Table 2-3a** thru **Table 2-3d**. It should be noted these tables do not constitute a standard but should be used for general planning purposes only. The methods of Chapter 15 (Urban Streets) of the 2000 HCM offers a more detailed analysis of Level-of-Service for signalized arterial streets; however, detailed intersection turning movement counts and delay information, which are required for the analysis, were not available.

The values shown in **Table 2-3a** thru **Table 2-3d** should be reduced by approximately 30 percent for those thoroughfares with more than three major signalized intersections per mile. Streets exhibiting this characteristic are Burton Street, M-11 east of Burlingame Avenue, 36th Street east of Clyde Park Avenue, 44th Street east of Clyde Park Avenue, and 54th Street. Existing (2009) Average Daily Traffic on each thoroughfare were used to determine its Level-of-Service based on the maximum values contained in **Table 2-3a** thru **Table 2-3d**. The results of the Level-of-Service analysis are displayed in **Figure 2-4**.

TABLE 2-3a (45 MPH)
MAXIMUM PEAK-HOUR SERVICE FLOWS AND APPROXIMATE MAXIMUM ADT VALUES
FOR VARIOUS LEVELS-OF-SERVICE AND FACILITY TYPES

Facility Type	Level-of-Service									
	A		B		C		D		E	
	MSF (pcph*)	ADT	MSF (pcph)	ADT						
2-lane Undivided	270	5,444	446	9,000	644	13,000	853	17,222	1,045	21,111
3-lane Undivided	449	9,074	743	15,000	1,073	21,667	1,421	28,704	1,742	35,185
4-lane Undivided	539	10,889	891	18,000	1,287	26,000	1,705	34,444	2,090	42,222
4-lane Divided	719	14,519	1,188	24,000	1,716	34,667	2,273	45,926	2,787	56,296
5-lane Undivided	719	14,519	1,188	24,000	1,716	34,667	2,273	45,926	2,787	56,296
6-lane Undivided	809	16,333	1,337	27,000	1,931	39,000	2,558	51,667	3,135	63,333
6-lane Divided	988	19,963	1,634	33,000	2,360	47,667	3,126	63,148	3,832	77,407

*pcph = passenger cars per hour in peak direction

Note: Table 2-3a has been developed from Table 21-2 of the 2000 Highway Capacity Manual. The above table assumes turn lanes have two thirds the capacity of through lanes, g/C = 0.55, k-factor = 0.09, and free-flow speed = 45 mph.

TABLE 2-3b (40 MPH)
MAXIMUM PEAK-HOUR SERVICE FLOWS AND APPROXIMATE MAXIMUM ADT VALUES
FOR VARIOUS LEVELS-OF-SERVICE AND FACILITY TYPES

Facility Type	Level-of-Service									
	A		B		C		D		E	
	MSF (pcph*)	ADT	MSF (pcph)	ADT						
2-lane Undivided	237	4,778	396	8,000	572	11,556	759	15,333	990	20,000
3-lane Undivided	394	7,963	660	13,333	953	19,259	1,265	25,556	1,650	33,333
4-lane Undivided	473	9,556	792	16,000	1,144	23,111	1,518	30,667	1,980	40,000
4-lane Divided	631	12,741	1,056	21,333	1,525	30,815	2,024	40,889	2,640	53,333
5-lane Undivided	631	12,741	1,056	21,333	1,525	30,815	2,024	40,889	2,640	53,333
6-lane Undivided	710	14,333	1,188	24,000	1,716	34,667	2,277	46,000	2,970	60,000
6-lane Divided	867	17,519	1,452	29,333	2,097	42,370	2,783	56,222	3,630	73,333

*pcph = passenger cars per hour in peak direction

Note: Table 2-3b has been developed from Table 21-2 of the 2000 Highway Capacity Manual. The above table assumes turn lanes have two thirds the capacity of through lanes, g/C = 0.55, k-factor = 0.09, and free-flow speed = 40 mph.

TABLE 2-3c (35 MPH)
MAXIMUM PEAK-HOUR SERVICE FLOWS AND APPROXIMATE MAXIMUM ADT VALUES
FOR VARIOUS LEVELS-OF-SERVICE AND FACILITY TYPES

Facility Type	Level-of-Service									
	A		B		C		D		E	
	MSF (pcph*)	ADT	MSF (pcph)	ADT						
2-lane Undivided	204	4,111	347	7,000	501	10,111	660	13,333	935	18,889
3-lane Undivided	339	6,852	578	11,667	834	16,852	1,100	22,222	1,558	31,481
4-lane Undivided	407	8,222	693	14,000	1,001	20,222	1,320	26,667	1,870	37,778
4-lane Divided	543	10,963	924	18,667	1,335	26,963	1,760	35,556	2,493	50,370
5-lane Undivided	543	10,963	924	18,667	1,335	26,963	1,760	35,556	2,493	50,370
6-lane Undivided	611	12,333	1,040	21,000	1,502	30,333	1,980	40,000	2,805	56,667
6-lane Divided	746	15,074	1,271	25,667	1,835	37,074	2,420	48,889	3,428	69,259

*pcph = passenger cars per hour in peak direction

Note: Table 2-3c has been developed from Table 21-2 of the 2000 Highway Capacity Manual. The above table assumes turn lanes have two thirds the capacity of through lanes, g/C = 0.55, k-factor = 0.09, and free-flow speed = 35 mph.

TABLE 2-3d (30 MPH)
MAXIMUM PEAK-HOUR SERVICE FLOWS AND APPROXIMATE MAXIMUM ADT VALUES
FOR VARIOUS LEVELS-OF-SERVICE AND FACILITY TYPES

Facility Type	Level-of-Service									
	A		B		C		D		E	
	MSF (pcph*)	ADT	MSF (pcph)	ADT						
2-lane Undivided	171	3,444	297	6,000	429	8,667	556	11,222	880	17,778
3-lane Undivided	284	5,741	495	10,000	715	14,444	926	18,704	1,467	29,630
4-lane Undivided	341	6,889	594	12,000	858	17,333	1,111	22,444	1,760	35,556
4-lane Divided	455	9,185	792	16,000	1,144	23,111	1,481	29,926	2,347	47,407
5-lane Undivided	455	9,185	792	16,000	1,144	23,111	1,481	29,926	2,347	47,407
6-lane Undivided	512	10,333	891	18,000	1,287	26,000	1,667	33,667	2,640	53,333
6-lane Divided	625	12,630	1,089	22,000	1,573	31,778	2,037	41,148	3,227	65,185

*pcph = passenger cars per hour in peak direction

Note: Table 2-3d has been developed from Table 21-2 of the 2000 Highway Capacity Manual. The above table assumes turn lanes have two thirds the capacity of through lanes, g/C = 0.55, k-factor = 0.09, and free-flow speed = 30 mph.

As shown in Figure 2-4, existing Levels-of-Service of most roadway segments within the city are acceptable (LOS "D" or better). Only one roadway operates in the Level of Service "E" or "F" area—54th Street between US-131 and Division Avenue. It should be noted that peak-hour Levels of Service at individual intersections may be worse than what is depicted in Figure 2-4.

2.5 Existing Right-of-Way

The ability to add lanes and increase the capacity of a thoroughfare is dependant upon many factors. One important factor is the availability of right-of-way. Without right-of-way, property must be purchased before a lane can be constructed; however, the added expense may make capacity increases financially infeasible. **Figure 2-5** contains the basic existing right-of-way widths for corridors throughout the City of Wyoming. As shown in Figure 2-5, the existing right-of-way varies from road to road. The right-of-way widths shown in Figure 2-5 may vary slightly along each route, particularly near intersections where additional space is needed to construct turn lanes and provide clear vision corners.

2.6 Traffic Signals and Roundabouts

Traffic signals are needed at intersections with high volumes of traffic; however, they reduce the capacity of a street by frequently stopping traffic. There are a total of 71 signalized intersections in the City of Wyoming. The majority of these signals are located at major intersections. One (1) signal is located at factory/business entrances which have heavy traffic for short durations during peak traffic hours (Eastern Avenue / 40th Street). Other traffic signals are located at intersections with a higher concentration of pedestrians (Godfrey Avenue at Joosten Street). The location of each traffic signal in the City of Wyoming is shown in **Figure 2-6**.

Some of the traffic signals shown in Figure 2-6 have actuated features. Actuated traffic signals utilize traffic detectors, inductance loops imbedded into the pavement or video cameras that sense the presence of vehicles. Actuated traffic signals respond to demand and apportion green time more efficiently than pre-timed traffic signals. Most of the traffic signals in the City of Wyoming have detectors in left-turn lanes which allow the traffic signal controllers to apportion more green time to the heaviest-volume turning movements during peak periods.

The reduction of capacity caused by traffic signals can be diminished somewhat by interconnecting and coordinating the timing schemes of multiple traffic signals. Coordination of traffic signals provides efficient progression of the traf-

fic stream through the street network. With efficient progression, vehicles can be served by a series of traffic signals without being stopped. The traffic signals in the City of Wyoming, including the traffic signals along state trunkline roadways, are a part of a master system coordinated by the City of Grand Rapids. The City of Grand Rapids has optimized the timings of several key corridors in Wyoming, including the east-west corridors of 54th Street, 44th Street, 36th Street, and Burton Street, and the north-south corridors of Wilson Avenue, Byron Center Avenue, and Division Avenue. MDOT optimized the traffic signal timings along 28th Street (M-11) in 2009.

Modern roundabouts are becoming more popular across the United States as an alternative method for controlling traffic at an intersection. The modern roundabout involves a circular roadway with entry and exit points. Approaching vehicles must yield at the roundabout entry and circulate counterclockwise until reaching the desired exit point. The modern roundabout has been shown to reduce crash severity, since vehicles are required to slow down in order to enter the roundabout. Crashes that occur at roundabouts are typically slow-speed sideswipe crashes. Modern roundabouts, if designed properly, can also serve high volumes of traffic at Levels of Service equal to or better than traffic signals.

The City of Wyoming constructed its first roundabout at the intersection of Jacob Street and Maple Tree Court, just west of Canal Avenue in the southwest corner of the city. Modern roundabouts at major intersections could also be considered at intersections such as the Burton Street/Burlingame Avenue/Lee Street intersection and at the I-196BL (Chicago Drive)/Godfrey Street intersection. These intersections may be good candidates for the modern roundabout, given the 6-legged nature of the Burton Street location and the skewed north approach at the I-196BS location. Additional study would be required, as standards of practice are still evolving for how best to handle pedestrians at both single-lane and multi-lane roundabouts.

2.7 On-Street Parking

The existence of on-street parking can impact the capacity of a street, as the presence of parked vehicles directly adjacent to the traffic stream tends to restrict the flow of traffic. The location of all on-street parking areas in the City of Wyoming is shown in **Figure 2-7**. When compared with **Figure 2-2** and **Figure 2-4**, it can be seen that those streets which allow “on-street” parking are all low-volume streets without any existing capacity problems.

2.8 Crash Analysis

Crash records from January 1, 2007 through December 31, 2009 were analyzed for all thoroughfare intersections. A summary of the results are shown in the following tables:

Table 2-4a	I-196BS, Burton Street, and Porter Street (10 intersections)
Table 2-4b	M-11, Prairie Parkway, and 32 nd Street (18 intersections)
Table 2-4c	36 th Street and 54 th Street (17 intersections)
Table 2-4d	44 th Street, 52 nd Street, 56 th Street, and Gezon Parkway (16 intersections)

The tables include the three (3) most common crash types at each intersection as well as crash severity and crash rate statistics. Two (2) fatalities occurred during the 3-year period. Both of the fatal crashes took place in 2009.

Intersection crash rates were evaluated to determine which intersections had higher-than-average crash rates when compared to intersections with similar entering ADT. **Table 2-5** lists intersections with higher-than-average crash rates and **Figure 2-8** depicts the locations of these intersections. It should be noted that only limited data exists regarding the rate of crashes at intersections. The average crash rate data was supplied by the Southeast Michigan Council of Governments (SEMCOG), which is the Metropolitan Planning Organization (MPO) for the metropolitan Detroit area.

A total of 14 intersections (of 61 intersections studied) had higher-than-average crash rates. The following five (5) intersections were more than 50% above the average crash rate:

- M-11 @ Burlingame Avenue
- M-11 @ Michael Avenue
- M-11 @ Clyde Park Avenue
- M-11 @ Division Avenue
- 44th Street @ Clyde Park Avenue

These intersections as well as the other intersections with higher-than-average crash rates should be examined more closely to determine if any intersections have correctable crash patterns.

TABLE 2-4a
INTERSECTION CRASH ANALYSIS (I-196BS, BURTON STREET, AND PORTER STREET)

Intersection	Top 3 Crash Types		Total Crashes	% Injury Crashes	# of People Injured	ADT Entering Intersection	Average Crash Rate (per MEV*)	Actual Crash Rate (per MEV*)
	Crash Type	%						
I-196BS (Chicago Dr) @ Byron Center Avenue	Fixed Object	23.1	6	17.0	1	16,900	1.70	0.32
	Rear-End Straight	23.1						
	Side-Swipe Same	23.1						
I-196BS (Chicago Dr) @ Burlingame Avenue	Head-On Left-Turn	35.0	20	25.0	5	24,050	1.43	0.76
	Angle	35.0						
	Rear-End	15.0						
I-196BS (Chicago Dr) @ Godfrey Avenue	Rear-End Straight	33.3	24	29.2	10	20,000	1.70	1.10
	Head-On Left-Turn	20.8						
	Angle	20.8						
I-196BS (Chicago Dr) @ Clyde Park Avenue	Rear-End Straight	29.2	24	16.7	4	33,500	1.21	0.65
	Angle	16.7						
	Bicycle	12.5						
Burton Street @ Burlingame Avenue	Rear-End Straight	61.1	18	11.1	3	29,400	1.43	0.56
	Angle	11.1						
	Side-Swipe Same	5.6						
Burton Street @ Cleveland Avenue	Angle	25.0	8	50.0	4	22,900	1.43	0.32
	(6 Types)	12.5						
Burton Street @ Godfrey Avenue	Rear-End Straight	50.0	16	18.8	3	27,300	1.43	0.54
	Side-Swipe Same	18.8						
	Angle	18.8						
Burton Street @ Clyde Park Avenue	Rear-End Straight	37.3	59	20.3	14	42,300	1.14	1.27
	Angle	30.5						
	Head-On Left-Turn	10.2						
Porter Street @ Burlingame Avenue	Head-On Left-Turn	40.0	15	20.0	5	17,300	1.70	0.79
	Angle	26.7						
	Rear-End Straight	13.3						
Porter Street @ Byron Center Avenue	Angle	50.0	8	25.0	2	11,250	1.70	0.65
	(4 Types)	12.5						

*MEV – million entering vehicles

**TABLE 2-4b
INTERSECTION CRASH ANALYSIS (M-11, PRAIRIE PARKWAY, AND 32ND STREET)**

Intersection	Top 3 Crash Types		Total Crashes	% Injury Crashes	# of People Injured	ADT Entering Intersection	Average Crash Rate (per MEV*)	Actual Crash Rate (per MEV*)
	Crash Type	%						
M-11 (28 th Street) @ Byron Center Avenue	Rear-End Straight	33.3	60	31.7	22	36,650	1.21	1.50
	Angle	33.3						
	Head-On Left-Turn	11.7						
M-11 (28 th Street) @ Burlingame Avenue	Rear-End Straight	51.2	84	14.3	12	40,250	1.14	1.91
	Angle	13.1						
	Side-Swipe Same	10.7						
M-11 (28 th Street) @ Michael Avenue	Rear-End Straight	43.8	73	15.1	13	36,800	1.21	1.81
	Angle	20.5						
	Side-Swipe Same	17.8						
M-11 (28 th Street) @ Clyde Park Avenue	Rear-End Straight	43.9	98	26.5	33	44,800	1.14	2.00
	Angle	22.4						
	Side-Swipe Same	11.2						
M-11 (28 th Street) @ SB US-131	Rear-End Straight	57.8	45	22.2	11	42,200	1.14	0.97
	Side-Swipe Same	13.3						
	Angle	11.1						
M-11 (28 th Street) @ NB US-131	Rear-End Straight	69.4	36	19.4	8	44,100	1.14	0.75
	Angle	8.3						
	Rear-End Right-Turn	8.3						
M-11 (28 th Street) @ Buchanan Avenue	Rear-End Straight	45.8	72	22.2	26	47,350	1.14	1.39
	Side-Swipe Same	16.7						
	Angle	16.7						
M-11 (28 th Street) @ Division Avenue	Rear-End Straight	48.6	111	21.6	27	56,500	1.18	1.79
	Angle	18.2						
	Side-Swipe Same	13.5						
Prairie Parkway @ Byron Center Avenue	Angle	38.1	21	23.8	6	23,850	1.43	0.80
	Rear-End Straight	19.0						
	Side-Swipe Same	14.3						
Prairie Parkway @ Burlingame Avenue	Angle	58.6	29	24.1	10	24,000	1.43	1.10
	Rear-End Straight	20.7						
	Head-On Left-Turn	17.2						
Prairie Parkway @ Michael Avenue	Angle	40.0	10	10.0	1	18,350	1.70	0.50
	Fixed Object (4 Types)	20.0						
		10.0						
32nd Street @ Michael Avenue	Side-Swipe Same (4 Types)	33.3	6	16.7	1	15,350	1.70	0.36
		16.7						
32nd Street @ Clyde Park Avenue	Head-On Left-Turn	38.1	21	33.3	7	21,500	1.43	0.89
	Angle	28.6						
	Side-Swipe Same	19.0						
32nd Street @ Buchanan Avenue	Angle	60.0	5	40.0	2	28,950	1.43	0.16
	Rear-End Straight	20.0						
	Side-Swipe Opp	20.0						
32nd Street @ Division Avenue	Angle	41.9	43 (1 fatal)	41.9	25	40,100	1.14	0.98
	Rear-End Straight	30.2						
	Head-On Left-Turn	14.0						
32nd Street @ Jefferson Avenue	Angle	28.6	7	57.1	5	12,700	1.70	0.50
	Head-On Left-Turn	14.3						
	Side-Swipe Same	14.3						
32nd Street @ Madison Avenue	Head-On Left-Turn	33.3	6	33.3	2	15,050	1.70	0.36
	Angle	33.3						
	Rear-End Straight	33.3						
32nd Street @ Eastern Avenue	Rear-End Straight	35.7	14	35.7	7	24,450	1.43	0.52
	Head-On Left-Turn	21.4						
	Angle	21.4						

*MEV – million entering vehicles

**TABLE 2-4c
INTERSECTION CRASH ANALYSIS (36TH STREET AND 54TH STREET)**

Intersection	Top 3 Crash Types		Total Crashes	% Injury Crashes	# of People Injured	ADT Entering Intersection	Average Crash Rate (per MEV*)	Actual Crash Rate (per MEV*)
	Crash Type	%						
36th Street @ Byron Center Avenue	Head-On Left-Turn	27.6	29	31.0	13	26,900	1.43	0.98
	Rear-End Straight	27.6						
	Angle	20.7						
36th Street @ Burlingame Avenue	Rear-End Straight	38.1	42	33.3	15	35,000	1.21	1.10
	Angle	23.1						
	Head-On Left-Turn	11.9						
36th Street @ Michael Avenue	Angle	28.6	28	39.3	18	29,000	1.43	0.88
	Rear-End Straight	25.0						
	Head-On Left-Turn	21.4						
36th Street @ Clyde Park Avenue	Rear-End Straight	34.6	52	26.9	20	37,350	1.21	1.27
	Angle	28.8						
	Head-On Left-Turn	13.5						
36th Street @ SB US-131	Rear-End Straight	37.5	40	15.0	9	35,400	1.21	1.03
	Angle	27.5						
	Side-Swipe Same	17.5						
36 Street @ NB US-131	Rear-End Straight	32.1	28	17.9	8	33,700	1.21	0.76
	Head-On Left-Turn	25.0						
	Angle	14.3						
36th Street @ Clay Avenue	Rear-End Straight	50.0	12	33.3	4	30,700	1.21	0.36
	Bicycle	16.7						
	Angle	16.7						
36th Street @ Buchanan Avenue	Rear-End Straight	24.0	25	44.0	15	35,900	1.21	0.64
	Head-On Left-Turn	24.0						
	Side-Swipe Same	20.0						
36th Street @ Division Avenue	Rear-End Straight	51.1	47	25.5	17	48,050	1.14	0.89
	Angle	23.4						
	Head-On Left-Turn	17.0						
36th Street @ Jefferson Avenue	Angle	46.1	13	46.2	10	23,400	1.43	0.51
	Head-On Left-Turn	30.8						
	Side-Swipe Same	7.7						
36th Street @ Madison Avenue	Angle Straight	40.0	20	20.0	5	27,700	1.43	0.66
	Rear-End Straight	25.0						
	Rear-End Right-Turn	10.0						
36th Street @ Eastern Avenue	Rear-End Straight	34.8	23 (1 fatal)	21.7	5	34,500	1.21	0.61
	Head-On Left-Turn	21.7						
	Angle	17.4						
54th Street @ Gezon Parkway / Clyde Park Avenue	Rear-End Straight	52.5	59	23.7	15	44,100	1.14	1.22
	Side-Swipe Same	16.9						
	Angle	11.9						
54th Street @ SB US-131	Rear-End Straight	60.1	46	13.0	7	47,700	1.14	0.88
	Angle	15.2						
	Side-Swipe Same	8.7						
54th Street @ NB US-131	Rear-End Straight	58.6	29	20.7	8	47,800	1.14	0.55
	Side-Swipe Same	13.8						
	Angle	10.3						
54th Street @ Clay Avenue	Rear-End Straight	85.7	28	17.9	6	47,900	1.14	0.53
	Other	7.1						
	Head-On Left-Turn	3.6						
54th Street @ Division Avenue	Angle	32.9	82	24.4	25	59,000	1.18	1.27
	Rear-End Straight	26.8						
	Side-Swipe Same	14.6						

*MEV – million entering vehicles

TABLE 2-4d
INTERSECTION CRASH ANALYSIS (44TH STREET, 52ND STREET, 56TH STREET, AND GEZON PARKWAY)

Intersection	Top 3 Crash Types		Total Crashes	% Injury Crashes	# of People Injured	ADT Entering Intersection	Average Crash Rate (per MEV*)	Actual Crash Rate (per MEV*)
	Crash Type	%						
44th Street @ Byron Center Ave	Rear-End Straight	45.6	90	28.9	32	50,800	1.18	1.62
	Head-On Left-Turn	17.8						
	Angle	15.6						
44th Street @ Burlingame Avenue	Rear-End Straight	56.7	60	30.0	22	42,900	1.14	1.28
	Angle	10.0						
	Head-On Left-Turn	6.7						
44th Street @ Clyde Park Avenue	Rear-End Straight	44.2	86	18.6	18	44,800	1.14	1.75
	Angle	19.8						
	Side-Swipe Same	16.3						
44th Street @ SB US-131	Rear-End Straight	43.1	51	19.6	13	42,600	1.14	1.09
	Angle	21.6						
	Side-Swipe Same	13.7						
44th Street @ NB US-131	Rear-End Straight	45.0	40	15.0	9	36,100	1.21	1.01
	Angle	30.0						
	Head-On Left-Turn	10.0						
44th Street @ Clay Ave	Rear-End Straight	40.0	45	13.3	11	35,000	1.21	1.17
	Side-Swipe Same	24.4						
	Angle	17.8						
44th Street @ Buchanan Avenue	Angle	38.9	18	33.3	6	34,400	1.21	0.48
	Rear-End Straight	27.8						
	Head-On Left-Turn	11.1						
44th Street @ Division Avenue	Rear-End Straight	48.8	84	29.8	29	49,700	1.14	1.54
	Side-Swipe Same	15.5						
	Angle	14.3						
44th Street @ Roger B Chaffee Blvd	Rear-End Straight	44.4	18	44.4	8	26,300	1.43	0.63
	Side-Swipe Same	16.7						
	Fixed Object	11.1						
52nd Street @ Wilson Avenue	Rear-End Straight	40.0	10	10.0	1	23,150	1.43	0.39
	Side-Swipe Same	30.0						
	Angle	10.0						
52nd Street @ Ivanrest Avenue	Angle	42.9	14	21.4	4	17,050	1.70	0.75
	Rear-End Straight	21.4						
	Head-On Left-Turn	14.3						
52nd Street @ Byron Center Avenue	Head-On Left-Turn	31.8	22	36.4	11	28,550	1.43	0.70
	Rear-End Straight	27.3						
	Angle	27.3						
52nd Street @ Burlingame Avenue	Angle	45.5	11	45.5	6	14,550	1.70	0.69
	Rear-End Straight	18.2						
	(4 Types)	9.1						
56th Street @ Wilson Avenue	Rear-End Straight	44.4	9	11.1	2	21,400	1.43	0.38
	Fixed Object	22.2						
	Head-On Left-Turn	11.1						
56th Street @ Gezon Parkway	Rear-End Straight	25.0	44	13.6	6	34,950	1.21	1.15
	Angle	25.0						
	Head-On Left-Turn	9.1						
Gezon Parkway @ Burlingame Avenue	Rear-End Straight	38.5	13	23.1	3	22,600	1.43	0.53
	Head-On Left-Turn	23.1						
	Angle	15.4						

*MEV – million entering vehicles

**TABLE 2-5
INTERSECTIONS WITH HIGHER-THAN-AVERAGE CRASH RATES (2007-2009)**

Entering ADT Range	Intersection	ADT Entering Intersection	Average Crash Rate (1)	Actual Crash Rate*
30,000– 40,000 ADT	M-11 @ Michael Avenue	36,800	1.21 *	1.81
	M-11 @ Byron Center Avenue	36,650		1.50
	36 th Street @ Clyde Park Avenue	37,350		1.27
40,000– 50,000 ADT	M-11 @ Clyde Park Avenue	44,800	1.14 *	2.00
	M-11 @ Burlingame Avenue	40,250		1.91
	44 th Street @ Clyde Park Avenue	44,800		1.75
	44 th Street @ Division Avenue	49,700		1.54
	M-11 @ Buchanan Avenue	47,350		1.39
	54 th Street @ Gezon Parkway/ Clyde Park Avenue	44,100		1.32
	44 th Street @ Burlingame Avenue	42,900		1.28
	Burton Street @ Clyde Park Avenue	42,300		1.27
Over 50,000 ADT	M-11 @ Division Avenue	56,500	1.18 *	1.79
	44 th Street @ Byron Center Avenue	50,800		1.62
	54 th Street @ Division Avenue	59,000		1.27

(1) Source: Southeast Michigan Council of Governments (SEMCOG), Traffic Safety Manual, 2nd Edition.

* crashes per Million Entering Vehicles (MEV)

2.9 Existing Speed Limits, Truck Routes, Transit Routes, and Non-Motorized Network

The City of Wyoming is modifying the speed limits on its major thoroughfares to comply with Public Act 85 of 2006 and is reviewing the adequacy of its system of truck routes, transit routes, and non-motorized facilities.

Speed Limits

Figure 2-9 shows the speed limit of each of the City's thoroughfares, as modified to comply with Public Act 85 (Michigan Compiled Law, Chapter 257 (Motor Vehicles), Section 627, modified in 2006). Public Act 85 describes new criteria for how speed limits can be established. The prima facie speed limit is based on the number of access points (driveways and intersections), but can otherwise be set higher or lower if an engineering study determines it is appropriate to do so. Most drivers will drive at a speed that enables them to safely respond to potential roadside hazards, so engineering studies typically use the 85th-percentile speed (the speed such that 85% of motorists drive at that speed or lower) to determine what a reasonable speed limit should be. There are various exceptions that allow for fixed lower speed limits (such as in platted residential areas or in the vicinity of schools).

The City of Wyoming posts the speed limit on each city thoroughfare based on engineering studies that determine the 85th-percentile speed.

Truck Routes

Figure 2-10 shows the City's truck route system.

Transit Routes

Figure 2-11 shows the City's current transit route system, which is a fixed-route transit service.

The City of Wyoming is a partner in the Interurban Transit Partnership (ITP) through which the city receives transit bus service called *The Rapid*. In early 2007, the Rapid completed a preliminary study that involved a review of numerous transportation corridors in the Grand Rapids area for application of major public transportation investment. Division Avenue from 60th Street in Wyoming/Kentwood border northerly into downtown Grand Rapids surfaced as the preferred route for application of Bus Rapid Transit (BRT) to connect a highly-transit-dependent population with major employment centers in downtown Grand Rapids. BRT is a system of buses traveling on a corridor in such a way as to emulate the speed, comfort, and convenience of a rail transit system.

Division Avenue BRT would allow “buses only” in the outside lane of Division Avenue during certain peak hours (AM and PM). Passenger cars and trucks would be able to use the outside lane only to turn right at key intersections. Studies are currently ongoing in order to determine what impact the use of “bus only” lanes will have on the capacity of Division Avenue.

Each bus and a handful of signalized intersections along Division Avenue would be outfit with “transit signal priority” technology which would allow the extension of the green signal phase in order to ensure that the bus makes it through the cross-street intersection. The extra green time would be taken from the side street green time. The intersections at Burton Street, 28th Street (M-11), 36th Street, 44th Street, and 54th Street would likely be excluded from using transit signal priority due to the heavy volumes of traffic on these east-west thoroughfares which lack any excess green time during peak hours.

The Division Avenue BRT project is included in the Rapid’s Regional Long Range Plan. The Rapid has applied for federal funding from the Federal Transit Administration (FTA), since the Division Avenue BRT meets the requirements of the FTA’s Very Small Starts Program. The total project cost (minus the cost of BRT vehicles) is \$36.3 million. An Environmental Assessment for Division Avenue BRT is currently being completed, which will be closely followed by preliminary design.

Non-Motorized Network

Like many cities, the City of Wyoming has been built to accommodate mobility patterns that are best supported by the automobile. Additional non-motorized connections within and through the City are desirable. Based on feedback from City personnel, there are a substantial number of non-motorized users that are primarily recreational in nature. Non-motorized users that do not have other available mobility alternatives are more prevalent in the dense, urban portion of Wyoming, and less common in the suburban and rural areas of the City. While there is typically a sociological and economic relationship that influences the number of non-motorized users who have no other mobility options, more households may choose to use non-motorized facilities if and when future improvements are made to the non-motorized network.

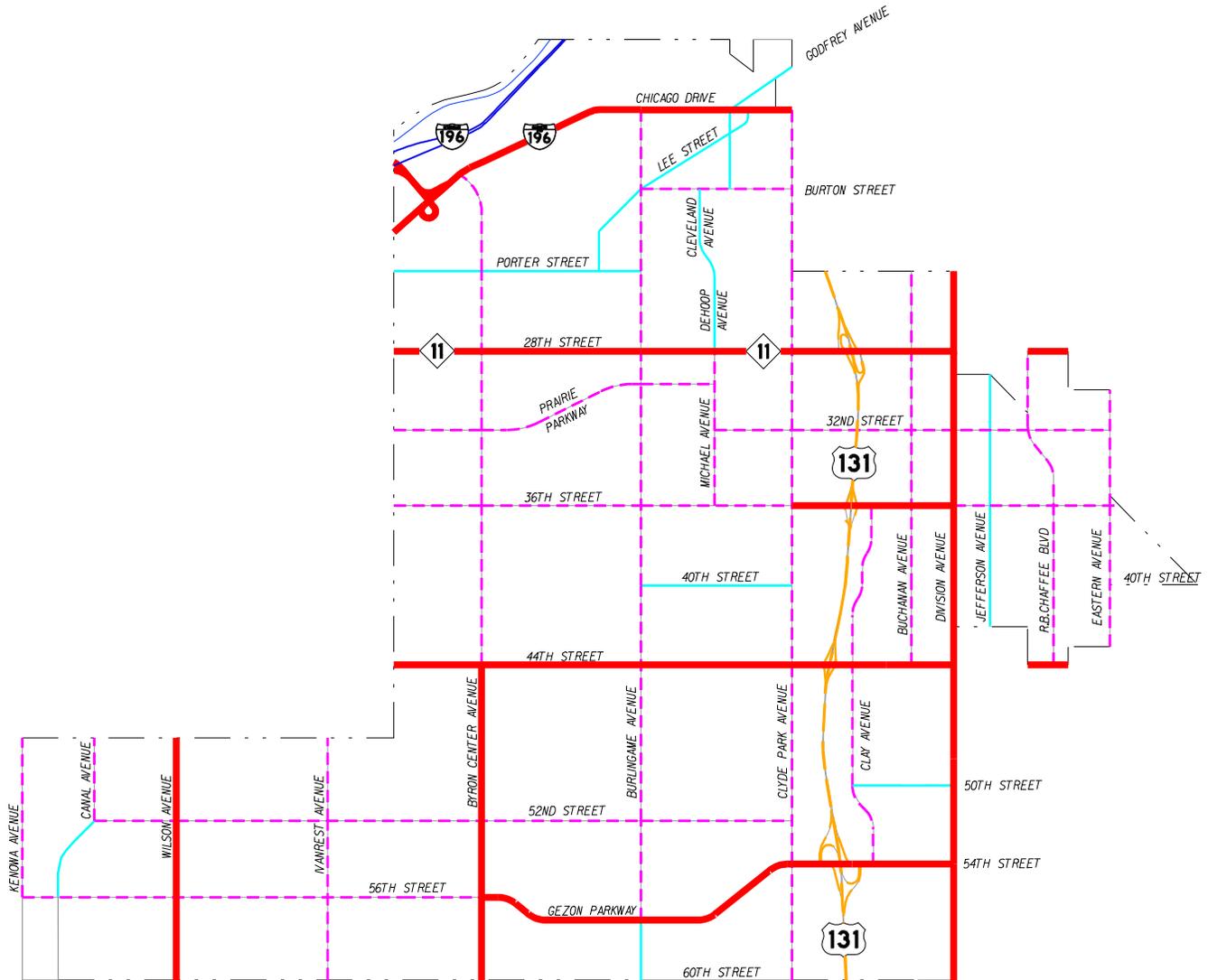
The City has not completed a formal process of detailing the long-term non-motorized needs; however, the City has capitalized on several opportunities to provide recreational facilities, including the Kent Trails, the Interurban Trail, and Buck Creek Trail.

Figure 2-12 shows the existing non-motorized facilities within and nearby the City. Most of the City’s residential streets and major thoroughfares are lined with sidewalks.

The State of Michigan became the fourteenth state to enact “complete streets” legislation when Public Acts 134 and 135 of 2010 were signed into law in August. The legislation requires the needs of pedestrians, bicyclists, people with disabilities, and transit users to be considered in all roadway projects. The legislation also acknowledges that road planning needs vary depending on the setting (rural, urban, suburban) and that cost factors must also be considered.

LEGEND:

- INTERSTATES
- OTHER FREEWAYS
- OTHER PRINCIPAL ARTERIALS
- - - MINOR ARTERIALS
- COLLECTORS
- LOCAL STREETS



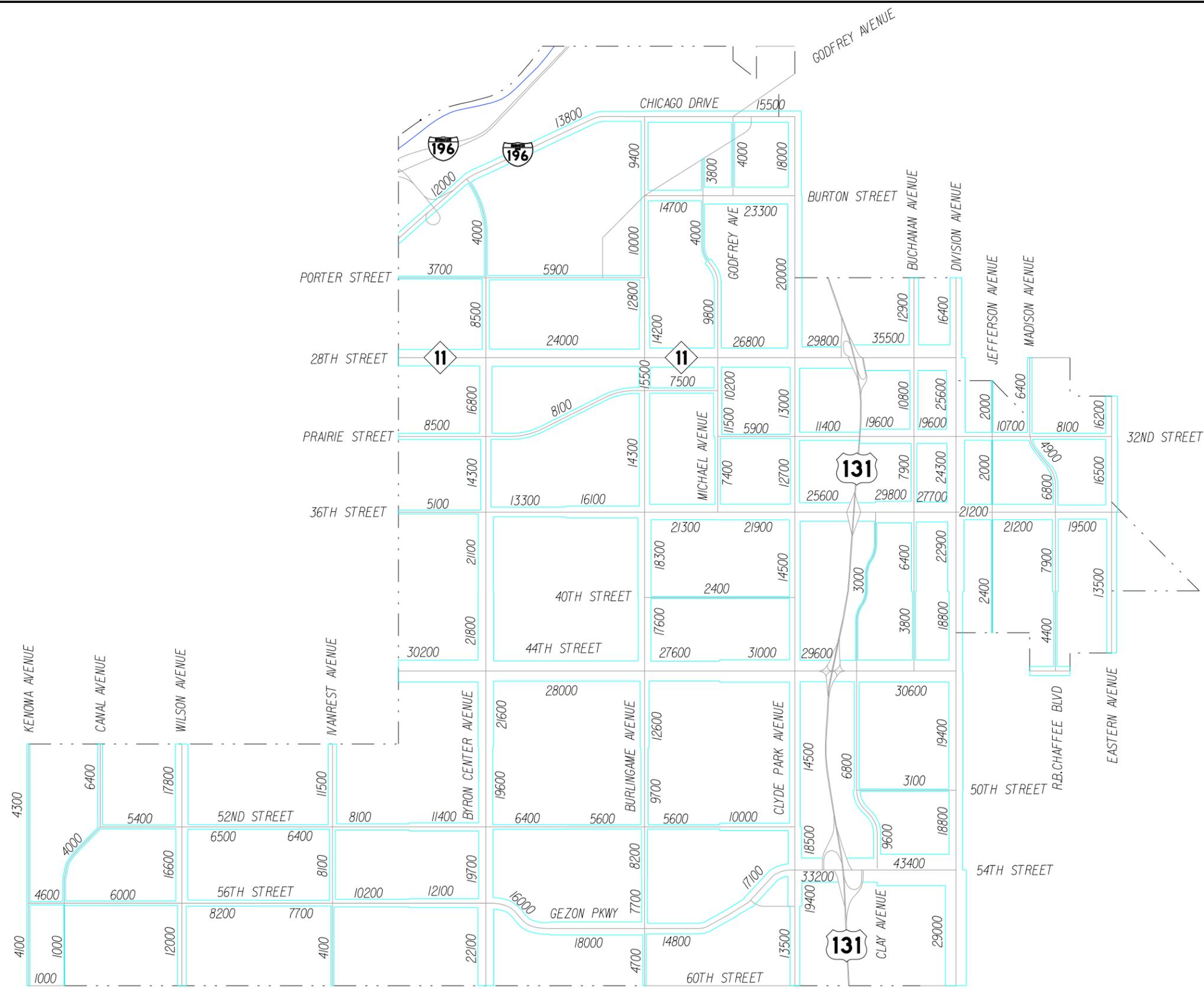
CITY OF WYOMING 2035 THOROUGHFARE PLAN



URS

**EXISTING
FUNCTIONAL CLASSIFICATION**

**FIGURE
2-1**



NOTES

- 1) 2008 ADT DATA USED IN PLACE OF 2009 ADT DATA ON 44TH STREET DUE TO MAJOR CONSTRUCTION AT THE US-131/44TH STREET INTERCHANGE
- 2) 2008 ADT DATA USED WHERE 2009 ADT DATA NOT AVAILABLE

DATA SOURCES:
CITY OF WYOMING, MDT, URS CORPORATION

CITY OF WYOMING 2035 THOROUGHFARE PLAN

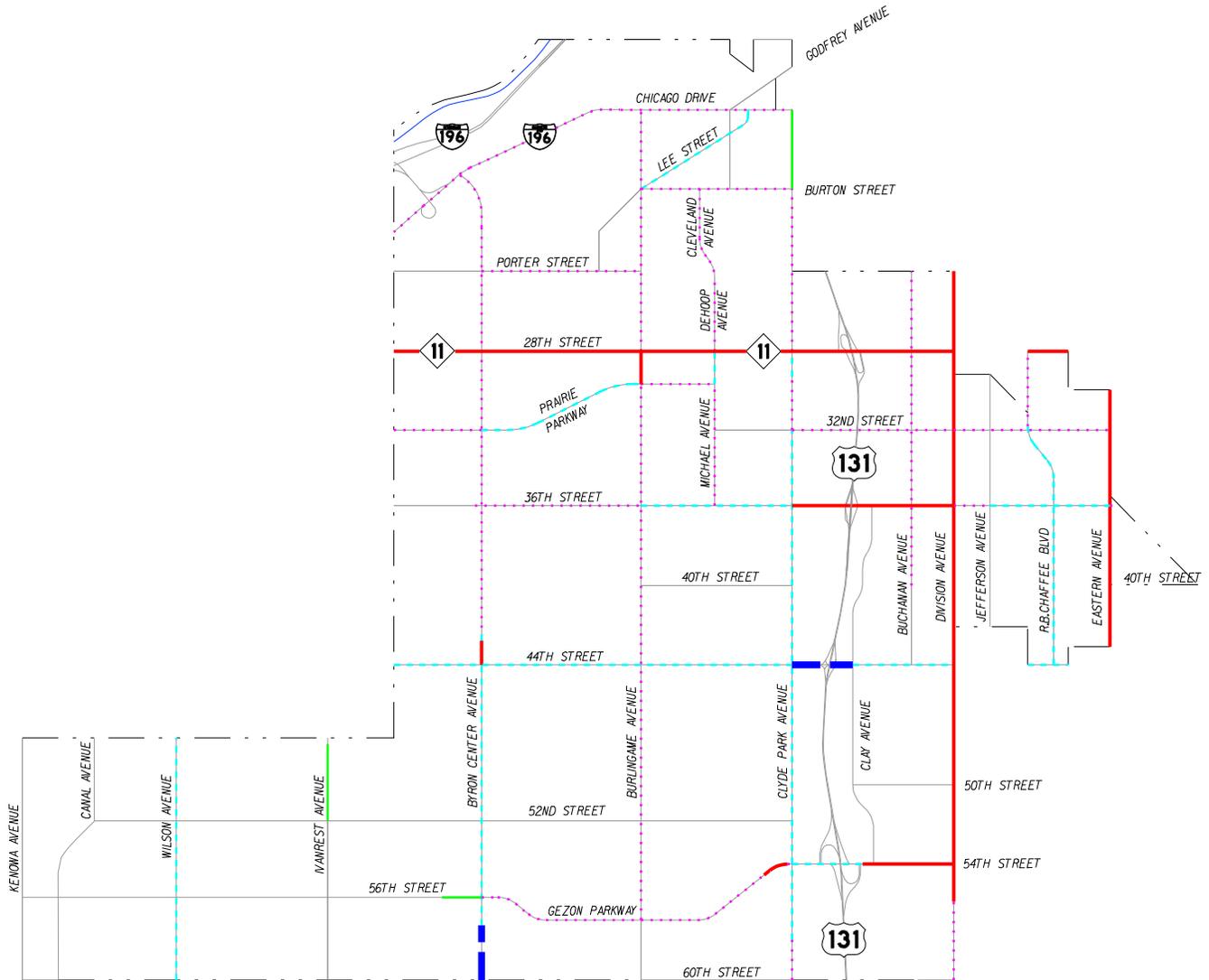


**EXISTING (2009)
AVERAGE DAILY TRAFFIC**

**FIGURE
2-2**

LEGEND:

- 6-LANE DIVIDED
- 5-LANE UNDIVIDED
- - - 4-LANE DIVIDED
- - - 4-LANE UNDIVIDED
- 3-LANE UNDIVIDED
- 2-LANE UNDIVIDED



CITY OF WYOMING 2035 THOROUGHFARE PLAN



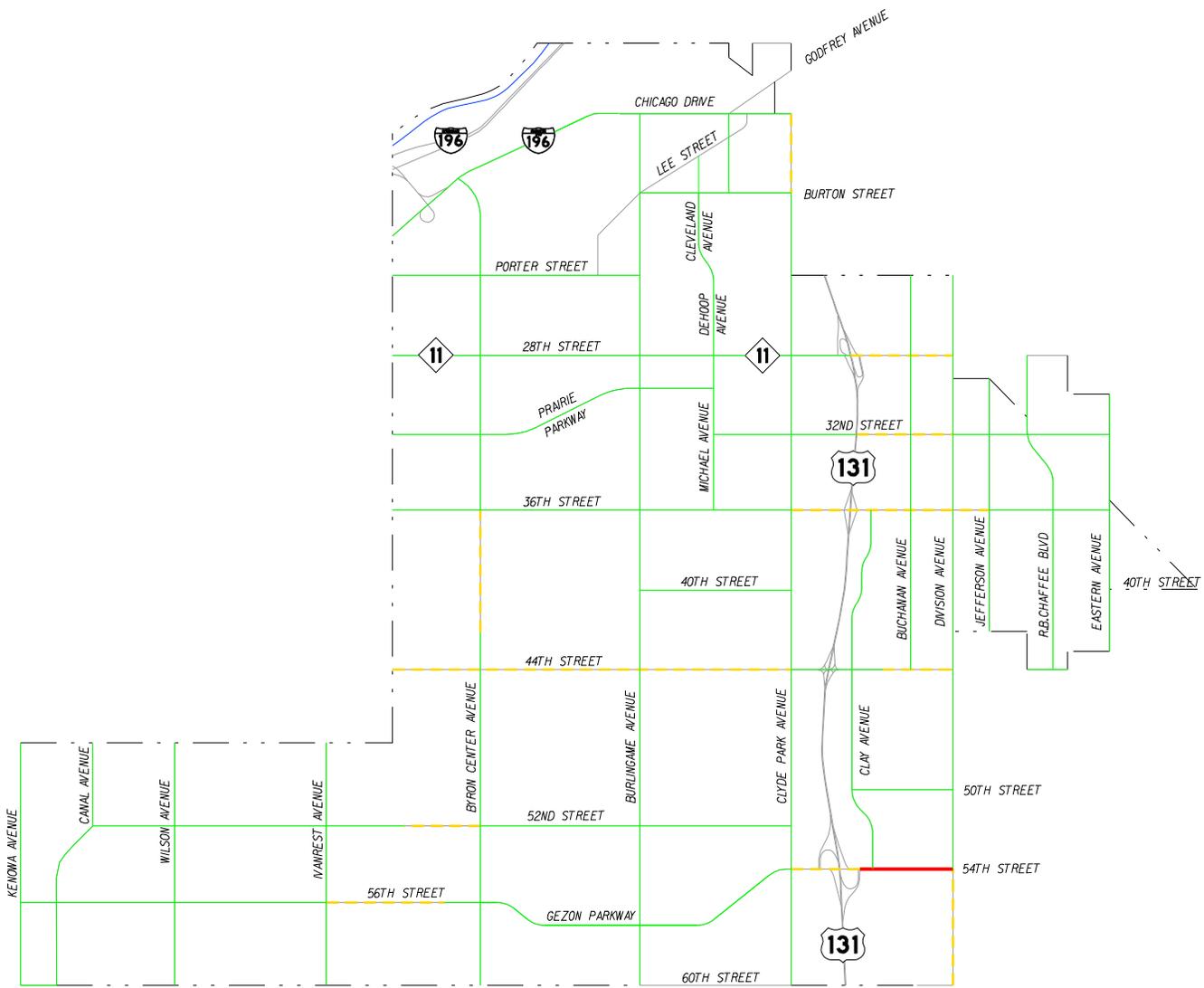
URS

EXISTING NUMBER OF LANES

**FIGURE
2-3**

LEGEND:

	LOS E-F
	LOS D
	LOS A-C



CITY OF WYOMING 2035 THOROUGHFARE PLAN

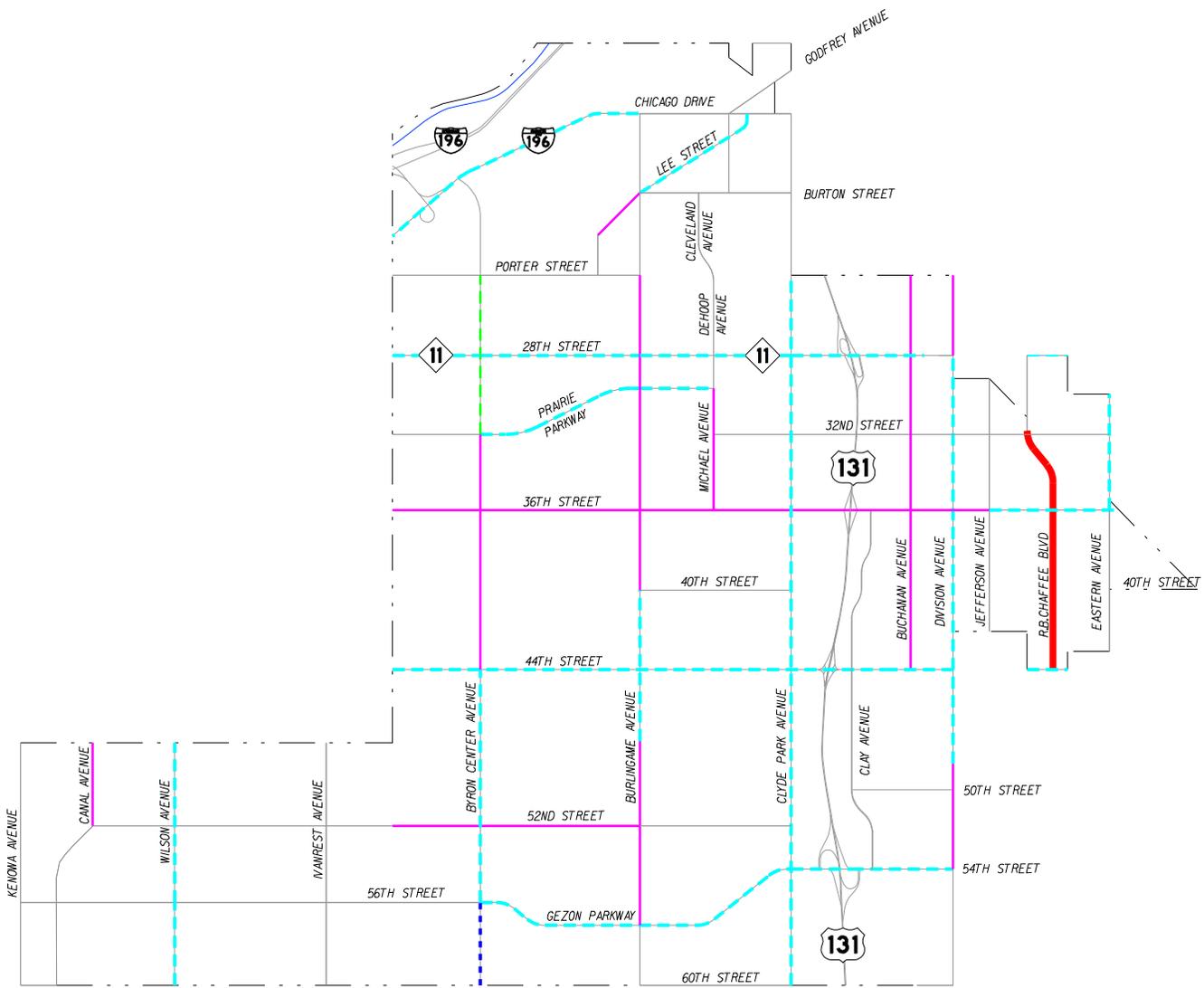


EXISTING (2009) PEAK-HOUR
LEVEL OF SERVICE

FIGURE
2-4

LEGEND:

	150 FEET
	125 FEET
	100 FEET
	80 FEET
	73 FEET
	66 FEET



CITY OF WYOMING 2035 THOROUGHFARE PLAN



EXISTING RIGHT OF WAY

FIGURE
2-5



LEGEND:	
●	SIGNALIZED INTERSECTION
■	SIGNALIZED 4-WAY STOP

CITY OF WYOMING 2035 THOROUGHFARE PLAN

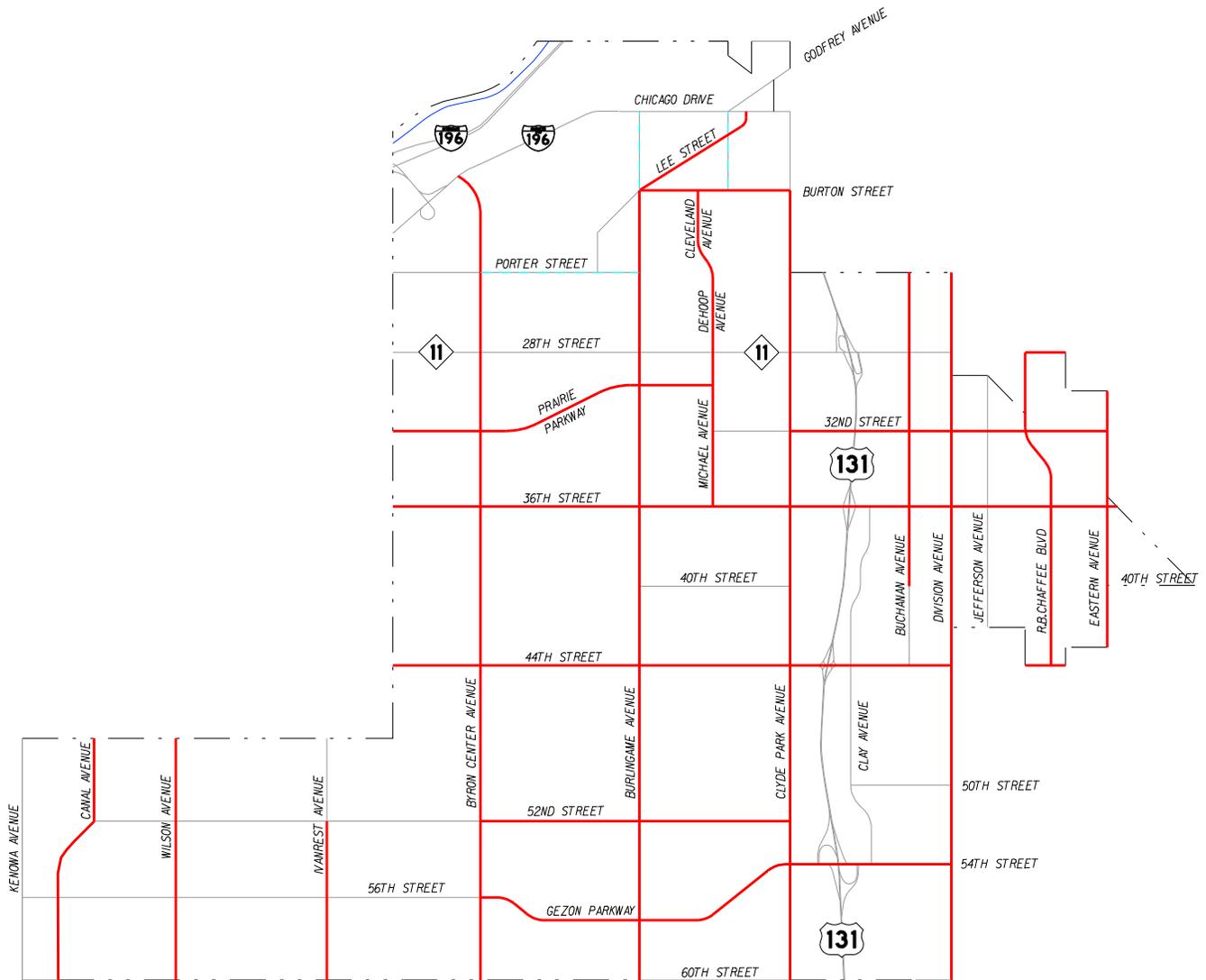


EXISTING TRAFFIC SIGNALS

FIGURE 2-6

LEGEND:

- NO PARKING AT ANY TIME
- - - NO PARKING (6-9AM, 3-6PM, M-F)
- PARKING RESTRICTIONS ARE NOT POSTED



CITY OF WYOMING 2035 THOROUGHFARE PLAN



URS

EXISTING PARKING RESTRICTIONS

**FIGURE
2-7**



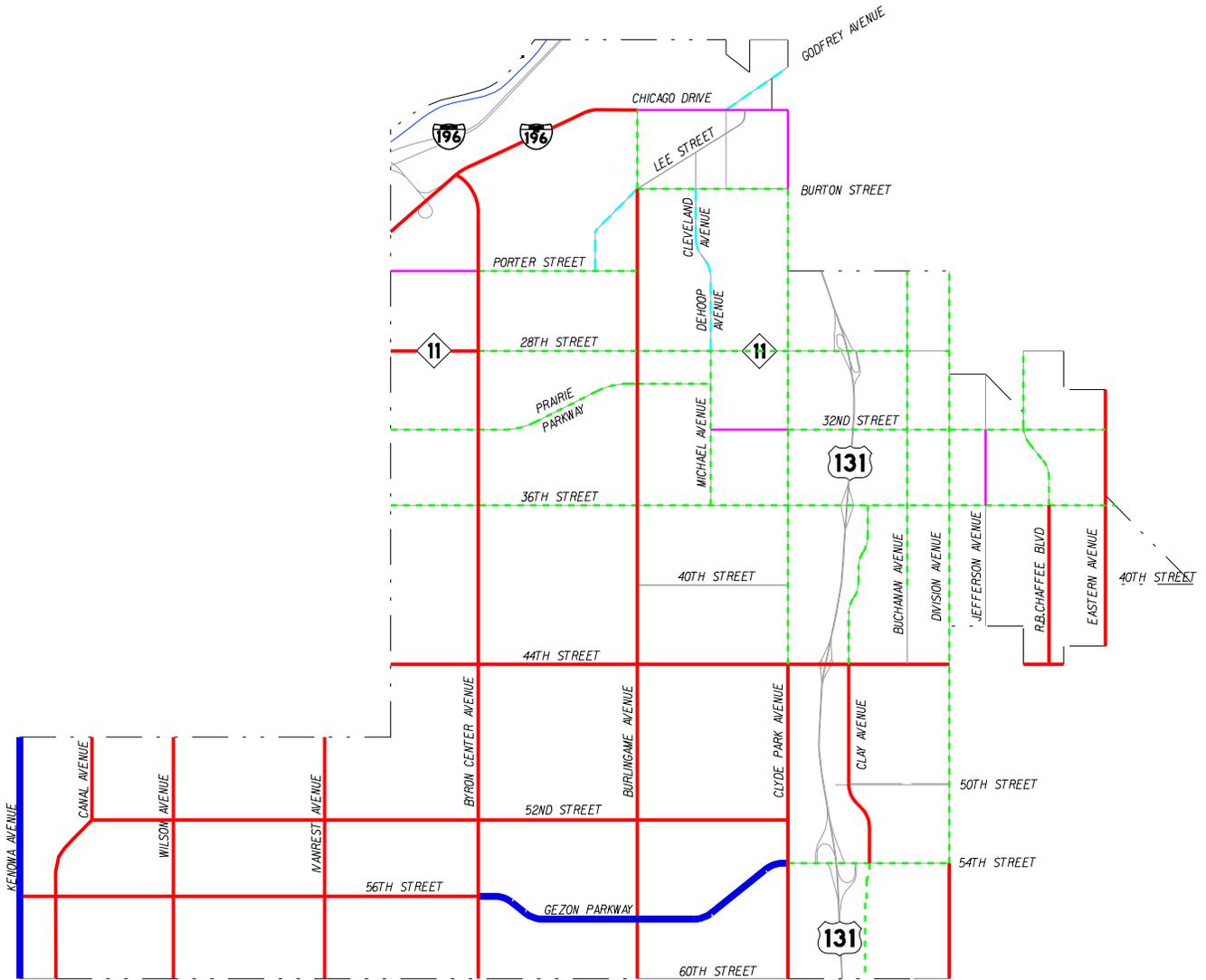
LEGEND:	
●	SIGNALIZED INTERSECTION
■	SIGNALIZED 4-WAY STOP
○	HIGHER-THAN-AVERAGE CRASH RATE (SEE TABLE 2.5)

CITY OF WYOMING 2035 THOROUGHFARE PLAN

			<p>INTERSECTIONS WITH HIGHER-THAN-AVERAGE CRASH RATES</p>	<p>FIGURE 2-8</p>
--	--	--	------------------------------------------------------------------	--------------------------

LEGEND:

- SPEED LIMIT 50
- SPEED LIMIT 45
- - - SPEED LIMIT 40
- SPEED LIMIT 35
- - - SPEED LIMIT 30
- SPEED LIMIT 25



NOTE:
I-196 AND US-131 SPEED LIMIT IS 70 MPH.

CITY OF WYOMING 2035 THOROUGHFARE PLAN

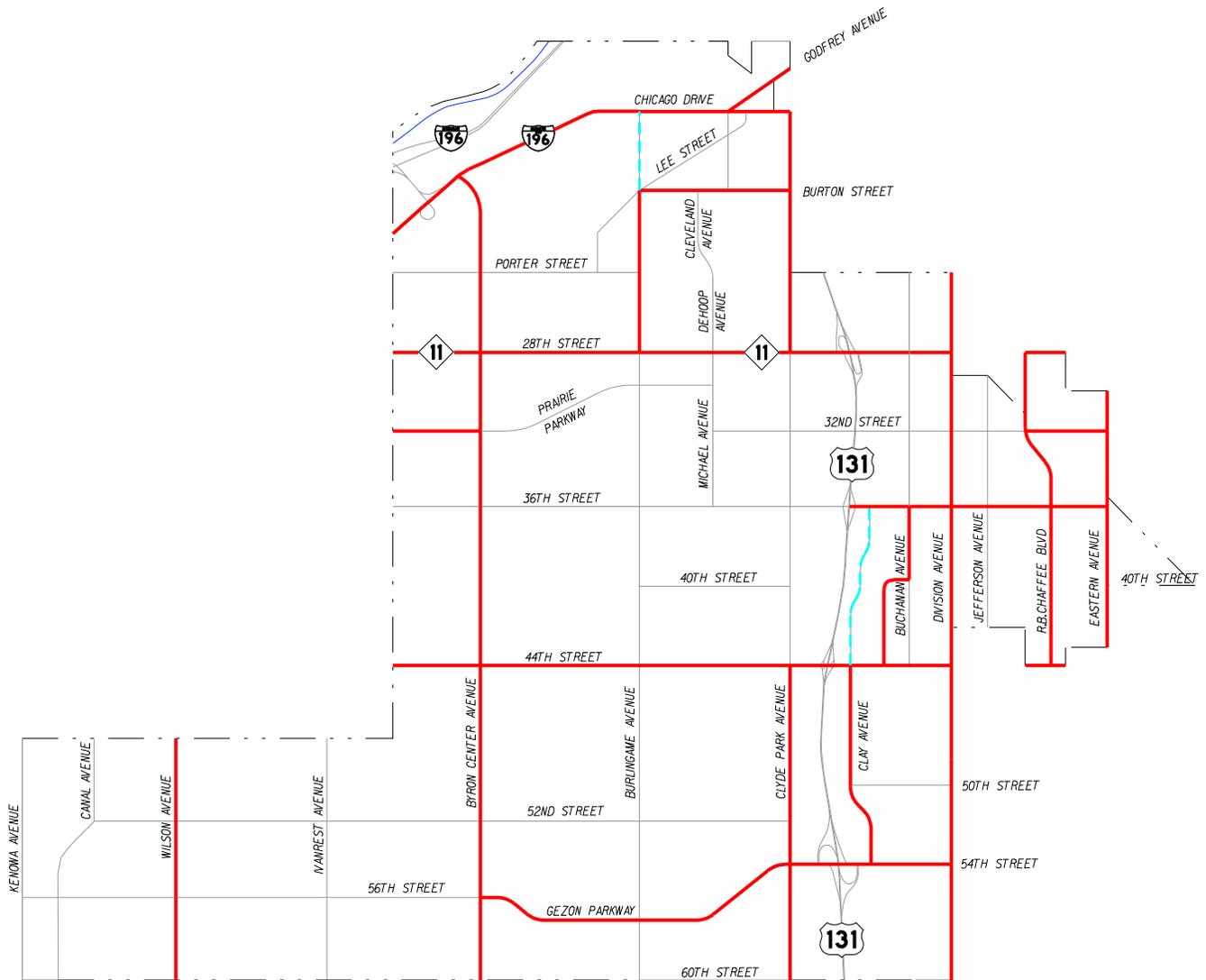


EXISTING SPEED LIMITS

FIGURE
2-9

LEGEND:

- TRUCK ROUTE
- - - TRUCK ROUTE (6AM - 7PM)

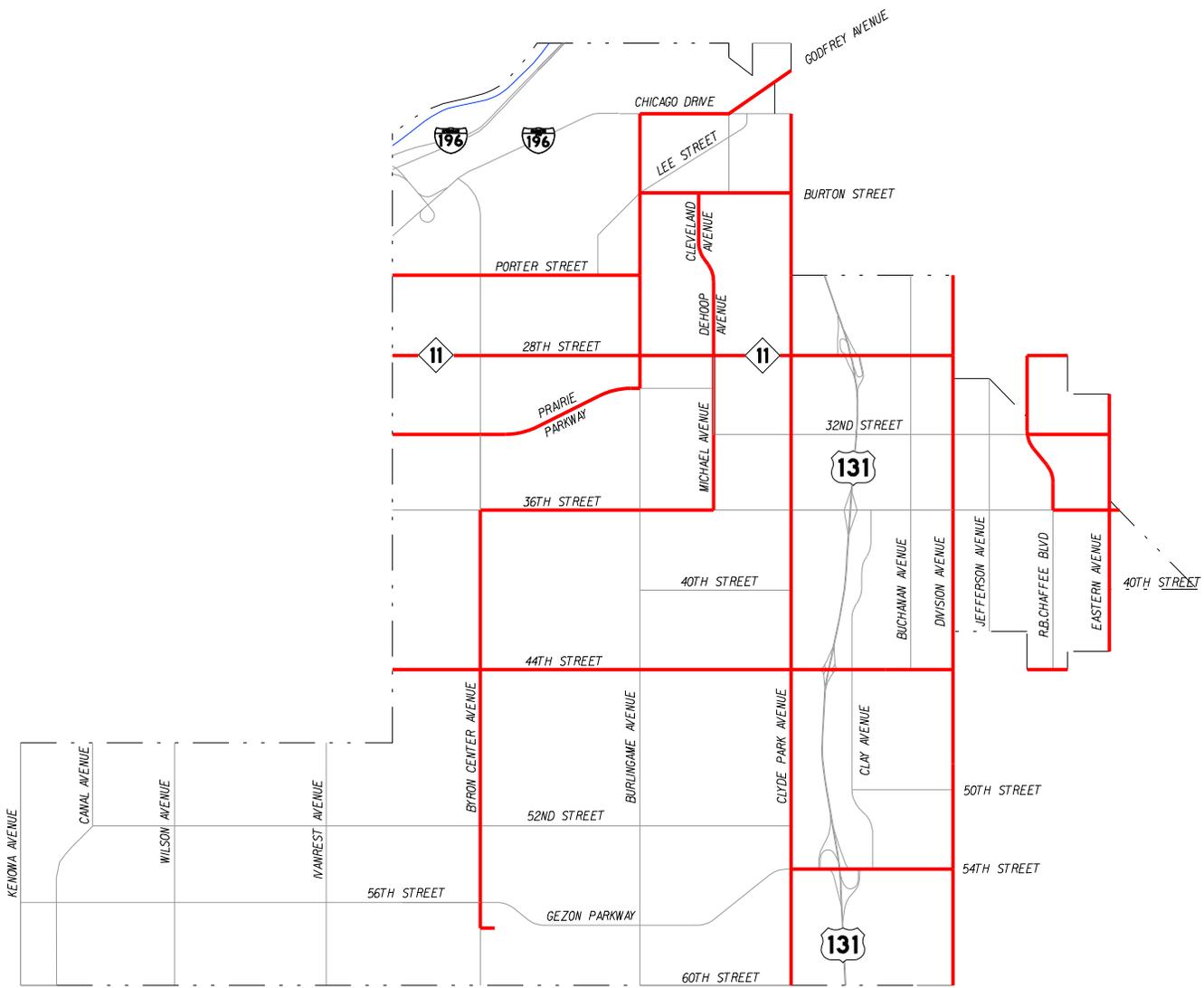


CITY OF WYOMING 2035 THOROUGHFARE PLAN



EXISTING TRUCK ROUTES

FIGURE
2-10

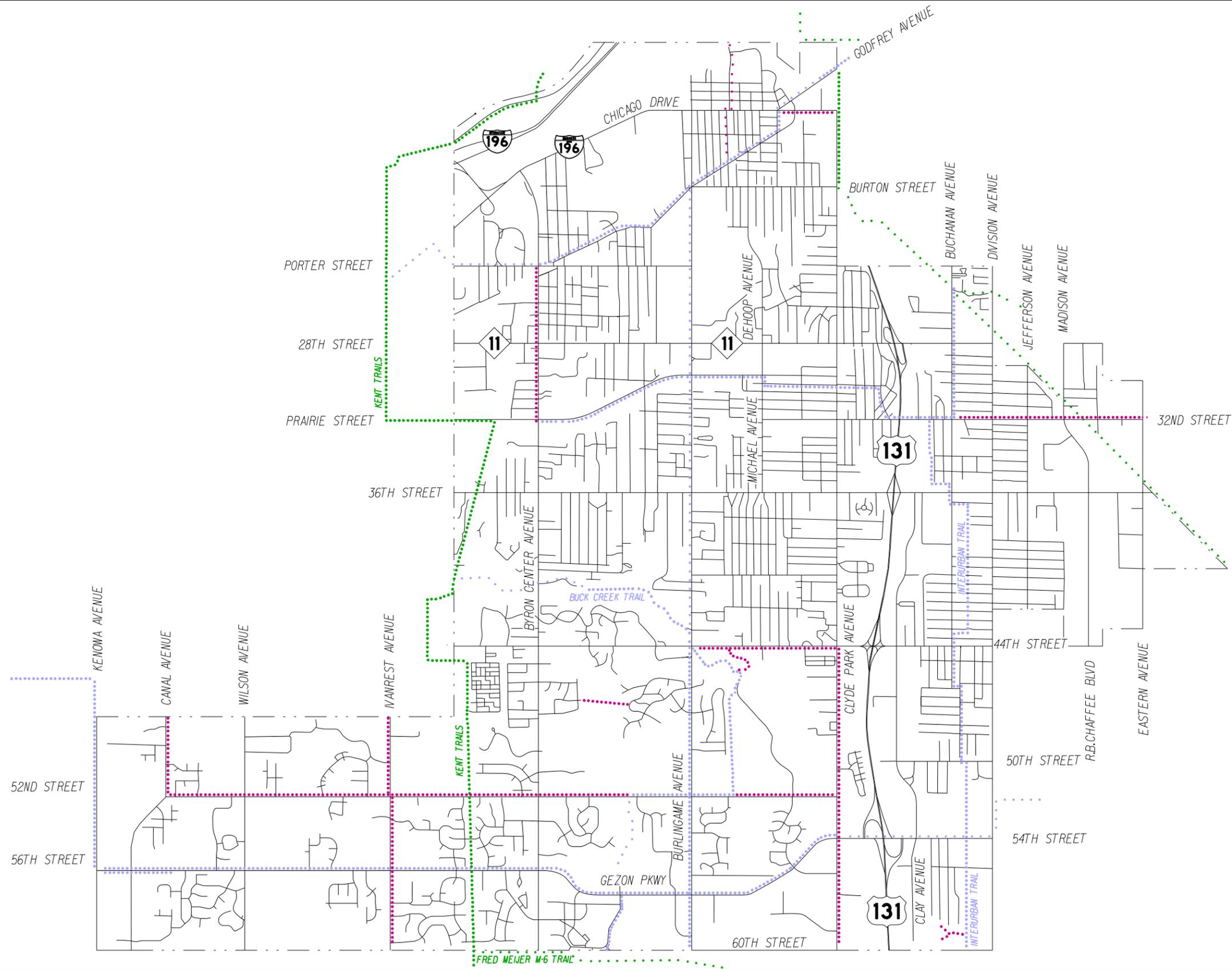


LEGEND:
 TRANSIT ROUTE (THE RAPID)

SOURCE: INTERURBAN TRANSIT PARTNERSHIP

CITY OF WYOMING 2035 THOROUGHFARE PLAN

			<h2>EXISTING TRANSIT ROUTES</h2>	FIGURE 2-11
-------------------------------------------------------------------------------------	-------------------------------------------------------------------------------------	-------------------------------------------------------------------------------------	----------------------------------	------------------------------



LEGEND

- - - - - REGIONAL EXISTING
 - · - · - REGIONAL PROPOSED
- - - - - LOCAL MAJOR EXISTING
 - · - · - LOCAL MAJOR PROPOSED
- - - - - LOCAL MINOR EXISTING
 - · - · - LOCAL MAJOR PROPOSED

DATA SOURCES: CITY OF WYOMING, URS

CITY OF WYOMING 2035 THOROUGHFARE PLAN



URS

**EXISTING
NON-MOTORIZED FACILITIES**

**FIGURE
2-12**

3.0 FUTURE (2035) CONDITIONS

This section contains an analysis of design year (2035) traffic operations and Level-of-Service on City of Wyoming thoroughfares. The transportation planning model developed by the Grand Valley Metro Council (GVMC) for the Grand Rapids metropolitan area was used to project traffic volumes in the City of Wyoming for the year 2035. The GVMC planning model utilizes future land-use projections and socioeconomic data to estimate the number of trips generated on each link in the roadway network. The GVMC planning model projections are calibrated against a network containing actual 2009 traffic volumes.

3.1 Future Year (2035) Traffic Projections

The GVMC planning model network analysis of Base Case conditions assumes that the laneage of all city thoroughfares is the same as existing (2009) laneage. The model also assumes implementation of Bus Rapid Transit along the Division Avenue corridor as discussed on page 2-6. Future land use data for the City of Wyoming was supplied to GVMC by the City prior to calibration of the planning model by GVMC. The Average Daily Traffic (ADT) projections on City of Wyoming thoroughfares for the analysis of Base Case conditions are shown in **Figure 3-1**. Projections were developed by comparing base (2009) model and future (2035) model ADT volumes and applying the percent change in model ADT to actual (2009) traffic volumes.

Table 3-1 depicts a comparison of existing (2009) ADT values against projected (2035) ADT values for a variety of thoroughfares in the city. A review of Table 3-1 indicates that much of the growth in traffic volumes is anticipated in the southern and western parts of the city where residential, commercial, and industrial development is ongoing. Traffic volumes along thoroughfares such as Wilson Avenue, Ivanrest Avenue, Byron Center Avenue, Gezon Parkway, and 56th Street are projected to experience the greatest levels of traffic growth. Thoroughfares in the already-urbanized sections of the city are anticipated to experience much lower growth in traffic volumes, with the exception of Division Avenue. Traffic volumes on Division Avenue are anticipated to increase due to the presence of BRT and the associated transit-oriented development which is anticipated along the Division Avenue corridor.

**TABLE 3-1
TRAFFIC VOLUME COMPARISON ON VARIOUS THOROUGHFARES (2009 TO 2035)**

Thoroughfare	Location	Actual ADT (2009)	Projected ADT (2035)*	% Change
Wilson Avenue	Just south of 52 nd Street	16,600	22,200	34%
56 th Street	Just west of Byron Center Avenue	12,100	16,100	33%
Ivanrest Avenue	Just north of 52 nd Street	11,500	14,100	23%
Gezon Parkway	Just east of Byron Center Avenue	16,000	19,200	20%
36 th Street	Just east of US-131	25,600	30,600	20%
Division Avenue	Just south of 36 th Street	22,900	27,100	18%
Byron Center Avenue	Just south of 52 nd Street	19,700	22,800	16%
54 th Street	Just east of Clay Avenue	43,400	50,000	15%
Eastern Avenue	Just south of 36 th Street	13,500	15,500	15%
M-11	Just west of Byron Center Avenue	24,000	27,200	13%
44 th Street	Just east of US-131	29,600	33,400	13%
Clyde Park Avenue	Just south of 44 th Street	14,500	16,200	12%
Byron Center Avenue	Just north of 36 th Street	14,300	15,400	8%
52 nd Street	Just west of Clyde Park Avenue	10,000	10,700	7%
Burlingame Avenue	Just north of M-11	14,200	14,400	1%

*Source: Grand Valley Metropolitan Council traffic demand model.

3.2 Future Year (2035) Volume-to-Capacity Ratio

The ADT projections shown in Figure 3-1 were used to estimate future year (2035) peak-hour “volume-to-capacity” ratios for each thoroughfare in the City of Wyoming. The volume-to-capacity (v/c) ratio is a measure that can be used to determine whether a thoroughfare is able to service the traffic demand. Ratios of v/c that are greater than 1.0 are indicative of roadways that experience traffic demand that is greater than the road’s capacity. Such roads are in need of capacity improvements (construction of additional through lanes, intersection improvements, etc.). A v/c ratio in the range of 0.8 to 1.0 indicates a roadway that is approaching the need for additional through-lane capacity. Ratios less than 0.8 describe roadways with sufficient capacity. The projected (2035) v/c ratios for the various thoroughfares in the city are displayed in Figure 3-2.

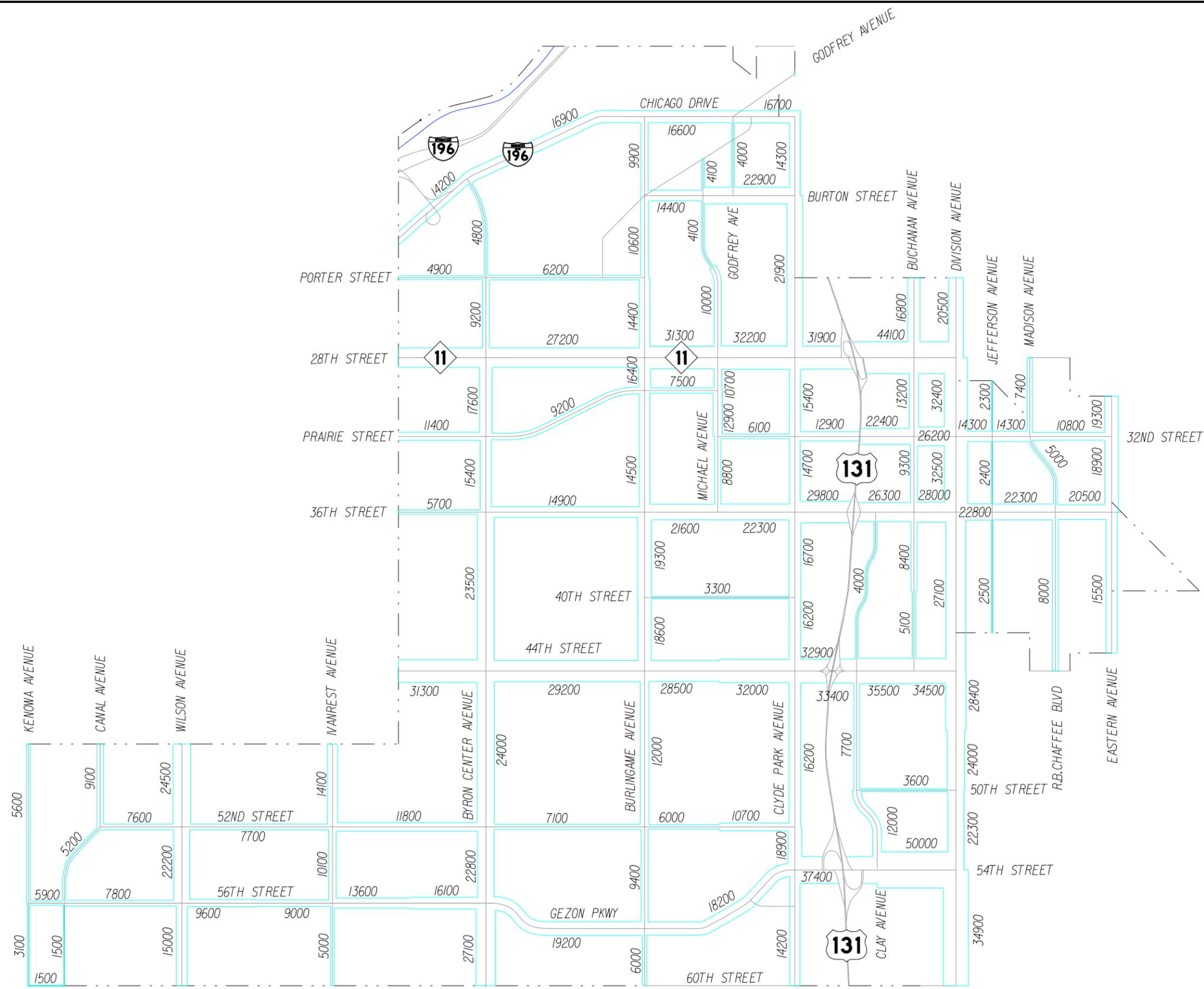
Roadway segments projected to operate with v/c ratios greater than 0.9 are also listed in Table 3-2. Segments with a v/c ratio exceeding 1.0 are shaded and bolded in Table 3-2. The roadway segments with projected v/c ratios greater than 1.0 are in the greatest need of capacity improvement.

**TABLE 3-2
ROADWAY SEGMENTS WITH PROJECTED (2035) v/c RATIOS GREATER THAN 0.9**

Thoroughfare	Segment	Roadway Type*	ADT (2035)	v/c ratio
EAST – WEST THOROUGHFARES				
M-11 (28 th Street)	Michael Avenue to Clyde Park Avenue	5L-U	26,800	0.93
	Clyde Park Avenue to US-131		29,800	0.92
	US-131 to Buchanan Avenue		35,500	1.27
	Buchanan Avenue to Division Avenue		35,700	1.27
32 nd Street	Buchanan Avenue to Division Avenue	4L-U	26,200	0.99
44 th Street	Burlingame Avenue to Clyde Park	4L-D	32,000	0.92
	Clay Avenue to Buchanan Avenue		35,500	1.02
	Buchanan Avenue to Division Avenue		34,500	0.99
52 nd Street	Ivanrest Avenue to Byron Center Avenue	2L-U	11,800	0.98
54 th Street	Clyde Park Avenue to US-131	4L-D	37,400	1.07
	US-131 to Clay Avenue	5L-U	37,400	1.07
	Clay Avenue to Division Avenue		50,000	1.44
56 th Street	Ivanrest Avenue to Byron Center Avenue	2L-U / 3L-U	16,100	1.33
NORTH – SOUTH THOROUGHFARES				
Ivanrest Avenue	52 nd Street to North City Limit	3L-U	14,100	1.16
Clay Avenue	54 th Street to 50 th Street	2L-U	12,000	0.99
Division Avenue	60 th Street to 54 th Street	5L-U / 4L-U	34,900	1.32
	36 th Street to 32 nd Street	5L-U	32,500	0.93
	32 nd Street to 36 th Street		32,400	0.93

*L = Lane, D = Divided, U = Undivided (5L-U = 5-Lane Undivided)

As shown in Table 3-2, various segments of M-11 (28th Street), 44th Street, 54th Street, 56th Street, Ivanrest Avenue, and Division Avenue are in need of some form of capacity improvement in order to reduce the projected (2035) v/c ratio to something below 1.0.



NOTES
 1) 2035 ADT PROJECTIONS WERE DEVELOPED BY COMPARING BASE (2009) AND FUTURE (2035) MODEL ADT VOLUMES AND APPLYING THE PERCENT CHANGE IN MODEL ADT TO ACTUAL (2009) TRAFFIC VOLUMES.

CITY OF WYOMING 2035 THOROUGHFARE PLAN

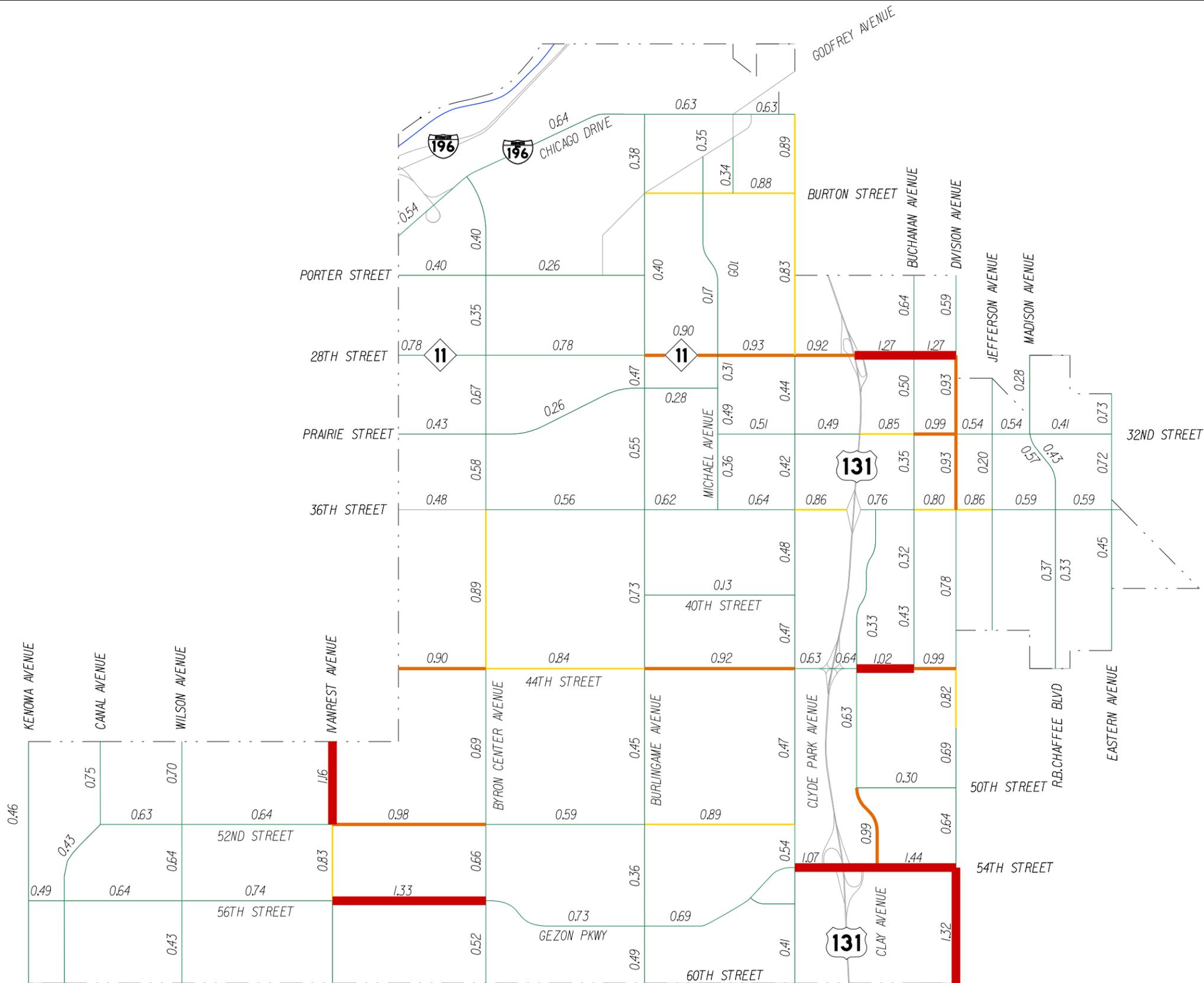


**DESIGN YEAR (2035)
 AVERAGE DAILY TRAFFIC**

**FIGURE
 3-1**

V/C RATIO LEGEND:

- >= 1.00
- 0.90 - 1.00
- 0.80 - 0.90
- < 0.80



NOTES

CITY OF WYOMING 2035 THOROUGHFARE PLAN



URS

DESIGN YEAR (2035)
V/C RATIO MAP

FIGURE
3-2

4.0 2035 Thoroughfare Plan

This section contains the recommended improvements for the City of Wyoming *2035 Thoroughfare Plan*. The items included in the *2035 Thoroughfare Plan* are a result of the existing conditions and future conditions analyses included in previous sections of this document. The Plan includes the expansion of some thoroughfares to increase capacity and improve Level-of-Service. The Plan also includes recommendations for development of a more comprehensive non-motorized plan. The recommended 2035 Thoroughfare Plan is depicted in **Figure 4-1** and is discussed herein.

4.1 Recommended Expansion of City Thoroughfares

The City desires to plan for the expansion of those thoroughfares that would otherwise operate at poor Levels-of-Service during the year 2035. Those thoroughfares which require additional capacity are discussed herein.

56th Street

56th Street should be considered for widening as a 4-lane undivided roadway from Ivanrest Avenue to Byron Center Avenue, matching the existing 4-lane undivided roadway cross-section of Gezon Parkway east of Byron Center Avenue. Projected (2035) ADT along 56th Street is anticipated to exceed 16,000 vehicles per day.

Ivanrest Avenue

Ivanrest Avenue should be considered for widening as a four-lane undivided roadway north of 52nd Street in order to serve the projected (2035) ADT of 14,100 vehicles per day. Widening into a 3-lane undivided section between 56th Street and 52nd Street may also be needed if traffic grows at a greater rate than projected by the GVMC model, which is possible considering the recommended widening of 56th Street between Ivanrest Avenue and Byron Center Avenue.

54th Street

54th Street is the city's highest volume thoroughfare and is projected to have the greatest ADT under future year (2035) conditions. The congested intersection at 54th Street/Clay Avenue reduces the overall capacity of the 54th Street corridor, as it acts as a bottleneck during peak hours. The close proximity of the intersection to the northbound US-131 ramps exacerbates the situation.

54th Street should be considered for widening from some point east of Clay Avenue to the northbound US-131 ramps. Specifically, a third westbound through lane is recommended with additional turn-lane capacity at the 54th Street/Clay Avenue intersection. Interconnection of the traffic signals along 54th Street at Clay Avenue and the northbound US-131 ramp signal is recommended as part of this effort. Widening of 54th Street between Haughey Avenue and Division Avenue is not recommended, nor is widening of 54th Street recommended west of US-131.

44th Street

Traffic volumes have fallen or stabilized along the 44th Street corridor due to the opening of the M-6 freeway in 2004 and with completion of Gezon Parkway in the late 1990's. While the previous Thoroughfare Plan included widening of 44th Street as a 6-lane boulevard within the city limits, that improvement no longer appears necessary. The most heavily-traveled section of 44th Street occurs within the vicinity of the US-131 interchange. The city and MDOT recently completed a project to reconstruct and modernize the interchange at US-131, a project that included widening of 44th Street in order to carry three lanes in each direction from west of Clyde Park Avenue to east of Clay Avenue.

Roadway capacity along 44th Street is expected to increase in the future, as the City of Wyoming plans to close the median and remove the signal at the Buchanan Avenue intersection. Left-turns between 44th Street and Buchanan Avenue would be completed at median crossovers along 44th Street on each side of Buchanan Avenue. The improvements at the 44th Street/Buchanan Avenue intersection are expected to increase roadway capacity so that the projected (2035) v/c ratio is reduced below 1.0.

No additional capacity improvements along 44th Street are contemplated.

M-11 (28th Street)

Projected (2035) v/c ratios are greater than 1.0 between US-131 and Division Avenue. M-11 is under the jurisdiction of MDOT, as M-11 is a state trunkline highway. MDOT reconstructed M-11 as a 5-lane undivided pavement between Buchanan Avenue and Division Avenue in 2008. The M-11 bridge over US-131 was replaced by MDOT in 2006. The signal timings along the M-11 corridor were optimized in 2009, which has improved traffic signal progression and marginally increased capacity.

MDOT has no plans to widen the M-11 corridor, so only Transportation System Management (TSM) improvements are feasible. It is recommended that MDOT modify the left-turn signal phasing for the eastbound and westbound left-turn movements at Buchanan Avenue by providing permissive/protected phasing (instead of protected-only phasing). Such a phasing change would undoubtedly increase the capacity for the eastbound and westbound left-turn movements. The eastbound and westbound through movements could potentially be given additional green time to improve overall roadway capacity if the amount of protected green time given to the eastbound and westbound left-turn movements could be reduced.

The Downtown Development Authority is beginning to consider changes to the operation of 28th Street in downtown Wyoming (Clyde Park Avenue to Burlingame Avenue). One conceptual cross-section being considered is a unique five-lane section in which the outside lanes are separated from the middle three and provide access to some form of on-street parking. Theoretically, motorists would use the outside lane if they had destinations within the downtown zone, while motorists within the middle three lanes would be for motorists with no mid-block driveway destination. Impacts to capacity and Level of Service would need to be considered as the study moves forward. Approval from MDOT would also be required.

Division Avenue

Major capital improvements to Division Avenue are not recommended to be included in the Thoroughfare Plan, despite what the projected (2035) v/c ratios indicate. The proposed Bus Rapid Transit (BRT) system along Division Avenue and elsewhere will undoubtedly impact traffic operations along Division Avenue during peak-hour drive times. The long-term impact of BRT is difficult to project, given the few operating BRT corridors in the United States and the present state of the economy. While BRT is intended to spur economic growth, which would mean additional trips along Division Avenue, it remains to be seen what the demand for BRT along Division Avenue will be and how BRT will impact the passenger car mode.

The City of Wyoming will continue to work with the Interurban Transit Partnership (ITP) to ultimately launch and operate BRT along Division Avenue. Once the impacts of BRT are more fully recognized, the city will be able to develop a plan for any future capacity improvements along Division Avenue. Given that Division Avenue is primarily a five-lane undivided roadway with commercial land uses running up and down the corridor, it is unlikely that Division Avenue will ever be widened in the future. The City of Kentwood intends to widen the last remaining four-lane undivided segment of Division Avenue north of 60th Street in 2014.

If the proposed BRT system does not move forward, it is anticipated that Division Avenue will maintain sufficient capacity as a five-lane undivided roadway to serve future travel demands.

4.2 Non-Motorized Considerations

Non-motorized planning is particularly important along major corridors where the right-of-way is limited and in high-speed environments. A conscious and deliberate effort to either incorporate non-motorized users within a corridor or to provide an alternative parallel route is important to ultimately provide a practical, safe, comfortable, and functional non-motorized transportation network.

Providing adequate non-motorized facilities will reduce the need for non-motorized users to use traffic-oriented facilities which were not intended to support non-motorized users, particularly in high-speed environments. Pedestrian and bicycle safety varies based on a number of factors, including non-motorized user compliance with the rules of the road and situations when driver expectancy is violated. While the perception of user safety is a critical part of non-motorized facility planning and implementation, user comfort and convenience are equally important aspects of how and why the community may choose to use non-motorized facilities.

Non-Motorized Benefits

A well-conceived non-motorized transportation system may provide the community with the following benefits:

- Improved community sustainability by enhancing transportation options beyond the automobile, particularly for the population segment which is eligible to drive an automobile.
- A transportation network that provides improved connections to common destinations, such as employment, shopping, schools, and places of worship.
- Improved connections to local and regional recreational facilities, which promote healthy lifestyle opportunities.
- Improved walkability and neighborhood connectivity, which increases social interaction and strengthens sense of community.
- Reduced need for parking spaces and vehicle-oriented roadway improvements.
- Reduced air pollution, stormwater pollution, and carbon emissions.

Non-Motorized Planning

Due to the discretionary nature of many non-motorized trips, it is challenging to estimate the latent demand for non-motorized facilities. Adding non-motorized facilities will almost always increase the number of non-motorized users, particularly if the facilities meet specific needs.

A planning process is recommended to identify the corridors that would best serve the non-motorized needs of the community. Once these corridors are identified, then appropriate improvements can be considered with future roadway improvement projects. In general, a non-motorized planning process should include the following steps:

- Engage community stakeholders to determine the destinations and areas that should be particularly served by non-motorized facilities.
- Conduct a field survey to inventory the available right-of-way, existing street width, and evidence of non-motorized users.
- Gauge community preferences about non-motorized facility options, such as on-street bike lanes, shared lanes, and off-street paths.
- Identify corridors that best match the travel paths between destinations that are likely to be accessed by non-motorized users.
- Identify corridors that connect with existing and future recreational paths, such as the Kent Trails.
- Review other non-motorized plans developed by other peer communities and the standard non-motorized design practices.

-
- Identify standard applications for a range of non-motorized facilities that might apply to future projects, such as those projects identified in Section 4.1.

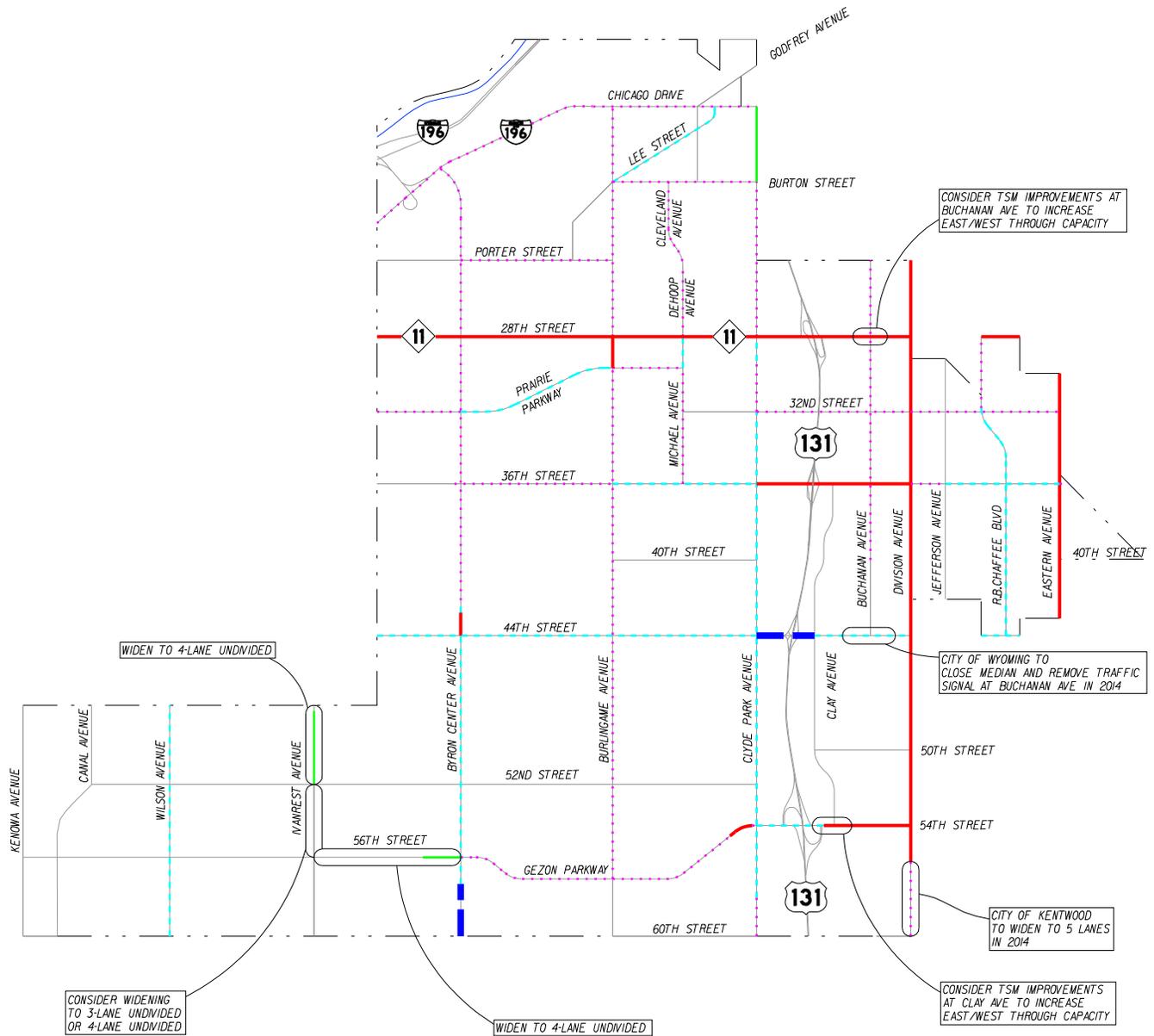
Areas for Consideration

Based on the existing network of non-motorized facilities, the following areas are likely to be the subject of future non-motorized planning:

- Routes to schools.
- Routes to fixed transit routes such as Bus Rapid Transit along Division Avenue.
- North-south connectivity north of 44th Street, which is currently limited to the far east and west edges of the City.
- East-west connectivity across US-131, which is primarily limited to the interchange bridges (there is only one US-131 crossing that is not an interchange—at 32nd Street).
- Connectivity between Prairie Parkway and Chicago Drive.
- Connectivity between 44th Street and Prairie Parkway.

LEGEND (EXISTING LANEAGE):

- 6-LANE DIVIDED
- 5-LANE UNDIVIDED
- - - 4-LANE DIVIDED
- · · 4-LANE UNDIVIDED
- 3-LANE UNDIVIDED
- 2-LANE UNDIVIDED



CITY OF WYOMING 2035 THOROUGHFARE PLAN



URS

POTENTIAL CAPITAL IMPROVEMENT PROJECTS

FIGURE 4-1

RESOLUTION NO. _____

A RESOLUTION TO APPROVE AMENDMENTS TO
THE WYOMING COMMUNITY DEVELOPMENT
2011/2012 ONE YEAR CDBG ACTION PLAN

WHEREAS, the City Council approved a One Year CDBG Action Plan on April 18, 2011, with Resolution #23838, which covers the period from July 1, 2011 through June 30, 2012, and

WHEREAS, on April 22, 2011, the Department of Housing and Urban Development (HUD) informed us that the US Congress had reduced federal spending amounts by 16.5%, giving us a reduction of about \$84,389 to our 2011-2012 CDBG Grant, and

WHEREAS, the Wyoming Community Development Committee met on May 25, 2011, and approved, by consensus vote, the following budget amendments to the 2011-2012 One Year CDBG Action Plan:

1. Acquisition of Floodway Structures (From \$1,000 to \$500).
2. Public Facility Improvements Senior Center and Other Eligible Park Facilities (From \$10,000 to \$0).
3. Public Facility Improvements Hillcroft Park (From \$15,000 to \$10,000).
4. Clearance/Demolition (From \$20,000 to \$10,000)
5. Rehabilitation - Single Unit Residential (From \$150,000 to \$121,101).
6. Rehabilitation - Home Repair Services (From \$75,000 to \$50,000).
7. Contingency Funds (From \$44,760 to \$39,770), and

WHEREAS, a public notice was published on June 21, 2011, allowing citizens, at least, 30 days to comment on the proposed amendments, and

WHEREAS, there has been no public comments submitted to date, on this matter, now, therefore,

BE IT RESOLVED, that the Wyoming City Council does hereby approve the above listed budget amendments related to the 2011-2012 CDBG One Year Action Plan.

Councilmember _____ moved, seconded by Councilmember _____, that the above Resolution be adopted.

Motion carried: ____ Yeas ____ Nays

I hereby certify that the foregoing Resolution was adopted by the City Council for the City of Wyoming, Michigan, at a regular session held on the ____ day of _____, 2011.

Heidi A. Isakson
Wyoming City Clerk

RESOLUTION NO. _____

RESOLUTION TO AWARD A CONTRACT FOR THE
GEZON PARKWAY WIDENING

WHEREAS, Gordon Foods Service (GFS) has committed to relocate their headquarters to a new campus located along Gezon Parkway between Clyde Park Avenue and Burlingame Avenue, in Wyoming, and

WHEREAS, in preparation for the GFS development, a traffic study was performed which recommended the addition of a center turn-lane on Gezon Parkway adjacent to the proposed Gordon Foods Service campus, and

WHEREAS, on July 26, 2011, the City received four (4) bids for the Gezon Parkway Widening with Kamminga & Roodvoets, Inc. submitting the low bid of \$434,518.30, and

WHEREAS, it is in the best interest of the City to perform the aforementioned work, and

WHEREAS, the costs for this project can be financed out of the Capital Improvement Program Fund Account No. 400-441-50200-972502 for Gordon Food Service Project; now, therefore,

BE IT RESOLVED that the City Council hereby awards the contract to Kamminga & Roodvoets, Inc. for the Gezon Parkway Widening in the amount of \$434,518.30.

Councilmember _____ moved, seconded by Councilmember _____, that the above Resolution be adopted.

Motion carried: _____ Yeas, _____ Nays.

I hereby certify that the foregoing Resolution was adopted by the City Council for the City of Wyoming, Michigan, at a _____ session held on the _____ day of _____, 2011.

HEIDI A. ISAKSON
Wyoming City Clerk

Resolution No. _____

M E M O R A N D U M

TO: Mayor and City Council
FROM: William D. Dooley, Director of Public Works
DATE: August 1, 2011
SUBJECT: Gezon Parkway Widening - Award of Bid

On July 26, 2011, the City of Wyoming received four (4) bids for the Gezon Parkway Widening. Kamminga & Roodvoets, Inc. submitted the low bid of \$434,518.30 which is 2.8% below the Engineer's estimate. The project will add a center left-turn lane on Gezon Parkway adjacent to the Gordon Foods Service campus on Gezon Parkway.

The project cost is \$510,000 including Engineering and Contingencies, which can be financed out of the Capital Improvement Program Fund:

<u>Project Account</u>	<u>Construction Cost</u>	<u>Project Cost</u>
400-441-50200-972502 Gordon Food Service Project	\$434,518.30	\$510,000.00

It is recommended that the City Council award the Gezon Parkway Widening to Kamminga & Roodvoets, Inc. in the amount of \$434,518.30.

RJH:nll

Attachments: Bid Tabulation
Contract Form

City of Wyoming Bid Comparison

Contract ID: 145.11
Contract Year: 2011
Description: Gezon Parkway Wedinering to 5 lanes
Location: Gezon Parkway
Projects(s): 145.11

Rank	Bidder	Total Bid	% Over Low	% Over Est.
0	ENGINEER'S ESTIMATE	\$447,125.10	2.90%	0%
1	(9) Kamminga & Roodvoets, Inc	\$434,518.30	0%	-2.81%
2	(4) Dykema Excavators, Inc.	\$446,800.69	2.82%	-0.07%
3	(2) Diversco Construction Co. Inc.	\$473,301.30	8.92%	5.85%
4	(5) Nagel Construction	\$498,845.57	14.80%	11.56%

Line	Pay Item Code	Quantity	Units	(0) ENGINEER'S ESTIMATE		(1) Kamminga & Roodvoets, Inc		(2) Dykema Excavators, Inc.	
Description				Bid Price	Total	Bid Price	Total	Bid Price	Total
0001	1000	1.00	LS	\$5,000.00	\$5,000.00	\$30,000.00	\$30,000.00	\$20,200.00	\$20,200.00
	MOBILIZATION								
0002	1008	3,816.00	LF	\$7.00	\$26,712.00	\$2.25	\$8,586.00	\$3.00	\$11,448.00
	REMOVE CURB & GUTTER								

Line	Pay Item Code	Quantity	Units	(0) ENGINEER'S ESTIMATE		(1) Kamminga & Roodvoets, Inc		(2) Dykema Excavators, Inc.	
				Bid Price	Total	Bid Price	Total	Bid Price	Total
0003	1010	130.00	LF	\$5.00	\$650.00	\$2.00	\$260.00	\$1.84	\$239.20
	SAW CUT EX BITUMINOUS OR CONCRETE								
0004	1035	641.00	SY	\$7.00	\$4,487.00	\$3.00	\$1,923.00	\$3.94	\$2,525.54
	REMOVE SIDEWALK								
0005	1045	370.00	SY	\$8.00	\$2,960.00	\$3.00	\$1,110.00	\$1.75	\$647.50
	REMOVE PAVEMENT								
0006	1142	13.00	EA	\$200.00	\$2,600.00	\$200.00	\$2,600.00	\$200.00	\$2,600.00
	REMOVE EX COVER AND CASTINGS								
0007	1207	9,181.00	Syd	\$0.85	\$7,803.85	\$1.25	\$11,476.25	\$2.00	\$18,362.00
	COLD MILLING EXISTING PAVEMENT 1 3/4"								
0008	4005	55.00	LF	\$15.00	\$825.00	\$20.00	\$1,100.00	\$6.50	\$357.50
	6" EDGE DRAIN IN SOCK								
0009	4026	4,875.00	Lb	\$1.00	\$4,875.00	\$1.00	\$4,875.00	\$1.10	\$5,362.50
	DRAINAGE STRUCTURE CASTINGS S-6B								
0010	4203	55.00	LF	\$40.00	\$2,200.00	\$60.00	\$3,300.00	\$25.00	\$1,375.00
	STORM SEWER CL III 12" 0' TO 14' DEEP C76 CLIII								
0011	4380	12.00	Ea	\$400.00	\$4,800.00	\$1,450.00	\$17,400.00	\$675.00	\$8,100.00
	RELOCATE EXISTING DRAINAGE STRUCTURE								
0012	5055	8.00	EA	\$800.00	\$6,400.00	\$2,650.00	\$21,200.00	\$2,550.00	\$20,400.00
	RELOCATE EX HYDRANT								
0013	6010	3,212.00	SY	\$6.50	\$20,878.00	\$4.00	\$12,848.00	\$3.95	\$12,687.40
	PLACE SAND CLII SUBBASE (AS REQUIRED)								
0014	6114	1,897.00	LF	\$14.00	\$26,558.00	\$10.00	\$18,970.00	\$14.25	\$27,032.25
	STREET GRADE								
0015	6139	3,212.00	Syd	\$6.50	\$20,878.00	\$5.50	\$17,666.00	\$3.32	\$10,663.84
	4" AGGREGATE BASE(CIP)								
0016	6220	1,115.00	Ft	\$20.00	\$22,300.00	\$16.50	\$18,397.50	\$24.00	\$26,760.00
	CONCRETE GUTTER - DET I27A								
0017	6240	2,435.00	Lft	\$16.00	\$38,960.00	\$13.00	\$31,655.00	\$15.50	\$37,742.50
	CONCRETE CURB AND GUTTER, 30"								
0018	6270	5,315.00	SF	\$3.00	\$15,945.00	\$2.80	\$14,882.00	\$3.15	\$16,742.25
	CONCRETE SIDEWALK, 4"								

Line	Pay Item Code	Quantity	Units	(0) ENGINEER'S ESTIMATE		(1) Kamminga & Roodvoets, Inc		(2) Dykema Excavators, Inc.	
				Bid Price	Total	Bid Price	Total	Bid Price	Total
0019	6295	13.00	EA	\$200.00	\$2,600.00	\$410.00	\$5,330.00	\$185.00	\$2,405.00
	ADJUST CASTINGS								
0020	6331	1,001.00	T	\$61.00	\$61,061.00	\$53.55	\$53,603.55	\$58.00	\$58,058.00
	BITUMINOUS MIXTURE - 2C Modified								
0021	6333	1,001.00	TON	\$55.00	\$55,055.00	\$55.40	\$55,455.40	\$57.90	\$57,957.90
	BITUMINOUS MIXTURE - 3C Modified								
0022	6335	1,164.00	TON	\$60.00	\$69,840.00	\$55.15	\$64,194.60	\$58.33	\$67,896.12
	BITUMINOUS MIXTURE - 4C								
0023	6419	500.00	Lft	\$1.00	\$500.00	\$3.75	\$1,875.00	\$4.00	\$2,000.00
	THERMO PLASTIC PAVEMENT MARKING 6" YELLOW								
0024	6455	4.00	EA	\$250.00	\$1,000.00	\$115.00	\$460.00	\$75.00	\$300.00
	COLD PLASTIC PAVEMENT MARKING LEDGEND (ONLY)								
0025	6460	8.00	EA	\$250.00	\$2,000.00	\$105.00	\$840.00	\$75.00	\$600.00
	COLD PLASTIC PAVEMENT MARKING SYMBOL (LEFT-TURN ARROW)								
0026	6485	4,915.00	Lft	\$0.30	\$1,474.50	\$0.28	\$1,376.20	\$0.25	\$1,228.75
	PAVEMENT MARKING REGULAR 4" YELLOW								
0027	6487	3,810.00	Lft	\$0.30	\$1,143.00	\$0.28	\$1,066.80	\$0.25	\$952.50
	PAVEMENT MARKING REGULAR 4" WHITE								
0028	7005	2,087.00	SY	\$3.00	\$6,261.00	\$4.50	\$9,391.50	\$2.87	\$5,989.69
	TOP SOIL 4" SCREENED								
0029	7015	2,087.00	SY	\$1.25	\$2,608.75	\$0.50	\$1,043.50	\$0.75	\$1,565.25
	CLASS A SEED HYDRO-MULCH								
0030	8000	1.00	LS	\$4,000.00	\$4,000.00	\$750.00	\$750.00	\$2,500.00	\$2,500.00
	FLAG CONTROL								
0031	8010	1.00	LS	\$6,000.00	\$6,000.00	\$14,400.00	\$14,400.00	\$5,075.00	\$5,075.00
	MINOR TRAFFIC CONTROL DEVICES								
0032	8020	19.00	STA	\$550.00	\$10,450.00	\$100.00	\$1,900.00	\$500.00	\$9,500.00
	PART - WIDTH CONSTRUCTION								
0033	8100	105.00	EA	\$15.00	\$1,575.00	\$15.00	\$1,575.00	\$19.00	\$1,995.00
	BARRICADE TYPE II LIGHTED - FURNISHED								
0034	8101	105.00	EA	\$5.00	\$525.00	\$1.00	\$105.00	\$1.20	\$126.00
	BARRICADE TYPE II LIGHTED - OPERATED								

Line	Pay Item Code	Quantity	Units	(0) ENGINEER'S ESTIMATE		(1) Kamminga & Roodvoets, Inc		(2) Dykema Excavators, Inc.	
				Bid Price	Total	Bid Price	Total	Bid Price	Total
0035	8120	2.00	EA	\$700.00	\$1,400.00	\$550.00	\$1,100.00	\$600.00	\$1,200.00
	LIGHTED ARROW TYPE A - FURNISHED								
0036	8121	2.00	EA	\$200.00	\$400.00	\$1.00	\$2.00	\$110.00	\$220.00
	LIGHTED ARROW TYPE A - OPERATED								
0037	8122	1.00	EA	\$3,900.00	\$3,900.00	\$1,800.00	\$1,800.00	\$2,921.00	\$2,921.00
	MESSAGE BOARD - FURNISHED								
0038	8123	1.00	EA	\$500.00	\$500.00	\$1.00	\$1.00	\$1,065.00	\$1,065.00
	MESSAGE BOARD - OPERATED								
Bid Totals:					\$447,125.10		\$434,518.30		\$446,800.69

Line	Pay Item Code	Quantity	Units	(3) Diversco Construction Co. Inc.		(4) Nagel Construction		Bid Price	Total
				Bid Price	Total	Bid Price	Total		
0001	1000	1.00	LS	\$45,000.00	\$45,000.00	\$22,500.00	\$22,500.00		
	MOBILIZATION								
0002	1008	3,816.00	LF	\$5.00	\$19,080.00	\$3.10	\$11,829.60		
	REMOVE CURB & GUTTER								
0003	1010	130.00	LF	\$5.00	\$650.00	\$4.00	\$520.00		
	SAW CUT EX BITUMINOUS OR CONCRETE								
0004	1035	641.00	SY	\$5.00	\$3,205.00	\$3.45	\$2,211.45		
	REMOVE SIDEWALK								
0005	1045	370.00	SY	\$5.00	\$1,850.00	\$3.45	\$1,276.50		
	REMOVE PAVEMENT								
0006	1142	13.00	EA	\$200.00	\$2,600.00	\$225.00	\$2,925.00		
	REMOVE EX COVER AND CASTINGS								
0007	1207	9,181.00	Syd	\$1.00	\$9,181.00	\$0.95	\$8,721.95		
	COLD MILLING EXISTING PAVEMENT 1 3/4"								
0008	4005	55.00	LF	\$6.00	\$330.00	\$12.00	\$660.00		
	6" EDGE DRAIN IN SOCK								
0009	4026	4,875.00	Lb	\$2.00	\$9,750.00	\$1.10	\$5,362.50		
	DRAINAGE STRUCTURE CASTINGS S-6B								
0010	4203	55.00	LF	\$45.00	\$2,475.00	\$43.00	\$2,365.00		
	STORM SEWER CL III 12" 0' TO 14' DEEP C76 CLIII								
0011	4380	12.00	Ea	\$2,000.00	\$24,000.00	\$2,200.00	\$26,400.00		
	RELOCATE EXISTING DRAINAGE STRUCTURE								
0012	5055	8.00	EA	\$2,300.00	\$18,400.00	\$4,050.00	\$32,400.00		
	RELOCATE EX HYDRANT								
0013	6010	3,212.00	SY	\$1.65	\$5,299.80	\$5.00	\$16,060.00		
	PLACE SAND CLII SUBBASE (AS REQUIRED)								
0014	6114	1,897.00	LF	\$22.00	\$41,734.00	\$12.36	\$23,446.92		
	STREET GRADE								
0015	6139	3,212.00	Syd	\$4.00	\$12,848.00	\$8.35	\$26,820.20		
	4" AGGREGATE BASE(CIP)								
0016	6220	1,115.00	Ft	\$22.00	\$24,530.00	\$20.00	\$22,300.00		
	CONCRETE GUTTER - DET I27A								

Line	Pay Item Code	Quantity	Units	(3) Diversco Construction Co. Inc.		(4) Nagel Construction		Bid Price	Total
				Bid Price	Total	Bid Price	Total		
0017	6240	2,435.00	Lft	\$14.00	\$34,090.00	\$15.00	\$36,525.00		
	CONCRETE CURB AND GUTTER, 30"								
0018	6270	5,315.00	SF	\$3.50	\$18,602.50	\$3.00	\$15,945.00		
	CONCRETE SIDEWALK, 4"								
0019	6295	13.00	EA	\$400.00	\$5,200.00	\$200.00	\$2,600.00		
	ADJUST CASTINGS								
0020	6331	1,001.00	T	\$54.00	\$54,054.00	\$53.55	\$53,603.55		
	BITUMINOUS MIXTURE - 2C Modified								
0021	6333	1,001.00	TON	\$26.00	\$26,026.00	\$55.40	\$55,455.40		
	BITUMINOUS MIXTURE - 3C Modified								
0022	6335	1,164.00	TON	\$56.00	\$65,184.00	\$55.15	\$64,194.60		
	BITUMINOUS MIXTURE - 4C								
0023	6419	500.00	Lft	\$4.00	\$2,000.00	\$3.75	\$1,875.00		
	THERMO PLASTIC PAVEMENT MARKING 6" YELLOW								
0024	6455	4.00	EA	\$125.00	\$500.00	\$115.00	\$460.00		
	COLD PLASTIC PAVEMENT MARKING LEDGEND (ONLY)								
0025	6460	8.00	EA	\$125.00	\$1,000.00	\$105.00	\$840.00		
	COLD PLASTIC PAVEMENT MARKING SYMBOL (LEFT - TURN ARROW)								
0026	6485	4,915.00	Lft	\$0.30	\$1,474.50	\$0.30	\$1,474.50		
	PAVEMENT MARKING REGULAR 4" YELLOW								
0027	6487	3,810.00	Lft	\$0.30	\$1,143.00	\$0.30	\$1,143.00		
	PAVEMENT MARKING REGULAR 4" WHITE								
0028	7005	2,087.00	SY	\$3.00	\$6,261.00	\$3.20	\$6,678.40		
	TOP SOIL 4" SCREENED								
0029	7015	2,087.00	SY	\$0.50	\$1,043.50	\$1.00	\$2,087.00		
	CLASS A SEED HYDRO-MULCH								
0030	8000	1.00	LS	\$3,500.00	\$3,500.00	\$1,000.00	\$1,000.00		
	FLAG CONTROL								
0031	8010	1.00	LS	\$6,500.00	\$6,500.00	\$15,125.00	\$15,125.00		
	MINOR TRAFFIC CONTROL DEVICES								
0032	8020	19.00	STA	\$1,000.00	\$19,000.00	\$1,450.00	\$27,550.00		
	PART - WIDTH CONSTRUCTION								

Line	Pay Item Code	Quantity	Units	(3) Diversco Construction Co. Inc.		(4) Nagel Construction		Bid Price	Total
				Bid Price	Total	Bid Price	Total		
0033	8100	105.00	EA	\$17.00	\$1,785.00	\$17.00	\$1,785.00		
	BARRICADE TYPE II LIGHTED - FURNISHED								
0034	8101	105.00	EA	\$1.00	\$105.00	\$1.00	\$105.00		
	BARRICADE TYPE II LIGHTED - OPERATED								
0035	8120	2.00	EA	\$600.00	\$1,200.00	\$550.00	\$1,100.00		
	LIGHTED ARROW TYPE A - FURNISHED								
0036	8121	2.00	EA	\$100.00	\$200.00	\$100.00	\$200.00		
	LIGHTED ARROW TYPE A - OPERATED								
0037	8122	1.00	EA	\$3,000.00	\$3,000.00	\$2,800.00	\$2,800.00		
	MESSAGE BOARD - FURNISHED								
0038	8123	1.00	EA	\$500.00	\$500.00	\$500.00	\$500.00		
	MESSAGE BOARD - OPERATED								
Bid Totals:					\$473,301.30		\$498,845.57		

CONTRACT

THIS CONTRACT AWARDED on the 1st day of August 2011 by and between the CITY OF WYOMING, Kent County, Michigan, party of the first part, hereinafter called the "City" and

**KAMMINGA & ROODVOETS, INC.
3435 BROADMOOR AVENUE, SE
GRAND RAPIDS, MI 49512**

party of the second part, hereinafter called the "Contractor."

Witnesseth, that the City and the Contractor, for the considerations hereinafter mentioned, agree as follows:

ARTICLE I - THE CONTRACT

The following shall be deemed to be part of this Contract:

- A. Performance Bond, Payment Bond, and Resolution from Board of Directors**
- B. Information for Bidders**
- C. Bid Proposal Form**
- D. Award of Bid**
- E. Certificate of Insurance (per City of Wyoming's Contractor Insurance Requirements)**
- F. Subcontract Provision**
- G. Letter of Credit (5% of Annual Contract Amount)**
- H. Equal Employment Opportunity Statement**
- I. All Provisions required by law to be inserted in this contract, whether actually inserted or not.**
- J. Special Specifications as listed below:**

SEE ATTACHED

ARTICLE II - SCOPE OF THE WORK - SUBJECT MATTER

The Contractor hereby agrees to furnish all of the materials and all of the equipment and labor necessary to perform all of the work shown on the drawing and described in the specifications for the project entitled:

GEZON PARKWAY WIDENING

all in accordance with the requirements and provisions of the Contract. The Contractor likewise agrees to do all incidental and appurtenant work in connection therewith.

ARTICLE III - TIME OF COMPLETION

The work to be performed under this contract shall be completed on or before October 28, 2011. Should the Contractor be obstructed or delayed in the prosecution or completion of his work by any act, neglect or default of the City, then the time herein fixed for completion of the work shall be extended for a period equivalent to the time lost by reason of such delay for the causes herein mentioned. The duration of such extension shall be determined by the City Manager or his authorized representative.

ARTICLE IV - LIQUIDATED DAMAGES

Should the work under this Agreement not be finished within the time specified, the City is hereby authorized to deduct out of the money which may be due or become due to the Contractor under this Agreement, as damages for the noncompletion of the work aforesaid, within the time hereinbefore stipulated for its completion, the sum of Four Hundred Fifty Dollars (\$450.00) for each calendar day by which the Contractor shall fail to complete the work or any part thereof in accordance with the provisions hereof, and such liquidated damages shall not be considered as a penalty. It is understood that the City shall not forfeit its right to liquidated damages in the event that delay is partly caused by it. In this event, said damages shall be apportioned so that each day of delay attributable to the City shall be subtracted from the total days of delay, and the Contractor shall pay liquidated damages for the remainder.

ARTICLE V - THE CONTRACT SUM

The City agrees to pay and the Contractor agrees to accept the sum of:

FOUR HUNDRED THIRTY-FOUR THOUSAND FIVE HUNDRED EIGHTEEN DOLLARS AND THIRTY CENTS (\$434,518.30)

the amount named in the Proposal, as full compensation for all labor, supervision, equipment, materials, and incidental expense required in executing all of the work contemplated in this Contract, including also all loss or damage arising out of the nature of the work or from the action of the elements or from any unforeseen obstruction or difficulties which may be encountered in the prosecution of the same or from other causes of every description connected with the work.

It is further agreed that the sum named may be increased or decreased in accordance with the units of work actually completed at the Contract unit prices, and may be increased by the addition of items of work not included in the proposal items.

ARTICLE VI - CITY CHARTER TO BE GOVERNING DOCUMENT

The City Charter of the City shall be the governing document in all contractual relations with the City of Wyoming. In the prosecution of the work under this Contract, eight (8) hours shall constitute a day's labor. The City retains the right to determine finally all questions as to the proper performance under this Contract or any unfinished portion thereof, and in case of improper, dilatory or imperfect performance thereof to suspend the work at any time and to order the partial or entire reconstruction of the same. The City likewise retains full power to determine all questions arising under this Contract according to the true intent and meaning thereof.

ARTICLE VII - COMPLIANCE WITH LAWS, ORDINANCES AND REGULATIONS

The Contractor shall keep himself fully informed of and shall at all time comply with all local, state, and federal laws, rules, and regulations applicable to this Contract and the work to be done hereunder.

RESOLUTION NO. _____

RESOLUTION TO ACCEPT A PROPOSAL FOR
CATHODIC PROTECTION SYSTEMS FOR LIFT STATIONS

WHEREAS, as detailed in the attached memorandum from the City's Clean Water Plant Maintenance Supervisor, the City recently installed Cathodic protection systems at four of its twelve lift stations, and

WHEREAS, Corrpro Companies has provided the City with a proposal for to provide Cathodic protection at four additional lift stations in the amount of \$35,360.00 and it is recommended the City accept the proposal, and

WHEREAS, funds for the Cathodic protection are available in the Clean Water Plant transmission account number #590-441-54200-930000, now therefore

BE IT RESOLVED, that the Wyoming City Council does hereby authorize the acceptance of the proposal for Cathodic protection systems from Corrpro Companies as detailed in the attached memorandum, and

BE IT FURTHER RESOLVED, that the Wyoming City Council does hereby waive the provisions of Sections 2-252, 2-253, 2-254 and 2-256 of the City Code regarding publication and posting of bid notices, notification of bidders and the bid opening procedure.

Councilmember _____ moved, seconded by Councilmember _____, that the above Resolution be adopted.

Motion carried: _____ Yeas, _____ Nays

I hereby certify that the foregoing Resolution was adopted by the City Council for the City of Wyoming, Michigan, at a regular session held on the 1st day of August, 2011.

Attachments: Memorandum
Proposal

Heidi A. Isakson
Wyoming City Clerk

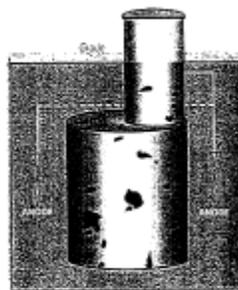
Resolution No. _____

Memorandum

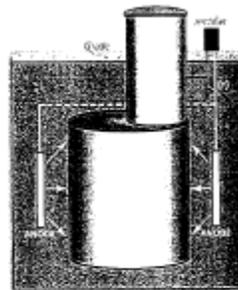
To: Kim Oostindie, Human Resources Supervisor
From: Tom Wilson, Clean Water Plant Maint. Supervisor
Date: July 20, 2011
Re: Resolution for the Installation of Cathodic Protection Systems at Four City Owned Lift Stations

In April of this year, City Council approved Resolution #23836 for the first of three phases of the installation of new cathodic protection systems at the following lift stations: 2801 Union Street SE, 18 Himes Street SE, 4660 Pinehurst Avenue SW, and 2608 Poe Avenue SW. This work was completed by Corpro Companies for a total of \$35,360.00 according to their bid received on March 29, 2011.

To refresh your memory on this subject, throughout the City of Wyoming, there are twelve lift stations that house two pumps that pump raw sewage from a low elevation to a higher elevation. The force of gravity then allows the raw sewage to flow to the City of Wyoming Clean Water Plant to be treated. Most of the lift stations, made of steel and buried underground, were installed during the 1960's and 1970's.



Depleted cathodic protection system allows corrosion to occur.



Effective cathodic protection system prevents corrosion and extends life of lift station.

Lift stations are normally provided with corrosion control for the exterior steel surfaces in contact with the soil. The simplest method to apply cathodic protection is by connecting the metal to be protected with another more easily corroded metal to act as the anode of the electrochemical cell. Cathodic protection systems are used to protect a wide range of metallic structures in various environments. Common applications are steel water and storage tanks and fuel pipelines. In our application, anodes are buried underground alongside the station, and discharge a positive current to the lift station. Over time, these anodes deteriorate and eventually become ineffective. When this occurs, the exterior of the station becomes vulnerable to corrosion and potential failure. The result could mean a costly and disruptive repair; or in some cases total replacement.

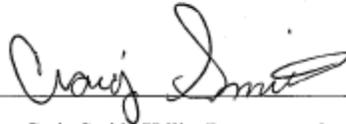
Recently, we had these lift stations tested by a cathodic protection engineering firm to measure the levels of the current cathodic protection of the stations. The data from this study indicates that the measurements taken at these stations do not meet the National Association of Corrosion Engineers (NACE) -850mV criterion for cathodic protection which indicates the cathodic protection has failed. To assure proper protection and to extend the life of these lift stations, it is essential that a new cathodic protection system be installed so as to meet the required NACE levels.

As I mentioned at the beginning of this Memo, Council approved Phase 1 of the installation of this protection in April, 2011 and that work has been successfully completed. Phase 2 of this project is included in the budget for the 2011/2012 Fiscal Year. Identical cathodic protection systems would be installed at the lift stations located at 3398 Robin Street SW, 3720 Clyde Park Avenue SW, 2050 – 30th Street SW, and 343 Pickett Street SW.

At the beginning of the bid process for completion of this work, bid documents were sent to fifteen prospective bidders. The only bid received at the bid opening on March 29, 2011 was from Corpro Companies, Inc. In my discussions with Corpro Companies, Inc., the company that was originally awarded Phase 1 of this project, they have agreed to complete the Phase 2 installation of cathodic protection at the four lift stations noted above at the same cost that was charged for Phase 1.

Since Corpro Companies, Inc. was the only bidder and because they have agreed to complete Phase 2 of this project at the same cost as Phase 1, it is my recommendation that City Council forego the bid procedure and award the contract to Corpro Companies, Inc. in the amount of \$35,360.00. Adequate funds are available in the Transmission Account #590-441-54200-930000.

Reviewed and Approved by:



Craig Smith, Utility Department Superintendent

Reviewed and Approved by:



Thomas Kent, Deputy Director of Public Works



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Medina, OH 44256
Tel: (330) 723-5082
Fax: (330) 722-7606
www.corrpro.com

April 27, 2011

Attention: Tom Wilson (Maintenance Supervisor)
City of Wyoming, Clean Water Plant
2350 Ivanrest
Wyoming, MI 49509-0905

Subject: Cathodic Protection for Lift Stations

Dear Mr. Wilson:

Corrpro Companies, Inc. is pleased to submit this proposal for cathodic protection installation services. Corrpro recently installed cathodic protection systems at (4) four lift stations for the City of Wyoming under contract #2011-00000821.

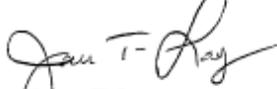
It is our understanding the City of Wyoming is interested in installing cathodic protection at four additional sites. Corrpro will provide these services at the fees identified below provided we can do all 4 sites in one mobilization:

- 3398 Robin Street SW	\$8,840.00
- 3720 Clyde Park SW	\$8,840.00
- 2050 30th Street SW	\$8,840.00
- 343 Pickett SW	\$8,840.00

AC power supply and hookup shall be by provided by others. We do not intend to sub contract out any of the work associated with this project. All work would be performed by full time direct employees of Corrpro who are non union and at non prevailing wage rates. Validity of this proposal is 180 days. Upon completion of the installation, our engineer would commission the systems and issue a formal report.

Corrpro Companies Inc. appreciates the opportunity to submit this proposal and we look forward to working with you on this project. Should you have any questions or require additional information, please do not hesitate to contact our office at 330-723-5082 (x1215).

Respectfully,
CORRPRO COMPANIES INC.


James T. Lary
NACE Corrosion Technologist

RESOLUTION NO. _____

RESOLUTION TO AUTHORIZE THE
PURCHASE OF AMMUNITION

WHEREAS, the City of Wyoming's Police Department purchases ammunition on an as needed basis, and

WHEREAS, as detailed in the attached memorandum from the Captain of Administrative Services of the Police Department, it is recommended ammunition be purchased from MI Police Equipment Co., CMP Distributors, Inc., and Vance Outdoors using the State of Michigan contracts, and

WHEREAS, funds for the ammunition purchase are budgeted in account number 101-305-31500-740000, now therefore

BE IT RESOLVED, that the Wyoming City Council does hereby authorize the purchase of ammunition from MI Police Equipment Co., CMP Distributors, Inc., and Vance Outdoors using State of Michigan contract prices, and

BE IT FURTHER RESOLVED, that the Wyoming City Council does hereby waive the provisions of Sections 2-252, 2-253, 2-254 and 2-256 of the City Code regarding publication and posting of bid notices, notification of bidders and the bid opening procedure.

Councilmember _____ moved, seconded by Councilmember _____, that the above Resolution be adopted.

Motion carried: _____ Yeas, _____ Nays

I hereby certify that the foregoing Resolution was adopted by the City Council for the City of Wyoming, Michigan, at a regular session held on the 1st day of August, 2011.

Attachments: Memorandum
Cost Analysis

Heidi A. Isakson
Wyoming City Clerk

Resolution No. _____

Interdepartmental Correspondence

TO: Kim Oostindie, Administrative Assistant
FROM: Captain Kim Koster
DATE: July 21, 2011
SUBJECT: Patrol / Bid Specification Results (Ammunition)



Administration Services

Please begin the process and necessary paperwork to establish the annual purchases of Ammunition. On May 17, 2011, two responses were received in answer to our invitation to bid on the Police Department's ammunition. Thirty one invitations to bid were sent to prospective bidders.

This bid has since expired (over 60 days), however we are able to order all through Michigan State Contracts with MI Police Equipment Co, CMP Distributors Inc, and Vance Outdoors (and at cheaper prices, see attached spreadsheet), as follows:

MI Police Equipment Co.

Contract #071B1300240 (5/3/11 – 4/30/14)

- Hornaday #9754EL (practice)
.223 caliber, 55 grain full metal jacket @ \$225 per 1,000 rds

CMP Distributors Inc.

Contract #071B1300241 (5/3/11 – 4/30/14)

- Federal #AE40R1 (practice)
.40 cal pistol target @ \$185.44 per 1,000 rds
- Speer #53962 (duty)
.40 cal, 180 gr, Gold Dot Hollow Point @ \$277.60 per 1,000 rds
- Federal #LE13300 (duty)
12ga 00 Buck, Ranger Low Recoil/High Brass @ \$93.50 per 250 rds
- Federal #LEB127LRS (duty)
12ga 1 oz Slug, Ranger Low Recoil/High Brass
- Federal #LE308T1 (duty)
.308 cal, 165gr bonded soft point bullet Tactical Duty @ \$683.50 per 500 rds
- Speer #53619 (duty)
9mm, 147 gr, Gold Dot Hollow Point @ \$385 per 1,000 rds (not on any of the State Contracts, however CMP will sell to Wyoming at this discounted price, per Heather at CMP)
- Federal #T223A (duty)
.223 cal, 55 gr bonded soft point, Tactical Duty @ \$422 per 1,000 rds
- Federal #GM308M500 (duty)
.308 cal, 168 gr Gold Metal Match, Sierra Match King BTHP @ \$685.30 per 1,000 rds

Vance Outdoors

Contract #071B1300242 (5/3/11 – 4/30/14)

- Winchester #Q4172 (practice)
9mm, 124 grain total metal jacket – lead free, small pistol @ \$159 per 1,000 rounds

It is the recommendation of Sgt. Kirt Zuiderveen, to purchase ammunition from these Michigan State Contract vendors.

It is anticipated that the Police Department will expend approximately \$25,000 on all ammunition during the fiscal year.

Funding is allocated and available in the following accounts: 101-305-31500-740000

Please advise if additional information is required to continue processing this request.

Description	Est. Annual Qty.	Brand	MI State Bid (Contract # 071B1300240) MI Police Equipment Co		MI State Bid (Contract # 071B1300241) CMP Distributors Inc		MI State Bid (Contract # 071B1300242) Vance Outdoors	
.40 caliber pistol target ammunition, Case: Brass or Nickel, Primer: Lead Free, Small Pistol, Power Load: bullet velocity of 960 FPS Minimum Required, Bullet: 180 Grain, Total Metal Jacket	40,000	Winchester Q4238					\$189.00	50 rounds per box/ 500 rounds per case
		Federal AE40R1			\$185.44	50 rounds per box/1,000 rounds per case		
		Remington L40SW3	\$200.00	50 rounds per box/ 500 rounds per case				
Speer Gold Dot Hollow Point, .40 caliber, 180 gr.	4,000	Speer 53962			\$277.60	50 rounds per box 1,000 rounds per case	\$291.00	50 rounds per box / 500 rounds per case Winchester (not Speer)
Ranger Low Recoil/High Brass, 12 gauge 00 Buck	1,500	Federal LE13300			\$93.50	5 rounds per box /250 rounds per case		
Ranger Low Recoil/High Brass, 12 gauge 1 oz. Slug	250	Federal LEB127LRS			\$103.00	5 rounds per box /250 rounds per case		
Tactical Duty, .308 Caliber, 165 gr. bonded Soft Point Bullet	500	Federal LE308T1			\$683.50	20 rounds per box/ 200 rounds per case		
9mm, 124 grain total metal jacket - lead free, small pistol	10,000	Winchester Q4172					\$159.00	50 rounds per box/ 500 rounds per case Q4172
		Federal AE9AP			\$173.00	50 rounds per box/1,000 rounds per case		
		Remington R9MM3	\$235.00	50 rounds per box/ 500 rounds per case				
Speer Gold Dot Hollow Point 9mm, 147 gr.	1,000	Speer 53619			\$385.00	50 rounds per box 1,000 rounds per case Federal (not Speer) , however CMP will sell Speer #53619 to Wyoming at a discounted price of \$385, per Heather at CMP		
Tactical Duty .223 cal, 55 gr. bonded soft point	3,000	Federal T223A			\$422.00	20 rounds per box /200 rounds per case		
.223 Caliber, 55 Grain Full Metal Jacket	5,000	Federal AE223			\$297.80	20 rounds per box 500 rounds per case		
		Hornaday 9754EL	\$225.00	50 rounds per box/ 1,000 rounds per case			\$230.00	50 rounds per box 500 rounds per case
Gold Metal Match .308 Winchester 168 Grain Sierra Match King BTHP	3,000	Federal GM308M500			\$685.30	20 rounds per box 500 rounds per case		

RESOLUTION NO. _____

RESOLUTION FOR THE PURCHASE OF
LABORATORY SUPPLIES

WHEREAS, the City of Wyoming owns and operates the Donald K. Shine Water Treatment Plant which serves the potable water needs of its residents and those of its wholesale customer communities, and

WHEREAS, as detailed in the attached memorandum from the City's Laboratory Supervisor, the laboratory uses the QuantiTray technology for various laboratory tests and analysis, and

WHEREAS, supplies for the QuantiTray technology are only available from IDEXX and are purchased monthly and as needed to complete the required laboratory tests, and

WHEREAS, funds for the purchase of the IDEXX laboratory supplies are available in account number 591-591-55300-740000, now therefore

BE IT RESOLVED, that the Wyoming City Council does hereby authorize the continued purchase of laboratory supplies from IDEXX as detailed in the attached memorandum, and

BE IT FURTHER RESOLVED, that the Wyoming City Council does hereby waive the provisions of Sections 2-252, 2-253, 2-254 and 2-256 of the City Code regarding publication and posting of bid notices, notification of bidders and the bid opening procedure.

Councilmember _____ moved, seconded by Councilmember _____, that the above Resolution be adopted.

Motion carried: _____ Yeas, _____ Nays

I hereby certify that the foregoing Resolution was adopted by the City Council for the City of Wyoming, Michigan, at a regular session held on the 1st day of August, 2011.

Attachments: Memorandum
Quote

Heidi A. Isakson
Wyoming City Clerk

Memorandum

To: Kim Oostindie, Human Resources Supervisor
Cc: Jerry Caron, Water Treatment Plant Superintendent
From: Jaime Halm, Laboratory Supervisor
Date: 14 July 2011
Re: IDEXX expenses

As you know, we go through a considerable amount of consumable supplies in the Water Treatment Plant laboratory. The one type of supply we are likely to use at a rate that would exceed \$7500 on an annual basis is related to microbiological analysis. As you know, we use a method known as QuantiTray, which is a proprietary technology and therefore its manufacturer is our sole source for these supplies. The technique is accepted by Standard Methods for use in generating reportable drinking water data, and gives us greater accuracy with less labor and turnaround time.

We have a standing order for one case of IDEXX QuantiTrays (100 trays/case), two cases of bottles (200 bottles/case), and one box of Colilert media (200 tests/box) every month. We periodically order an additional box of something if we're running low, but the standing order pretty much covers our needs.

The attached quote from IDEXX gives unit pricing for the supplies described above. The cost is \$1,010.00 for product plus \$57.30 for shipping, for a monthly total of \$1,067.30. The grand total for the year is \$12,807.60.

Allowing for the extra odd order in case of emergencies would put us at a **maximum annual expense of \$20,000 per year including shipping**. I recommend seeking Council approval to continue to make this purchase on an as-needed basis. The ledger account 591-591-55300-740000 is the account we use for these purchases.

Approved:

Gerald Caron, WTP Superintendent



QUOTE

Number / Date
20087354 / July/13/2011

Ship to Address
CITY OF WYOMING
WATER TREATMENT PLANT
16700 NEW HOLLAND STREET
HOLLAND MI 49424
UNITED STATES
UNITED STATES

Sold to Address
CITY OF WYOMING
PO Box 905
WYOMING MI 49509-0905
UNITED STATES
UNITED STATES

Terms of delivery : FPS,

Bill-to Customer 12266

Payment Terms : Within 30 days

Net weight : 15.402

Material ID Commodity/COO	Description Batch	Exp.Date	Quantity BO Item	UnitPrice	Total Value
98-12973-00 3822001090/US	WP200I GAMMA IRRAD COLILE		1	640.00	640.00
98-21675-00 3926909910/US	WQT2K QUANTI-TRAY 2000 DI		1	150.00	150.00
98-09221-00 3926909910/US	WV120SBST-200, VESSELS W/S		2	110.00	220.00
Items total					1,010.00
Freight Value					57.30
Total amount					USD 1,067.30
					=====

All local taxes at customer charge

RESOLUTION NO. _____

RESOLUTION TO AWARD A CONTRACT FOR THE
LEMERY PARK ULTRA THIN OVERLAY

WHEREAS, the age and condition of the parking area in Lemery Park requires major maintenance to restore the life of the asphalt parking surface, and

WHEREAS, the parking area is stable and can remain in place with an thin overlay cap placed on top to seal the pavement and lengthen the useful life, and

WHEREAS, on July 26, 2011, the City received five (5) bids for the Lemery Park Ultra Thin Overlay with Michigan Paving and Materials, Co. submitting the low bid of \$23,536.75, and

WHEREAS, the bid is \$2,719.75 or 10.4% below the Engineer's estimate, and

WHEREAS, it is in the best interest of the City to perform the aforementioned work, and

WHEREAS, the costs for this project can be financed out of the Parks & Recreation Department's Park Facility Fund account number 208-752-75600-975.112 for Lemery Park Ultra Thin Overlay; now, therefore,

BE IT RESOLVED that the City Council hereby awards the contract to Michigan Paving and Materials, Co. for the Lemery Park Ultra Thin Overlay in the amount of \$23,536.75.

Councilmember _____ moved, seconded by Councilmember _____, that the above Resolution be adopted.

Motion carried: _____ Yeas, _____ Nays.

I hereby certify that the foregoing Resolution was adopted by the City Council for the City of Wyoming, Michigan, at a _____ session held on the _____ day of _____, 2011.

HEIDI A. ISAKSON
Wyoming City Clerk

Resolution No. _____

M E M O R A N D U M

TO: Mayor and City Council
FROM: Rebecca Rynbrandt, Director of Community Services
DATE: July 26, 2011
SUBJECT: Lemery Park Ultra Thin Overlay - Award of Bid

On July 26, 2011, the City of Wyoming received five (5) bids for the Lemery Park Ultra Thin Overlay. Michigan Paving and Materials, Co. submitted the low bid of \$23,536.75 which is 10.4% below the Engineer's estimate. The project will overlay the existing asphalt with a new asphalt cap, sealing the pavement and providing a nice new surface.

The project cost is \$25,500 including Engineering and Contingencies, which can be financed out of the Parks Fund:

<u>Project Account</u>	<u>Construction Cost</u>	<u>Project Cost</u>
208-752-75600-975.112	\$23,536.75	\$25,500.00

It is recommended that the City Council award the Lemery Park Ultra Thin Overlay to Michigan Paving and Materials, Co. in the amount of \$23,536.75. The FY 2012 Council approved budget for this project is \$26,000.

RR:rjh

Attachments: Bid Tabulation

City of Wyoming Bid Comparison

Contract ID: 144.11
Contract Year: 2011
Description: Lemery Park Ultra Thin Overlay
Location: Lemery Park
Projects(s): 144.11

Rank	Bidder	Total Bid	% Over Low	% Over Est.
0	ENGINEER'S ESTIMATE	\$26,256.50	11.55%	0%
1	(_23) Michigan Paving & Materials Co.	\$23,536.75	0%	-10.35%
2	(05059) A-1 Asphalt, Inc.	\$26,233.79	11.45%	-0.08%
3	(_21) Rieth-Riley Construction	\$26,934.00	14.43%	2.58%
4	(_10000) Jaran Construction Inc.	\$27,248.00	15.76%	3.77%
5	(08551) Superior Asphalt, Inc.	\$28,038.50	19.12%	6.78%

Line	Pay Item Code	Quantity	Units	(0) ENGINEER'S ESTIMATE		(1) Michigan Paving & Materials Co.		(2) A-1 Asphalt, Inc.	
				Bid Price	Total	Bid Price	Total	Bid Price	Total
0001	6324	293.00	T	\$85.00	\$24,905.00	\$77.25	\$22,634.25	\$86.03	\$25,206.79
Ultra Thin Overlay 3/4 inch thick Low Volume									

Line	Pay Item Code	Quantity	Units	(0) ENGINEER'S ESTIMATE		(1) Michigan Paving & Materials Co.		(2) A-1 Asphalt, Inc.	
				Bid Price	Total	Bid Price	Total	Bid Price	Total
0002	6476	2.00	EA	\$200.00	\$400.00	\$100.00	\$200.00	\$100.00	\$200.00
COLD PLASTIC PAVEMENT MARKING SYMBOL (HANDICAP)									
0003	6486	80.00	Lft	\$1.00	\$80.00	\$1.00	\$80.00	\$1.00	\$80.00
PAVEMENT MARKING REGULAR 4" BLUE									
0004	6489	2,490.00	Lft	\$0.35	\$871.50	\$0.25	\$622.50	\$0.30	\$747.00
PAVEMENT MARKING REGULAR 6" YELLOW									
Bid Totals:					\$26,256.50		\$23,536.75		\$26,233.79

Line	Pay Item Code	Quantity	Units	(3) Rieth-Riley Construction		(4) Jaran Construction Inc.		(5) Superior Asphalt, Inc.	
				Bid Price	Total	Bid Price	Total	Bid Price	Total
0001	6324	293.00	T	\$90.00	\$26,370.00	\$89.00	\$26,077.00	\$92.00	\$26,956.00
Ultra Thin Overlay 3/4 inch thick Low Volume									
0002	6476	2.00	EA	\$25.00	\$50.00	\$200.00	\$400.00	\$150.00	\$300.00
COLD PLASTIC PAVEMENT MARKING SYMBOL --(HANDICAP)--									
0003	6486	80.00	Lft	\$0.20	\$16.00	\$0.30	\$24.00	\$2.00	\$160.00
PAVEMENT MARKING REGULAR 4" BLUE									
0004	6489	2,490.00	Lft	\$0.20	\$498.00	\$0.30	\$747.00	\$0.25	\$622.50
PAVEMENT MARKING REGULAR 6" YELLOW									
Bid Totals:					\$26,934.00		\$27,248.00		\$28,038.50

7/5/11

ORDINANCE NO. 5-11

AN ORDINANCE TO AMEND SECTION 90-1 AND
TO ADD SECTIONS 90-2 THROUGH 90-25
OF THE CODE OF THE CITY OF WYOMING

THE CITY OF WYOMING ORDAINS.

Section 1. That Section 90-1 of the Code is hereby amended to read as follows:

ARTICLE I. DEFINITIONS

Section 90-1. Interpretations:

- (1) If the meaning of this Ordinance is unclear in a particular circumstance, then the individual or body charged with interpreting or applying Ordinance shall construe the provision to carry out the intent of the Ordinance, if the intent can be discerned from other provisions of the Ordinance or law.
- (2) All words and phrases shall be construed and understood according to the common preferred use of the language; but technical words and phrases that may have acquired a peculiar and appropriate meaning in the law shall be construed and understood according to that peculiar and appropriate meaning.
- (3) The definitions contained in this Article are for the purposes of this Ordinance.

Terms:

- (1) The word "shall" is always mandatory and not discretionary. The word "may" is permissive.
- (2) The terms "Ordinance" and "Act" shall be understood to include the term "as amended" where the context is appropriate.
- (3) The particular shall control the general. For terms used in this Ordinance the use of a general term shall not be taken to be the same as the use of any other specific term. For example, a "drugstore," as used in this

Ordinance, shall not be interpreted to be the same as a “retail business” since it is listed as a separate and distinct use.

- (4) Words used in the present tense shall include the future; words used in the singular number shall include the plural; and the plural the singular, unless the context clearly indicates otherwise.
- (5) A “building” or “structure” includes any part thereof.
- (6) The word “person” includes an individual, a corporation, a partnership, an incorporated association, or any other similar entity. A masculine term shall include the feminine version of the term and vice versa.
- (7) Unless the context clearly indicates the contrary, the conjunctions noted below shall be interpreted as follows:
 - a. “And” indicates that all connected items, conditions, provisions, or events shall apply.
 - b. “Or” indicates that the connected items, conditions, provisions or events may apply separately or in combination.
 - c. “Either...or” indicates that the connected items, conditions, provisions or events shall apply separately, not in combination.
- (8) Computing the number of days, the first day is excluded and the last day is included.
- (9) All measurements and calculations shall be to the nearest integer, unless otherwise specified herein; however, fractional measurements that are less restrictive than stated minimums or maximums are not allowed. Calculations that yield a fractional result of exactly one half (1/2, or .5) shall be rounded upwards to the next integer.
- (10) The phrase “used for” includes “arranged for,” “intended for,” “occupied for,” and “maintained for.”
- (11) Whenever a word or term defined hereinafter in the text of the Ordinance, its meaning shall be construed as defined herein. Words or terms not herein defined shall have the meaning customarily assigned to them.
- (12) Unless the context clearly indicates to the contrary, where an illustration accompanies any item in this Ordinance, the written text shall have precedence over said illustration.

- (13) The terms “abutting” or “adjacent to” include property along the lot lines of the subject site including those of another community, but do not include lands separated by a public street right-of-way.

Section 2. That Sections 90-2 through 90-25 are hereby added to the Code to read as follows:

Section 90-2. Definitions “A”

Accessible: A term used to describe a parcel of land that has frontage on, and vehicular access to, an improved public road or an improved private road that has been approved by the City.

Access Management: A technique to improve traffic operations along a major roadway and decrease the potential for accidents through the control of driveway locations and design; consideration of the relationship of traffic activity for properties adjacent to, and across from, one another; and the promotion of alternatives to direct access.

Accessory Use, Building, or Structure: A use, building or structure which is clearly incidental to, customarily found in connection with, subordinate to, and located on the same zoning lot as the principal use to which it is related, and devoted exclusively to the main use of the premises.

Administrative Approval: The City Departments of Building, Planning, Fire, Assessing, Public Works and Engineering who grant site plan approval for those development projects that do not require Planning Commission authorization.

Administrative Review: The City Departments of Building, Planning, Fire, Assessing, Public Works and Engineering whose responsibility is to review and comment on site plan submittals prior to Planning Commission review.

Adult Business: a business establishment catering to adults only, as defined in Section 14-88 of the City of Wyoming Code of Ordinances.

Adult Care Facilities:

- (1) Adult Care Facilities, State-Licensed: A facility for the care of adults, over eighteen (18) years of age, as licensed and regulated by the State under Michigan Public Act 218 of 1979, and rules promulgated by the State Department of Human Services, providing foster care to adults. It includes facilities and foster care homes for adults who are aged, mentally ill, developmentally disabled, or physically handicapped who require supervision on an ongoing basis, but do not require continuous nursing care. An adult foster care facility does not include nursing homes, homes for the aged, hospitals, alcohol or substance abuse rehabilitation center, or a residential center for persons released from or assigned to a correctional facility.
- (2) Adult Day-Care Facility: A facility other than a private residence, which provides care for more than six (6) adults for less than twenty-four (24) hour period.
- (3) Adult Foster Care Family Home: A private home with the approved capacity to receive 6 or fewer adults to be provided with foster care for twenty-four (24)

hours a day for 5 or more days a week and for 2 or more consecutive weeks. The adult foster care family home licensee must be a member of the household and an occupant of the residence.

- (4) Adult Foster Care Large Group Home: A private home with approved capacity to receive at least thirteen (13) but not more than twenty (20) adults to be provided supervision, personal care, and protection, in addition to room and board, for compensation, for twenty-four (24) hours a day, five (5) or more days a week, and for two (2) or more consecutive weeks.
- (5) Adult Foster Care Small Group Home: A private home with the approved capacity to receive seven (7) to twelve (12) adults who are provided supervision, personal care, and protection in addition to room and board, for twenty-four (24) hours a day, five (5) or more days a week, and for two (2) or more consecutive weeks for compensation.
- (6) Congregate Adult Care Facility: A private home with the approved capacity to receive more than twenty (20) adults.

Animals:

- (1) Domestic: Any animal customarily kept by humans for companionship, including but not limited to dogs, cats, birds, rabbits, hamsters, mice, turtles, and the like.
- (2) Exotic: Any species of animal not considered domestic or livestock, including but not limited to snakes, lizards and potbellied pigs.

Section 90-3. Definitions “B”

Basement: That portion of a building which is partly or wholly below grade but so located that the vertical distance from the average grade to the floor is greater than the vertical distance from the average grade to the ceiling. A basement shall not be counted as a story.

Boardinghouse: A dwelling where meals or lodging are provided for compensation to two or more persons for periods of one week or more.

Berm: A man-made mound of earth that is graded and shaped to a specified height and slope and improved with landscaping in such a fashion as to be used for visual and/or audible screening purposes.

Block: The property, abutting one (1) side of a street lying between the two (2) nearest intersecting streets or between an intersecting street and a railroad right-of-way; unsubdivided acreage, river or stream; or between any of the foregoing and any other barrier to the continuity of development.

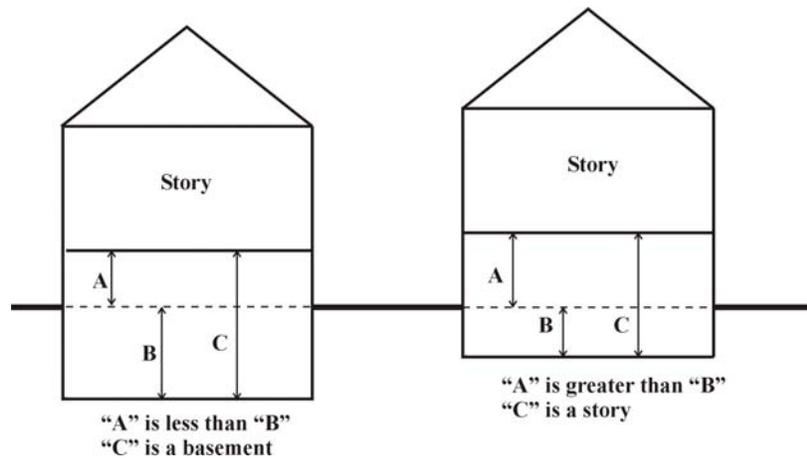


Figure 90-3-1: Basement and Story

Building:

- (1) **Alterations:** Any change, addition or modification in construction or type of occupancy; any change in the structural members of a building, such as walls, partitions, columns, beams, girders; or any change which may be referred to herein as "altered" or "reconstructed."
- (2) **Building:** A permanent structure having a roof supported by columns, or walls, for the shelter, support or enclosure of persons, animals or possessions, is a building. When any portion thereof is completely separated from every other part thereof by division walls from the ground up, and without openings allowing ingress or egress of persons, each separated portion of the building shall be deemed a separate building.
- (3) **Building Envelope (also called "buildable area"):** The ground area of a lot which is defined by the minimum setback and spacing requirements within which construction of a principal and any attached accessory structures (such as a garage) is permitted by the Ordinance. For condominium developments, the building envelope shall be illustrated on a site plan.

- (4) **Building Height:** The vertical distance measured from the established grade to the highest point of the roof for flat roofs; to the deck line for mansard roofs; and to the mean height (between eaves and ridges) for gable, hip, gambrel, and A-frame roofs.

- (5) **Building Permit:** An authorization issued by the Chief Building Official to move, erect or alter a structure within the City.

Buffer Zone: A strip of land often required between certain zoning districts reserved for plant material, berms, walls or fencing singularly or in combination to serve as a visual and/or noise barrier.

Bulk: The term used to indicate the size and setbacks of buildings and structures and their location with respect to one another, including standards for the height and area of buildings; the location of exterior walls in relation to lot lines, streets, and other buildings; gross floor area of buildings in relation to lot area, open space, and the amount of lot area required for each dwelling unit.

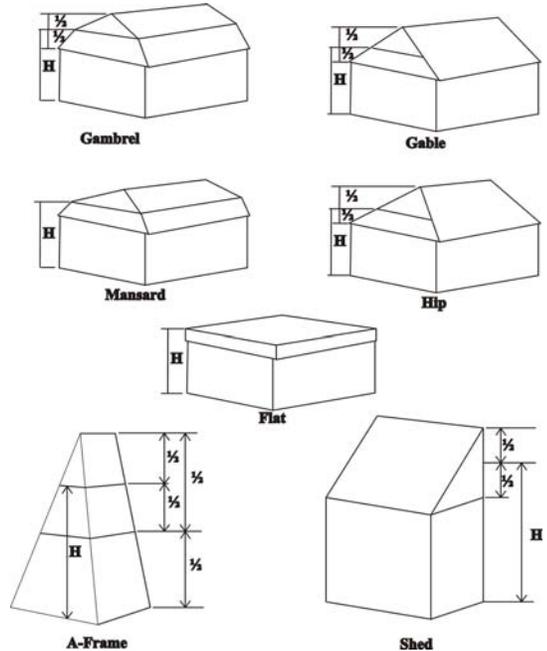


Figure 90-3-2: Building Height

Section 90-4. Definitions "C"

Caliper: The diameter of a tree trunk, measured at breast height.

Caretaker Living Quarters: An independent residential dwelling unit designed for and occupied by no more than two (2) persons, where at least one (1) is employed to look after goods, buildings, or property on the parcel on which the living quarters are located.

Carport: A shelter for vehicles consisting of a roof extended from a wall or a building or a partially open structure consisting of a roof and possibly walls. Carports shall comply with all yard requirements applicable to private garages.

Child Care/Residential Care Facilities:

- (1) Child Care Facility: A facility for the care of children under eighteen (18) years of age, as licensed and regulated by the State under Michigan Public Act 116 of 1973 and the associated rules promulgated by the State Department of Human Services.
- (2) Child Care Centers, Nursery Schools, and Day Nurseries: A facility, other than a private residence, receiving pre-school or school age children for group care for periods of less than twenty-four (24) hours a day, and where the parents or guardians are not immediately available to the child. It includes a facility which provides care for not less than two (2) consecutive weeks, regardless of the number of hours of care per day. The facility is generally described as a child care center, day care center, day nursery, nursery school, parent cooperative preschool, play group, or drop-in center. "Child care center" or "day care center" does not include a Sunday school conducted by a religious organization where children are cared for during short periods of time while persons responsible for such children are attending religious services.
- (3) Family day care home (six (6) or fewer children less than twenty-four (24) hours per day): A private home in which one (1) but less than six (6) minor children are received for care and supervision for periods of less than twenty-four (24) hours a day, unattended by a parent or legal guardian, except children related to an adult member of the family by blood, marriage or adoption. It includes a home that gives care to an unrelated child for more than four (4) weeks during a calendar year.
- (4) Group day care home (seven (7) to twelve (12) children less than twenty-four (24) hours per day): A private home in which more than seven (7) but not more than twelve (12) children are given care and supervision for periods of less than twenty-four (24) hours a day unattended by a parent or legal guardian, except children related to an adult member of the family by blood, marriage or adoption. It includes a home that gives care to an unrelated child for more than four (4) weeks during a calendar year.
- (5) Foster Family Group Home: A private home in which more than four (4), but less than seven (7) children, who are not related to an adult member of the household by blood, marriage, or adoption, are provided care for twenty-four (24) hours a day, for four (4) or more days a week, for two (2) or more consecutive weeks, unattended by a parent or legal guardian.
- (6) Foster Family Home: A private home in which one (1) but not more than four (4) minor children, who are not related to an adult member of the household by blood, marriage, or adoption, are given care and supervision for 24 hours a day,

for four or more days a week, for two or more consecutive weeks, unattended by a parent or legal guardian.

Church or Other Place of Worship: A site used for the regular assembly of persons for the conduct of religious services and related accessory uses. Accessory uses may include rectories, living quarters for church ministry and other members of the religious order who carry out their duties primarily on the site, religious education classes, day care, outdoor recreation facilities, religious office space and youth centers. Rescue missions, tent revivals and other temporary assemblies are not included in this definition.

City Officials:

- (1) City Council or Council: The elected, legislative body of the City of Wyoming.
- (2) Chief Building Official: The chief administrator of the Building Department.
- (3) Planning Commission or Commission: The Planning Commission of the City of Wyoming, as appointed by the Mayor and endorsed by the City Council, pursuant to the Municipal Planning Act.
- (4) City Planner: The administrator of the Planning Department for the City of Wyoming.
- (5) Board of Zoning Appeals or Board: The Board of Zoning Appeals of the City of Wyoming. Members are appointed by City Council pursuant to the Zoning Act.

Clinic, Medical: An establishment where human patients who are not lodged overnight are admitted for examination and treatment by a group of physicians, dentists or similar professionals. A medical clinic may incorporate customary laboratories and pharmacies incidental to or necessary for its operation or to the service of its patients, but may not include facilities for overnight patient care or major surgery.

Club: A non-profit organization of persons for charitable, fraternal or social purposes or for the promulgation of agriculture, sports, arts, science, literature, politics or the like, but not operated to espouse beliefs or further activity that is not in conformance with the Constitution of the United States or any laws or ordinances. The facilities owned or used by the organization may be referred to as a “club” in this Ordinance.

Commercial Use: The use of property for retail sales or similar businesses where goods or services are sold or provided directly to the consumer. As used in this Ordinance, “commercial use” shall not include industrial, manufacturing, or wholesale businesses.

Commercial Vehicle: Any vehicle bearing or required to bear commercial license plates and which falls into one (1) or more of the following categories:

- (1) Truck tractor;
- (2) Semi-trailer, which shall include flat beds, stake beds, roll-off containers, tanker bodies, dump bodies and full or partial box-type enclosures.
- (3) Refrigerated and box van vehicles of a type that are commonly used for the delivery of ice cream, milk, bread, fruit or similar vending supply or delivery trucks.

- (4) Tow trucks.
- (5) Commercial hauling trucks.
- (6) Vehicle repair service trucks.
- (7) Snow plowing trucks.
- (8) Any other vehicle with a commercial license plate having a gross vehicle weight in excess of ten thousand (10,000) pounds or a total length in excess of twenty-two (22) feet.
- (9) Limousine.

Community Center: A government or nonprofit facility used for recreational, social, educational, cultural services and activities. Services may be targeted to certain populations (e.g. youth, seniors) but membership is available to the general public. Examples of services include: tax assistance, fitness training, senior meals, after school tutoring sessions, food pantries and public assemblies. This use does not include schools, places of worship, banquet facilities, social or service club, or counseling services. A community center is different than a neighborhood center, which is a use that is accessory to a residential development.

Community Special Event: A temporary outdoor use that extends beyond the normal uses and standards allowed by the zoning ordinance of the City. Special events include auto shows, art shows, festivals and nonprofit fund raisers of community-wide interest.

Condominium:

- (1) Building Area: The portion of the condominium project designed and intended for separate ownership as described in the Master Deed, regardless of whether it is intended for residential, office, industrial, business, or recreational use as a time-share unit or any other type of use.
- (2) Condominium: A system of separate ownership of individual units and/or multiple-unit projects according to the Condominium Act.
- (3) Condominium Act: State of Michigan Public Act 59 of 1978, as amended.
- (4) Condominium Subdivision Plan: Drawings and information which show the size, location, area, and boundaries of each condominium unit, building locations, the nature, location, and approximate size of common elements, and other information required by Section 66 of the Condominium Act.
- (5) Condominium Unit Site: The area designating the perimeter within which the condominium unit must be built. After construction of the condominium unit, the balance of the condominium unit site shall become a limited common element. The term "condominium unit site" shall be equivalent to the term "lot" for purposes of determining compliance of a site condominium subdivision with the provisions of this Ordinance pertaining to minimum lot size, minimum lot width, minimum lot coverage and maximum floor area ratio.
- (6) General Common Element: The area of common elements other than the limited common elements intended for the common use of all co-owners.

- (7) Limited Common Element: Area of the common elements reserved in the master deed for the exclusive use of less than all of the co-owners.
- (8) Master Deed: The condominium document recording the condominium project as approved by the City including attached exhibits and incorporating, by reference, the approved by-laws for the project and the approved condominium subdivision plan for the project.
- (9) Site Condominium Project: A condominium project designed to function in a similar manner, or as an alternative, to a platted subdivision. A residential site condominium project shall be considered as equivalent to a platted subdivision for purposes of regulation in this Ordinance.

Contractor's Yard: A site on which a building contractor stores equipment, tools, vehicles, building materials, and other appurtenances used in or associated with building or construction. A contractor's yard includes outdoor storage.

Conservation Easement: A legal agreement in which the landowner retains ownership of private property, but conveys certain specifically identified rights to a land conservation organization or a public body.

Construction: Any act or process that is carried out under a current and valid building permit consisting of on-site erection, fabrication, installation, alteration, demolition, or removal of any structure, facilities or addition thereto, including related activities. Construction implies a diligent continuance of action toward completion, and any construction that has ceased due to expiration of a permit shall be considered inactive.

Convalescent Home or Nursing Home: A nursing care facility, but excluding a hospital or a facility created by Act No. 152 of the Public Acts of 1985, as amended, being Sections 36.1 to 36.12 of the Michigan Compiled Laws, which provides organized nursing care and medical treatment to seven (7) or more unrelated individuals suffering or recovering from illness, injury, or infirmity.

Curb cut: An opening from the public street to a private driveway or public drive serving an individual site or group of sites.

Section 90-5. Definitions "D"

Density: The number of dwelling units situated on or to be developed per net acre of land. For purposes of calculating maximum density, only twenty-five percent (25%) of the acreage determined to be wetlands protected by the Goemaere-Anderson Wetland Protection Act, PA 203 of 1979, or land within the 100 year floodplain elevation shall be calculated toward the total site acreage. All open bodies of water, public rights-of-way and private road easements are excluded from this calculation.

Detention Basin: A man-made or natural water collector facility designed to collect surface water in order to impede its flow and to release the water gradually at a rate not greater than that prior to the development of the property, onto natural or man-made outlets.

Development: The construction of a new building or other structure, the relocation of an existing building, or a new use of open land.

Disposal: The final placement or destruction of either hazardous or non-hazardous substances or waste. Disposal includes placing the above substances in landfills, surface impoundments, land farms, deep well injection or underground injection wells or incineration.

Distribution Center: A use which typically involves both warehouse and office/administration functions, where short and/or long term storage takes place in connection with the distribution operations of a business.

District: A portion of the City within which, on a uniform basis, certain uses of land and buildings are permitted and within which certain regulations and requirements apply under the provisions of the Ordinance. Districts in the City of Wyoming include:

- (1) Overlay District: A district which is applied over an underlying, controlling district, except as modified or restricted by the regulations of the overlay district.
- (2) Zoning District or District: A portion of the City of Wyoming where certain uses of land and buildings are permitted and certain yards, open spaces, lot areas, and other requirements are established.

Downtown Development Authority Area: That area bordering both sides of 28th street between Division Avenue and Byron Center Avenue, as described in section 2-214 of this Code.

Drive-through Establishment: A business in which all or part of the business consists of providing goods and services from a drive-through window to patrons in motor vehicles.

Dumpster or Waste Receptacle: Any accessory exterior container used for the temporary storage of rubbish, pending collection, having the capacity of at least one cubic yard. Exterior compactors shall be considered to be dumpsters or waste receptacles for the purposes of screening regulations.

Dwelling:

- (1) Manufactured Home: A dwelling which is substantially built, constructed, assembled, and finished off the premises upon which it is intended to be located and transported to the building site on its own wheels or a flat bed trailer.
- (2) Multiple-Family: A building, or portion thereof, used or designed as residences for three (3) or more families living independently of each other and each doing their own cooking in the building, with the number of families in residence not exceeding the number of dwelling units provided. Multiple family buildings without a second floor common hallway are termed townhouses.
- (3) Single-Family: A detached building or manufactured home designed exclusively for the complete living accommodations of one (1) family, and containing one (1) dwelling unit only.
- (4) Single-Family, Attached: A single-family dwelling erected side by side to another similar unit in a single building, each unit being separated from the adjoining unit by an uninterrupted wall extending from the basement floor to the roof, and having a separate exterior entrance.

- (5) Site Built: A dwelling which is substantially built, constructed, assembled, and finished on the premises which are intended to serve as its final location. Site built dwellings also include those constructed of precut materials and panelized wall, roof and floor sections when such sections require substantial assembly and finishing on the premises which are intended to serve as its final location.
- (6) Two-Family: A detached building, designed for or occupied exclusively by two (2) families living independently of each other. May also be termed as a duplex.

Dwelling Unit:

- (1) Dwelling Unit: A building or portion thereof having cooking and housekeeping facilities, which is occupied wholly as the home, residence or sleeping place of one (1) family, either permanently or transiently, but in no case shall a motor home, trailer coach, garage, automobile chassis, portable building or tent be considered a dwelling. In case of mixed occupancy, where a building is occupied in part as a dwelling unit, the part so occupied shall be deemed a dwelling unit for the purpose of this Ordinance and shall comply with the applicable provisions relative to dwellings. A dwelling unit shall include both manufactured units (mobile homes and modular homes) and site built units.
- (2) Efficiency Unit: An efficiency unit is a dwelling unit consisting of one room, exclusive of bathroom, kitchen, hallway, closets, or dining alcove directly off the principal room providing not less than three hundred and fifty (350) square feet of floor area.

Section 90-6. Definitions “E”

Easement: A right, distinct from the ownership of the land, to cross property with facilities such as, but not limited to, driveways, roads, utility corridors, sewer lines, water lines, and transmission lines, or the right, distinct from the ownership of land, to reserve and hold an area for open space, recreation, drainage or access purposes.

Erected: Includes built, constructed, reconstructed, moved upon, and/or physical operations on the premises required for the building.

Essential Services and Public Utility:

- (1) Essential Services: The erection, construction, alteration, or maintenance of public utilities or municipal departments or commissions of underground, surface or overhead distribution of gas, electrical, cable TV, fuel, steam, or water transmission or distribution systems, collection, communication, supply or disposal systems, including mains, drains, sewers, pipes, conduits, wires, cables, transformers, splice boxes, police call boxes, fire alarm boxes, traffic signals, hydrants, towers, poles, and other similar equipment, and accessories in connection therewith but not including buildings or storage yards, other than such buildings as are primarily enclosures or shelters of the above essential service equipment reasonably necessary for furnishing adequate service to the City of Wyoming and immediate surrounding territory. Essential services shall not include wireless communication towers, unless located on public property and used as part of a governmental emergency communication network.

- (2) Public Utility: Any persons, firm, corporation, municipal department, board or commission duly authorized to furnish and furnishing, under Federal, State or municipal regulations, to the public, electricity, gas, steam, communications, telephone lines, transportation, water services or sewage disposal.

Excavation:

- (1) Excavating: Excavating shall be the removal of sand, stone, gravel or fill dirt below the average grade of the surrounding land and/or road grade, whichever shall be the highest.
- (2) Quarry Excavation: Any breaking of the ground to hollow out by cutting or digging or removing any soil or other matter to a depth greater than twelve (12) inches from the surface.
- (3) Soil Removal: Soil removal shall include the removal of any kind of soil or earth matter, including topsoil, sand, gravel, clay, or rock to a depth greater than twelve (12) inches.

Section 90-7. Definitions “F”.

Façade: The exterior wall of a building exposed to public view.

Family: Family means either of the following:

- (1) A domestic family which is one (1) or more persons living together and related by the bonds of blood, marriage or adoption, together with caretaker of the principal occupants and not more than one (1) additional unrelated person, with all of such individuals being domiciled together as a single, domestic housekeeping unit in a dwelling, or
- (2) The functional equivalent of the domestic family which is persons living together in a dwelling unit whose relationship is of a regular, permanent and distinct character or has a demonstrable and recognizable bond which renders the persons a cohesive unit. All persons must be cooking and otherwise operating as a single housekeeping unit.
- (3) This definition shall not include any society, club, fraternity, sorority, association, lodge, coterie, organization or group where the common living arrangement and/or the basis for the establishment of the functional equivalency of the domestic family is likely or contemplated to exist for a limited or temporary duration.

Fence:

- (1) Decorative/Ornamental Fence: A fence, ornamental in nature, that is more than fifty (50%) percent open to the free passage of air and light, not intended to provide a barrier to passage or for screening, including but not limited to: picket fences, wrought iron fences, and split rail fences. Decorative fencing does not include chain link or privacy fencing and may not be used as pool, protective or security fencing.

- (2) **Fence:** An artificially constructed barrier of wood, masonry, stone, wire, metal, plastic or any other manufactured material or combination of materials, used to prevent or control entrance, confine within, or mark a boundary.
- (3) **Privacy Fence:** A fence or wall that is designed to be used as a visual barrier to inhibit or prevent observation of an area and which is less than fifty (50%) percent open to the free passage of air and light.

Fill, Filling: The deposit or dumping of any matter onto or into the ground, except for common household gardening, farming and general ground care.

Floor Area:

- (1) **Gross Floor Area (GFA):** The area within the perimeter of the outside walls of the building under consideration, without deduction for hallways, stairs, closets/ storage rooms, thickness of walls, columns, or other features.
- (2) **Residential Floor Area:** For the purposes of computing the minimum allowable floor area in a residential dwelling unit, the sum of the horizontal areas of each story of the building shall be measured from the exterior faces of the exterior wall. The floor area measurement is exclusive of areas of basements, unfinished attics, attached garages, breezeways, and enclosed or unenclosed porches.

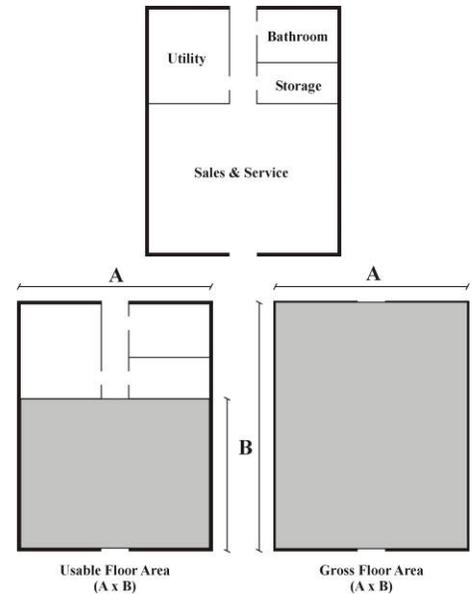


Figure 90-7: Floor Area

- (3) **Usable Floor Area (UFA):** That area used for or intended to be used for the sale of merchandise or services, or for use to serve patrons, clients or customers. Floor area which is used or intended to be used principally for the storage of merchandise, or areas such as hallways, stairways, elevator shafts, space for utilities or sanitary facilities, shall be excluded from this computation of UFA. Measurement of UFA shall be the sum of the horizontal areas of the several floors of the building, measured from the exterior faces of the exterior walls. When a detailed floor plan is not available, a factor of eighty percent (80%) shall be used to estimate the useable floor area for purposes of calculating parking requirements and other standards based on useable floor area.

Section 90-8. Definitions “G”.

Garage: An accessory building or portion of a principal building designed or used solely for the storage of motor vehicles, boats, and similar vehicles owned and used by the occupants of the building to which it is accessory.

Glare: The effect, measured at the lot line, of excessive brightness sufficient to cause annoyance, discomfort, or loss in visual performance and visibility.

Grade:

- (1) Grade, Average: The arithmetic average of the lowest and highest grade elevations in an area within six (6) feet of the foundation line of a building or structure.
- (2) Grade: The ground elevation established for the purpose of regulating the number of stories and the height of buildings.
- (3) Grade, Natural: The elevation of the ground surface in its natural state, before construction begins.

Greenbelt: A strip of land located between the property line and the front or secondary front yard building or parking setback line dedicated to for the planting of shrubs, trees and/ or grasses to serve as an obscuring screen or buffer between the property and the adjacent roadway.

Section 90-9. Definitions “H”.

Home Occupation: Any use customarily conducted entirely within the dwelling and carried on by the occupants thereof. It shall not include employees other than members of the immediate family residing within the dwelling. The use must be clearly incidental and secondary to the dwelling purposes. It shall not change the character thereof nor endanger the health, safety and welfare of any other person residing in the area because of noise, noxious odors, unsanitary or unsightly conditions, fire hazards and any other similar item, involved in or resulting from such occupation, profession or hobby. Single chair beauty salons, barbershops and instruction in a craft or fine art, are permitted on an individual basis. No article or service shall be sold or offered for sale on the premises except as is produced by such occupation. Such occupation shall not require any alterations or construction features, equipment, machinery, outdoor storage or signs not permitted in the residential areas. No home occupation shall generate more than the normal residential traffic either in amount or type. One nonilluminated nameplate, not more than two square feet in area, may be attached to the building and shall contain only the name and occupation of the residents of the premises. Day care centers, tea rooms, veterinarian’s offices, tourist homes, animal hospitals, and kennels and any other similar businesses or occupations shall not be deemed to be home occupations.

Hospital: An institution where sick or injured persons are given medical care and, in the course of same, are housed overnight, fed and provided nursing and related services. This definition shall include any related, accessory facilities such as laboratories, outpatient departments, training facilities, central service facilities and staff offices which are integral parts of the facility. This definition shall not include drug rehabilitation facilities, halfway houses, convalescent or nursing homes, institutions for mentally ill individuals, or other similar facilities.

Hotel: A series of attached, semidetached, or detached rental units which provide lodging on a temporary basis, and are offered to the public for compensation. The term “hotel” shall include tourists cabins and homes, motor courts, and motels. A hotel shall not be considered or construed to be a multiple-family dwelling.

Section 90-10. Definitions “I”.

Impervious Surface: Any man-made material which covers the surface of land and substantially reduces the infiltration of storm water to a rate of five percent (5%) or less. Impervious surfaces include but are not limited to pavement, buildings, and structures.

Indoor Recreation Facility: An establishment which provides indoor exercise facilities and/or indoor court and field sports facilities, and which may include spectator seating in conjunction with the sports facilities such as skating rinks, swimming pools, indoor golf facilities, pool or billiard halls and bowling alleys. Auditoriums and stadiums are not included.

Industrial:

- (1) **Heavy Industrial:** A use engaged in the basic processing and manufacturing of materials or products predominantly from extracted or raw materials, or a use engaged in storage of, or manufacturing processes using flammable or explosive materials, or storage or manufacturing processes that potentially involve hazardous or commonly recognized offensive conditions.
- (2) **Light Industrial:** A use engaged in the manufacture, predominantly from previously prepared materials, of finished products or parts, including processing, fabrication, assembly, treatment, packaging, incidental storage, sales, and distribution of such products, but excluding basic industrial processing.

Section 90-11. Definitions “J”.

Junk: For the purpose of this Ordinance, the term "junk" shall mean any motor vehicles, machinery, appliances, product, or merchandise with parts missing or scrap metals or other scrap materials that are damaged, deteriorated, or are in a condition which cannot be used for the purpose for which the product was manufactured.

Junk Yard/Scrap Metal Yard: Any area of more than two hundred (200) square feet unless entirely within an enclosed building, used for processing, storage, keeping or abandonment of junk, including scrap metals, other scrap materials, or reclaimed materials, or for the dismantling, demolition, or abandonment of automobiles or other vehicles or machinery or parts thereof.

Section 90-12. Definitions “K”.

Kenel: Any lot or premises on which three (3) or more dogs, cats, or other domestic animals age four (4) months or older are kept, either permanently or temporarily, either for sale, breeding, hobby, sporting activity, protection or pets, subject to the regulations set forth herein regulating private kennels.

Section 90-13. Definitions “L”.

Land Division Act: P.A. 288 of 1967, as amended (MCLA 560.101), as amended.

Loading Space: An unobstructed area within a building or on the premises and located outside of any public street, way or required parking space, designated and used for the

maneuvering, standing, loading or unloading, or delivery of items from or to a certain location.

Lot:

- (1) Lot: A parcel of land occupied, or intended to be occupied, by a main building or a group of such buildings and accessory buildings, or utilized for the principal use and uses accessory thereto, together with such yards and open spaces as are required under the provisions of this Ordinance. A lot may or may not be specifically designated as such on public records. For purposes of meeting the dimensional standards of this Ordinance, a lot does not include public rights-of-way or private road easements, but does include access easements for a service drive. A lot may be a single lot of record, a portion of a lot of record, a combination of contiguous lots of record, contiguous portions of lots of record, a parcel of land described by metes and bounds.
- (2) Lot of Record: A lot which actually exists in a subdivision plat as shown on the records of the County Register of Deeds, or a lot or parcel described by metes and bounds, the description of which has been so recorded. A lot may also mean a portion of a condominium project, as regulated by the Condominium Act, designed and intended for separate ownership and use. The term "lot" includes the term "plot" or "parcel."
- (3) Lot Area: The total horizontal area within the lot lines of a lot excluding public or private road right-of-ways or road easements.
- (4) Lot Coverage: The part or percent of the lot occupied by buildings or structures, including above ground accessory buildings or structures such as decks and porches.
- (5) Lot Depth: The mean horizontal distance from the front street line to the rear lot line
- (6) Lot Frontage: a straight line connecting the points where the side lot lines intersect the street right-of-way or private road easement.
- (7) Lot Width: The horizontal distance between the side lot lines, measured at the two (2) points where the minimum required front setback line intersects the side lot lines.

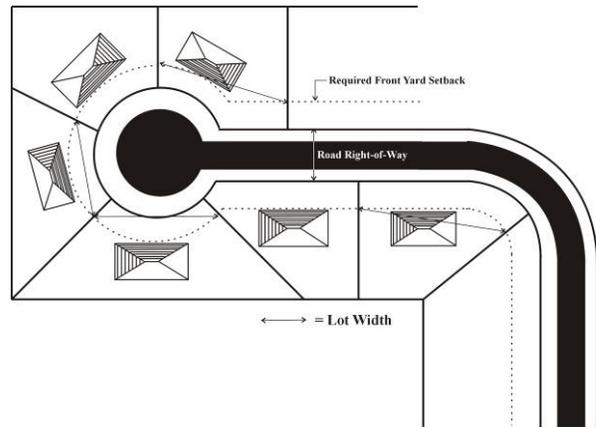


Figure 90-13-1: Lot Width

Lot Type:

- (1) Corner Lot: A lot located at the intersection of two (2) streets or a lot bounded on two (2) sides by a curving street, any two (2) chords of which form an angle of one hundred thirty-five (135) degrees or less. The point of intersection of the street lot lines is the "corner." For a curved street line, the corner is that point on the street lot line nearest to the point of intersection of the tangents described above.

- (2) Double Frontage Lot: An interior lot having frontages on two (2) more or less parallel streets as distinguished from a corner lot. In the case of a row of double frontage lots, one (1) street will be designated as the front street for all lots in the plat. If there are existing structures in the same block fronting on one (1) or both of the streets, the required front yard setback shall be maintained along both street frontages.
- (3) Interior Lot: A lot other than a corner lot with only one (1) lot line fronting on a street.

Lot Line:

- (1) Front Lot line: The front lot line shall mean any line separating the lot from the street right-of-way. In the case of a double frontage lot, the front lot line shall be that line separating the lot from the street designated as the front street in the plat or in the request for a building permit.
- (2) Rear Lot Line: The rear lot line is that boundary which is opposite and most distant from the front lot line. In the case of an irregular shaped lot, the rear lot line shall be that assumed line parallel to the front lot line not less than ten (10) feet long lying farthest from the front lot line and wholly within the lot.
- (3) Side Lot Line: Any lot line not a front lot line or a rear lot line. A side lot line separating a lot from another lot or lots is an interior side lot line.

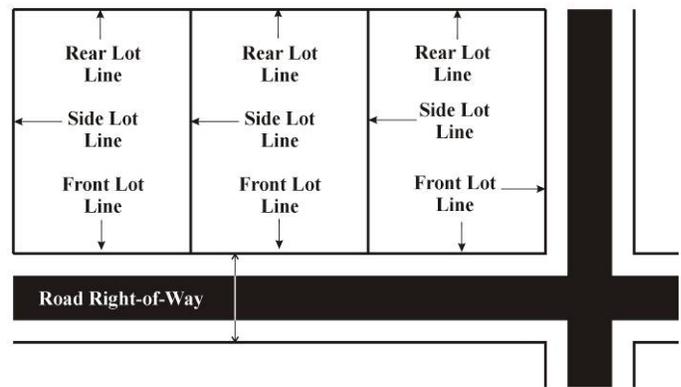


Figure 90-13-2: Lot Lines

Section 90-14. Definitions “M”.

Manufactured Home:

- (1) Manufactured Home: A detached residential dwelling unit designed for transportation after fabrication on streets or highways on its own wheels or a flat bed or other trailer, and further designed to be occupied as a dwelling without the necessity of further substantial construction or alteration, except for incidental assembly, unpacking, foundation work or construction, utility connections, skirting construction, site preparation and other minor work, construction or installation.
- (2) Manufactured Home Lot: A parcel or lot within a manufactured home park designed for the accommodation of one (1) manufactured home.
- (3) Manufactured Home Park: A parcel of land which has been designed and/or improved for the placement of manufactured homes for residential use.

Master Plan: The comprehensive long-range plan, adopted by the Wyoming Planning Commission and City Council, intended to guide growth and development within the City of Wyoming and that includes analysis, recommendation, and proposals for some or all of the following: population, economy, housing, transportation, community facilities and land use. This term is interchangeable with Land Use Plan.

Mezzanine: An intermediate floor in any story occupying not to exceed one-third (1/3) of the floor area of such story.

Motel: A series of attached, semi-detached, or detached rental units containing a bedroom, bathroom and closet space which provide for overnight lodging and are offered to the public for compensation and cater primarily to the public traveling by motor vehicle. A motel may include a restaurant or cocktail lounge, public banquet halls, ballrooms or meeting rooms.

Section 90-15. Definitions “N”.

Nonconforming:

- (1) **Nonconforming Building:** A building or portion thereof lawfully existing at the effective date of this Ordinance or amendments thereto, that does not conform to the provisions of this Ordinance for the district in which it is located.
- (2) **Nonconforming Lot:** Any lot which was of record at the time of adoption of this Ordinance that does not meet the requirements of this Ordinance for lot width and depth and available space for yards.
- (3) **Nonconforming Site:** A site that lawfully occupied land on the effective date of this Article or any amendments thereto, that does not conform to the site regulations of the district in which it is located, including landscaping and parking requirements.
- (4) **Nonconforming Use:** A use that lawfully occupied a building or land on the effective date of this Article or any amendments thereto, that does not conform to the use regulations of the district in which it is located.

Nuisance: An offensive, annoying, unpleasant, or obnoxious thing or practice, a cause or source of annoyance, especially a continuing or repeating invasion of any physical characteristics of activity or use across a property line which can be perceived by or affects a human being, or the generation of an excessive or concentrated movement of people or things, such as, but not limited to: noise, dust, smoke, odor, glare, fumes, flashes, vibration, shock waves, heat, electronic or atomic radiation, objectionable effluent, noise of congregation of people and traffic.

Nursery school, day care and child care center: See: “Child Care/Residential Care Facilities.”

Section 90-16. Definitions “O”.

Open Space: An area that is intended to provide light and air, and is designed for either environmental, scenic, or recreational purposes. Open space may include, lawns, decorative planting, walkways, gazebos, active and passive recreation areas,

playgrounds, fountains, swimming pools, woodlands, wetlands and water courses. Open space shall not be deemed to include driveways, parking lots or other surfaces designed or intended for vehicular travel, but may include a recreational clubhouse or recreation center.

Outdoor Display, Sales, or Storage: Outdoor display, sales, or storage that is accessory to a permitted commercial use or a business operated substantially outside of any building, including: retail sales of garden supplies and equipment (including but not limited to, trees, shrubbery, plants, flowers, seed, topsoil, trellises, and lawn furniture); sale of building and lumber supplies; automobiles, recreational vehicles, boats, mobile homes, garages, swimming pools, playground equipment, mowing equipment, farm implements, construction equipment and similar materials or equipment; rental and leasing establishments; and year-round flea markets, farmer's markets, roadside stands, and auctions.

Overhead Door: Any at-grade door in excess of sixty-four (64) square feet in surface area.

Section 90-17. Definitions "P".

Parking and Loading:

- (1) **Loading Space:** An off-street space on the same lot with a building or group of buildings, for temporary parking for a commercial vehicle while loading and unloading merchandise or materials.
- (2) **Off-street Parking Lot:** A facility providing vehicular parking spaces along with adequate drives and aisles for maneuvering so as to provide access for entrance and exit for parking of more than two (2) vehicles.

Passenger Vehicle: A privately owned and licensed motor vehicle intended primarily for the transport of people.

Pawn Broker: A person, corporation, partnership or firm whether operating under an assumed name or any other designation, which loans money on deposit or pledge of personal property or other valuable thing, other than securities or printed evidence of indebtedness, or who deals in the purchasing of personal property or other valuable things on condition of selling the same back again at a stipulated price.

Performance Guarantee: A security, in the form of cash deposit, certified check, irrevocable bank letter of credit, or surety bond, in an amount sufficient to cover the estimated cost of improvements required as part of an application for development that is deposited with the municipality to ensure that the improvements are satisfactorily completed.

Personal Service Establishment: A use that performs services on the premises, such as barber and beauty shops; watch, radio, television, clothing and shoe repair shops; tailor; photographic studios; locksmiths; and similar establishments requiring some minor retail activity.

Pet, Domestic: See: "Animal, Domestic"

Pool or Billiard Hall: An establishment wherein three or more pool or billiard tables are provided for use by patrons. See also “Indoor Recreation Facility.”

Porch:

- (1) Enclosed Porch: A covered projection from the main wall of a building, enclosed on three (3) sides by permanent or detachable glass sash, but not used as general living space. A porch shall become general living space when the enclosed space is heated or air-conditioned and when the percentage of window area to wall is less than fifty percent (50%).
- (2) Open Porch: A covered projection from the main wall of a building, open on three sides except for wire screening. A porch shall not be considered open if enclosed by either permanent or detachable glass sash.

Principal Building and Use:

- (1) Principal Building: (also called a “main building”) A building or, where the context so indicates, a group of buildings in which is conducted the principal use of the lot on which the building is situated.
- (2) Principal Use: (also called a “main use”) The primary use to which the premises are devoted and the primary purpose for which the premises exist.

Private Ponds: An outdoor body of standing water, accumulated in an artificially constructed basin or depression in the earth, partly or completely above or below the grade of the ground surface existing prior to construction, capable of holding water to a depth greater than twenty-four (24) inches filled to capacity. Temporary basins or depressions excavated in conjunction with construction of building foundations or building basements and swimming pools, and storm water detention ponds having permanent water surfaces, are not included in this definition.

Section 90-18. Definitions “R”.

Recreation:

- (1) Outdoor Recreation: Tennis courts, athletic fields, shuffleboard, horseshoe courts, miniature golf, golf driving range, children’s amusement park or similar recreation uses.
- (2) Recreation Vehicles:
 - a. Boats and boat trailers: Boats, personal watercrafts, canoes and rafts, and the normal equipment to transport the same on the highway.
 - b. Folding tent trailer: A canvas folding structure, mounted on wheels and designed for travel and vacation use.
 - c. Motorized home: A portable dwelling designed and constructed as an integral part of a self-propelled vehicle.

- d. Pickup camper: A structure designed primarily to be mounted on a pickup or truck chassis and with sufficient equipment to render it suitable for use as a temporary dwelling for travel, recreational, and vacation uses.
- e. Snow mobile and all terrain vehicles, and the normal equipment to transport them on the highway.
- f. Travel trailer: A vehicular, portable structure built on a chassis, designed to be used as a temporary dwelling for travel, recreational and vacation uses.

Residential Care Facilities: See: Child Care/Residential Care Facilities.

Restaurant: Any establishment whose principal business is the sale of food and beverages to the customer in a ready-to-consume state, and whose method of operation is characteristic of a carry-out, drive-through, fast food, standard restaurant, or bar/lounge, or combination thereof, as defined below. This classification includes businesses whose primary product is confectionaries (i.e., ice cream shops) and non-alcoholic beverages (i.e., a coffee shop).

- (1) Restaurant, Carry-Out: A business establishment whose method of operation primarily involves sale of food, beverages, and/or frozen desserts in disposable or edible containers or wrappers in a ready-to-consume state for consumption primarily off the premises.
- (2) Restaurant, Drive-Through: A business establishment whose method of operation includes the delivery of the prepared food to the customer in a motor vehicle, typically through a drive-through window, for consumption off the premises.
- (3) Restaurant, Standard: A business establishment whose method of operation involves either the delivery of prepared food by waiters and waitresses to customers seated at tables within a completely enclosed building or the prepared food is acquired by customers at an order counter or cafeteria line and is subsequently consumed by the customers at tables within a completely enclosed building.

Retail Store: Any building or structure in which goods, wares, or merchandise are sold to the consumer for direct consumption and not for resale.

Right-of-Way: A street, alley or other thoroughfare or easement intended to be occupied by a street, crosswalk, railroad, electric transmission line, oil or gas pipeline, water main, sanitary or storm sewer main, shade trees, or other facility or use, permanently established for passage of persons or vehicles and under the legal authority of the agency having jurisdiction over the right-of-way.

Section 90-19. Definitions “S”.

Satellite Television Antenna or Dish: A structure or an apparatus capable of receiving communications from a transmitter or a transmitter relay located in a planetary orbit.

School: An institution for the teaching of children or adults including primary and secondary schools, colleges, professional schools, dance schools, business schools, trade schools, art schools, and similar facilities.

- (1) Private or Business: Any building or group of buildings, the use of which meets state requirements for primary, secondary, or higher education, offers instruction in the several branches of learning and study required to be taught in the public schools and which does not secure the major part of its funding from any governmental agency.
- (2) Business, Trade, Technical, Industrial or Vocational: A school established to provide for the teaching of industrial, aviation, clerical, managerial, or artistic skills. This definition applies to schools that are owned and operated privately for profit and that do not offer a complete educational curriculum (e.g., beauty school, modeling school).

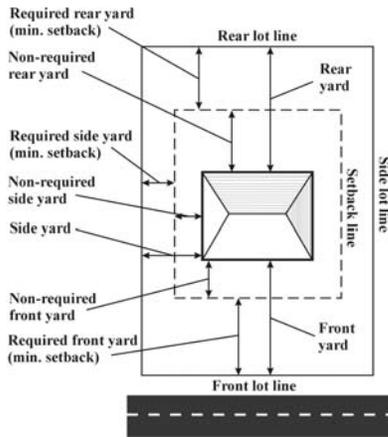
Secondhand Dealer:

- (1) Any person, including any corporation or other entity, whose business is that of dealing in buying, selling, storing or exchanging secondhand goods, articles or merchandise of any kind, including lead pipe, tools, lighting fixtures, plumbing fixtures, radios, watches, jewelry, precious stones, scrap metals, musical instruments, electrical motors, electrical appliances, firearms, automotive parts and accessories, bicycles, wearing apparel, micrometers, or any article of personal property or other valuable thing. This definition does not include:
 - (a) Householders selling articles owned and possessed by themselves or executors or administrators of any such householder.
 - (b) New articles, wares or merchandise from manufacturers, wholesale distributors or jobbers for retail sale to customers.
 - (c) Used car dealers.
 - (d) Secondhand or used tires when such tires are removed from vehicles to which such tires are attached in the presence of the person receiving them.
- (2) This definition does not apply to persons whose principal business is that of dealing in new goods, articles and merchandise and who do not buy secondhand goods, articles and merchandise outright, but occasionally accept in trade or repossess household appliances, watches, jewelry, precious stones and musical instruments.
- (3) Outdoor secondhand sales, except as permitted under Section 90-371 (19), are prohibited.
- (4) Temporary businesses established for the purchase or sale of secondhand merchandise are prohibited.
- (5) Nonprofit organizations selling donated goods are required to obtain Special Use Approval.

Self-storage Warehouse or Facility: A building or group of buildings in a controlled-access and fenced compound that contains varying sizes of individual, compartmentalized, and controlled-access stalls or lockers for the storage of customer's goods or wares.

Semi-Trailer: A trailer, which may be enclosed or not enclosed, having wheels generally only at the rear, and supported in front by a truck tractor or towing vehicle.

Senior Assisted Living: A type of semi independent housing facility for senior citizens containing congregate kitchen, dining, and living areas, but with separate sleeping rooms. Such facilities typically provide special support services, such as transportation and limited medical care.



Senior Apartments and Senior Independent Living: Multiple-family dwelling units occupied by persons 55 years of age or older. Units will include individual kitchen facilities; however, common dining and community facilities may be provided.

Service Truck: A pick-up or van that is used in conjunction with a repair or maintenance business, such as a plumbing, electrical or carpentry business.

Setback Line: A line marking the setback distance from the lot line which establishes the minimum required front, side, or rear yard of a lot.

Figure 90-19-1: Setbacks

Shopping Center: A structure of group of structures located on the same lot or parcel which is developed in accordance with an overall plan and designed and built as an interrelated project that provides a variety of commercial uses and also provides for common off-street parking, pedestrian access and vehicular movements. Buildings constructed on out lots shall not be considered part of the shopping center unless access and parking easements are provided.

Sign: For definitions of specific sign types and terms, see Article XXIII, Section 90-792.

Sight Distance: The length of roadway visible to the driver. Generally related to the distance or time (perception/reaction time) sufficient for the driver to execute a maneuver (turn from driveway or side street, stop or pass) without striking another vehicle or object in the roadway. Required sight distance shall be based on the standards of the City of Wyoming Engineering Department.

Site Plan: A scaled drawing, containing all required information and drawn in compliance with Section 90-48, illustrating existing conditions and containing the elements required as applicable to the proposed development to ensure compliance with zoning provisions.

Special Use Approval: A use of land not permitted by right, but which is permitted within a particular zoning district after demonstration of compliance with specific special land use standards, as determined by the Planning Commission.

Story:

- (1) Full Story: That portion of a building, other than a basement or mezzanine, included between the surface of any floor and the floor next above it, or if there is no floor above it, then the space between the floor and the ceiling above it.

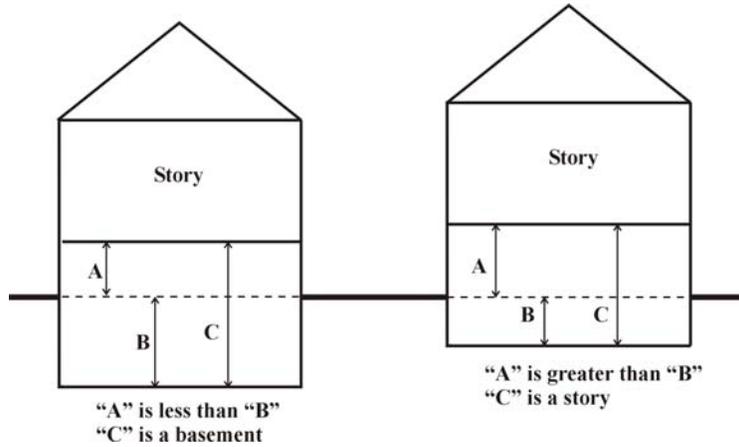


Figure 90-19-2: Story and Basement

- (2) Half Story: The part of a building between a pitched roof and the uppermost full story, having a floor area which does not exceed fifty percent (50%) of the floor area of the story immediately below. Tri-level shall be considered as one and one-half (1½) stories.

Street:

- (1) Alley: A dedicated public way which affords only a secondary means of access to abutting property and is not intended for general traffic circulation, parking, standing or loading.
- (2) Collector Street: A street used to carry traffic from local streets to arterials, including principal entrance streets of large residential developments or having a planned right-of-way width of at least eighty (80) feet.
- (3) Cul-de-sac: A local street of short length, having one (1) end permanently terminated by a vehicular turnaround.
- (4) Local Street: A street used primarily for access to abutting properties.
- (5) Major Thoroughfare: A street designed as a regional, major or minor arterial on the Wyoming Thoroughfare Plan, as adopted by the planning commission, in accordance with Act 285 of the Public Acts of Michigan of 1931 (MCL125.31 et. seq., as amended).
- (6) Marginal Access Street: A local street that is parallel and adjacent to arterials and which provides access to abutting properties and protection from through traffic.
- (7) Private Road: A street that is owned, and maintained by the landowners served and has not been dedicated to the city, county or state as a public street.

- (8) **Public Street:** A public dedicated right-of-way which affords traffic circulation and principal means of access to abutting property, including avenue, place, way, drive, line, boulevard, highway, road, and other thoroughfare, except an alley.

Structure: Anything constructed or erected, the use of which requires location above the ground or attached to something having location on the ground. A structure will include buildings (see “*Buildings*”), fences, walls, decks, towers, pools, and other similar above ground structures.

Structural Alteration: Any change in the supporting members of a building or structure, such as bearing walls or partitions, columns, beams or girders, or any change in the width or number of exits, or any substantial change in the roof.

Subdivision: The division of a tract of land into two or more lots, building sites, or other divisions for the purpose of sale or building development, in accordance with the Land Division Act and the Wyoming City Code - Subdivision Regulations, Section 74-176.

Swimming Pool: A permanent, structure or container located either above or below grade designed to allow holding of water to a depth of greater than 24 inches, intended for swimming, bathing or relaxation. The definition of swimming pool includes spa, hot tubs and similar devices.

Section 90-20. Definitions “T”.

Truck Terminal: A structure to which goods, except raw or unprocessed agricultural products, natural minerals, equipment or other resources, are delivered for immediate distribution or to be amalgamated or divided for delivery in larger or smaller units to other points, or for distribution, amalgamation, or division involving transfer to other modes of transportation. Also, a semi-trailer storage yard facility.

Section 90-21. Definitions “U”

Underground Storage Tank System: A tank or combination of tanks, including underground pipes connected to the tank or tanks, which is, was, or may have been used to contain an accumulation of hazardous substances, as defined in Part 213 of the State of Michigan Natural Resources and Environmental Protection Act, 1994 Public Act 451, as amended.

Use: See “Accessory Use, Building or Structure” and/or “Principal Building or Use”

Section 90-22. Definitions “V”.

Variance: A relaxation or modification of the requirements of this Ordinance, authorized by the Zoning Board of Appeals, under the provisions of this Ordinance and Act 110 of the Public Acts of 2006, as amended.

- (1) **Non-Use Variance:** A variance approved by the Zoning Board of Appeals relaxing one (1) or more requirements of this Ordinance related to buildings, lots, setbacks, and other dimensional requirements.

- (2) Use Variance: A variance approved by the Zoning Board of Appeals allowing a use of land not otherwise permitted in a zoning district.

Vehicle Related Uses:

- (1) Service Station: A building or structure designed or used for the retail sale or provision of fuels (which must be stored only in underground tanks), and may include the sale of lubricants, air, batteries, water and other operating commodities for motor vehicles, and the customary space and facilities for the installation of these commodities on or in vehicles, and including space for facilities for the temporary short-term storage, minor repair, or servicing. A service station may also include retails sales of convenience and grocery items.
- (2) Vehicle Wash Establishment: A building, or portion thereof, with a primary purpose of washing motor vehicles.

Veterinary Clinic: An institution which is licensed by the Michigan Department of Health to provide for the care, diagnosis, and treatment of sick or injured animals, including those in need of medical or surgical attention. A veterinary clinic may include customary pens or cages for the overnight boarding of animals indoors and such related facilities as laboratories, testing services, and offices.

Section 90-23. Definitions "W".

Warehouse: A building used for long-term and short-term storage and wholesale of manufactured products, supplies, and equipment related to the operation of a single business. The use will include truck loading and unloading, provided the area dedicated to the outdoor storage of trucks and trailers is no more than the area of the warehouse building. Where the area dedicated to the outdoor storage of trucks and trailers exceeds the area of the warehouse building, the use shall be considered a truck terminal.

Wireless Communication:

- (1) Attached Wireless Communication Facility (Antennae): Any wireless communication facility affixed to an existing structure, such as a building, tower, water tank, utility pole, etc., utilized to receive and transmit federally or state licensed communications services via dually licensed segments of the radio frequency spectrum. This definition shall not include support structures.
- (2) Wireless Communication Facility: All facilities, structural, attached, accessory or otherwise, related to the use of the radio frequency spectrum for the purposes of transmitting or receiving radio signals and may include, but is not limited to, radio and television towers, telephone devices and exchanges, microwave relay towers, telephone transmission equipment buildings, and commercial mobile radio service facilities. Not included within this definition are: citizen band radio facilities, short wave facilities, ham and amateur radio facilities, television reception antennae, satellite dishes, and governmental facilities which are subject to state and federal law or regulations that preempt municipal regulatory authority. A wireless communication facility shall not be included under the existing definition of "essential services."

- (3) Collocation: The location by two (2) or more wireless communications providers, public authority or other dually authorized party of wireless communications facilities on a common structure, tower or building, in a manner that reduces the overall need for additional or multiple freestanding single use communications facilities and/or support structures within the City of Wyoming.
- (4) Wireless Communication Support Structure (Tower): Any wireless communication facility erected or modified to support attached wireless communication facilities, or other antennae or facilities, including supporting lines, cables, wires, braces and masts intended primarily for the purpose of mounting an attached wireless communication facility or similar apparatus above grade. This includes, but is not limited to, any ground or roof-mounted pole, monopole, light pole, utility pole, wood pole, spire, other similar structure or combination thereof, subject to the requirements of this Ordinance.

Section 90-24. Definitions “Y”.

Yard:

- (1) Front Yard: The space between the front line of the building and the front lot line, extending from one side lot line to the other side lot line. A corner lot will be considered to have two front yards: a primary front yard and a secondary front yard.
- (2) Rear Yard: The space between the rear line of the building and the rear lot line, extending from one side lot line to the other side lot line. In the case of a corner lot, the rear yard may be opposite either street frontage, but there shall be only one (1) rear yard.
- (3) Required Yard: That portion of a lot that meets the required minimum front, side, or rear setback of the zoning district in which the property is located.
- (4) Side Yard: A yard on the same lot with a building between the side lot line and the nearest side line of the building and extending from the rear to the front yard. A side yard will also be referred to as an interior side yard where it adjoins another lot.
- (5) Yard: An open space, unoccupied and unobstructed from the ground upward, except as otherwise provided herein, and being on the same lot with a building. The measurement of a yard shall be construed as the minimum horizontal distance between the lot line and the building or structure.

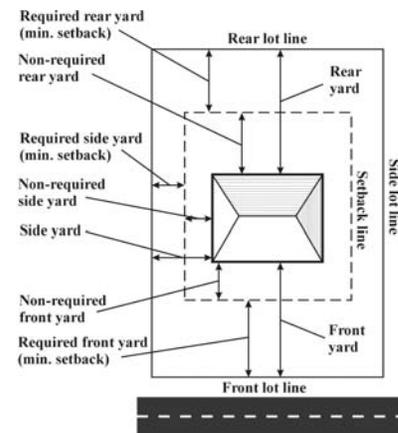


Figure 90-24 : Yards

Section 90-25. Definitions “Z”.

Zoning Enabling Act: The Michigan Zoning Enabling Act, P. A. 110 of 2006, as amended.

Section 3. This ordinance shall be in full force and effect on the 16th day of August, 2011.

I hereby certify that the above-entitled Ordinance was adopted by the City of Wyoming at a regular session of the City Council held on the 1st day of August, 2011.

Heidi A. Isakson
Wyoming City Clerk

Ordinance No. 5-11



June 27, 2011

MAYOR
Jack A. Poll

AT-LARGE COUNCILMEMBER
Sam Bolt

AT-LARGE COUNCILMEMBER
Dan Burrill

AT-LARGE COUNCILMEMBER
Kent Vanderwood

1ST WARD COUNCILMEMBER
William A. VerHulst

2ND WARD COUNCILMEMBER
Richard K. Pastoor

3RD WARD COUNCILMEMBER
Joanne M. Voorhees

CITY MANAGER
Curtis L. Holt

Ms. Heidi A. Isakson
City Clerk
Wyoming, MI

Subject: Request to amend Zoning Code Chapter 90 Article I
pertaining to Definitions.

Recommendation: To approve the subject Zoning Code amendments.

Dear Ms. Isakson:

The above referenced request was reviewed by the Wyoming Planning Commission at its regular meeting on June 21, 2011. Staff had the following comments:

The Definition section of the Wyoming Zoning Code has remained essentially unchanged since 1979. While the application of this section of the Code has proven to not be particularly problematic, it is recognized that improvements to this area of the Ordinance would have numerous community benefits. The Development Review Team believes the attached Zoning Code Definition amendments would accomplish the following:

- Provide greater clarity of the intent of the Zoning Code to the public, City staff, Board of Zoning Appeals, Planning Commission and City Council
- Attains compatibility with current societal understandings regarding the meaning of certain words contained within the Zoning Code
- Provide uniformity with many nearby communities Zoning Codes
- Greatly expands the range of definitions to provide greater understanding of the Zoning Code
- Inserts a substantial number of illustrations to aid in understanding the intent of a particular definition
- Provides clarity to particular words repeated within definitions
- Provides a structure as to how interpretations of the definitions will be determined
- Retains particular definitions exclusive to Wyoming, such as Community Centers, Home Occupations, and Second Hand Dealers
- Removes contradictory definitions that have been superseded and provided in other areas of the Zoning Code, such as references to signs

The Development Review Team suggested the Planning Commission recommend to the City Council the proposed Zoning Code Definition amendments.

A motion was made by Bloomquist, supported by Woodruff, to recommend to City Council the Zoning Code Definition amendments as recommended by the DRT. After discussion, the motion carried unanimously. Additional explanation regarding this proposal may be obtained from the Planning Commission minutes of June 21, 2011.

Respectfully submitted,



Timothy Cochran, City Planner
Planning and Development Department

cc: Curtis Holt, City Manager
Rebecca Rynbrandt, Director of Community Services